

Public Document Pack



WOKINGHAM
BOROUGH COUNCIL

Civic Offices
Shute End
Wokingham
RG40 1BN

Tel: 0118 974 6058
E-mail: democratic.services@wokingham.gov.uk

To:- All Committee Members

OVERVIEW AND SCRUTINY MANAGEMENT COMMITTEE - MONDAY, 3RD SEPTEMBER, 2018

I am now able to enclose, for consideration at the next Monday, 3rd September, 2018 meeting of the Overview and Scrutiny Management Committee, the following reports that were marked as 'to follow' on the agenda sent out recently.

Agenda No Item

36. **Reference Information for consideration by the Committee (Pages 3 - 286)**

- Report – Response to Call-In
- Response to Call-In – Supporting Information
- Executive Member Email
- School Consultation Comments
- Notices of Intent – Crossings
- Notification Sent to Schools/Town and Parish Councils
- Press Releases
- School Crossing Patrol Service Letters
- Consultation Report 2017
- Consultation Report 2018
- Safe School Crossings Consultation
- School Crossing Patrol Organiser – Job Description
- Salary and Other Expenditure Information
- Equality Impact Assessment – April 2018
- Road Safety Audits
- Construction Plans
- Designer Responses
- Councillor Keith Baker – Written Evidence

Yours sincerely

A handwritten signature in black ink, appearing to read 'Manjeet Gill', enclosed within a light blue rectangular border.

Manjeet Gill
Interim Chief Executive

TITLE	School Crossing Patrol Service — Consultation Report 2018 : CALL-IN RESPONSE
FOR CONSIDERATION BY	Overview and Scrutiny Management Committee on 3 September 2018
WARD	Coronation; Emmbrook; Loddon; Maiden Erleigh; Norreys; South Lake; Twyford; Wokingham Without.
DIRECTOR	Josie Wragg, Director of Locality and Customer Services

OUTCOME / BENEFITS TO THE COMMUNITY

The decision would cease the provision of a discretionary service to provide school crossing patrollers currently operating at seven locations across the borough during the morning and afternoon start and finish of the school day only. New crossings at these locations will provide safe crossing points to serve the wider community that will be available to all users at all times of the day throughout the year.

RECOMMENDATION

That the Overview & Scrutiny Management Committee uphold the decision of the Executive Committee:

That:

- 1) WBC continue with its proposal to provide safe, permanent crossings at the seven locations that currently have a school crossing patroller, and, following their installation, remove the school crossing patrol service once the permanent crossings are complete as set out in Option 2, Appendix 1 of the report;
- 2) all affected schools be reminded that they have access to the Council's road safety and My Journey teams who can facilitate further road safety training for pupils if requested.

SUMMARY OF REPORT

The decision to continue with the withdrawal of the discretionary service to provide school crossing patrollers has been called-in by five non-executive elected Members.

This report provides clarification on the points raised within the call-in.

Background

School Crossing Patrollers (SCP) are employed by the Council to help children cross the road safely, primarily to and from school. The provision of this service is discretionary. The Medium Term Financial Plan agreed at Full Council in 2015 to remove funding for the School Crossing Patrol (SCP) service and therefore end the provision of all School Crossing Patrollers in Wokingham Borough. This has been partially implemented with the removal of eight patrollers that operated on existing controlled crossing at the end of July 2017.

To complete the implementation and to maintain safer crossing facilities on routes to schools where a patroller operated it was proposed to implement new formal crossing facilities before the removal of the remaining seven patrollers. Removing the remaining SCP service will result in a reduction of service and the council therefore consulted on this in line with the constitution. The public consultation ran between 22 January and 2 March 2018. The results of the consultation did not raise any specific issues or comments that were not anticipated or that should prevent WBC continuing with its proposal to remove the SCP service and provide safe permanent crossings at the seven locations. This was reported to the Executive in July 2018 and the Executive agreed with the recommendation however this decision has now been called-in by five non-executive elected Members. Within the call-in, the following five specific grounds relating to the Councils decision making principles have been stated:

1.4.2 Principles of Decision Making

All decisions of the Council will be made in accordance with the following principles:

- a) proportionality (i.e. the action must be proportionate to the desired outcome);
- b) due consultation and the taking of professional advice from Officers;
- d) a presumption in favour of openness;
- e) clarity of aims and desired outcomes;
- f) details of all options and reasons for decision have not been recorded.

Analysis of Issues

Below are the stated reasons for the Call-In and the Council's response to each.

Reason 1:

The decision has contravened Section 1.4.2 a) of the Council's constitution, in that the action being proposed is not proportionate to the desired outcome.

- a. A blanket decision has been made to replace all remaining School Crossing Patrols with pedestrian crossings, despite the different characteristics and requirements of the locations.**
- b. The desired outcome appears to be to save money yet the costs both of the crossings and the school crossing patrol have not been fully or correctly stated. None of the other options quoted had any costs provided.**
- c. The decision was made on the basis of costs for crossings which have not yet been designed — as a redesign is taking place at 4 sites due to the first design not being suitable; it is therefore also unclear whether a crossing is actually the right answer for those locations, as well as the costs being unknown.**

Response:

As stated in the Executive report, the council approved the removal of funding for the SCP service from the Medium Term Financial Plan (MTFP) at Full Council in 2015. Implementation of this decision was delayed and a 'special item' within the MTFP was agreed for 2017/18 to cover the cost of the service for one final year subject to rolling the unspent balance forward into 2018/19 to fund the reduced service to July 2018 to enable the safe removal of the service. The decision to remove the service had therefore already been made but the constitution requires the council to consult on any changes result in a reduction in service.

The provision of a school crossing patrol service is discretionary and the council is under no obligation to replace patrollers with any other form of crossing facility. However, recognising the need to ensure safer routes to schools the council proposed to provide formal crossing facilities at or near to where a SCP operated.

The proposals to provide new crossings was in response to the council's withdrawal of funding for the service. The long term savings will be realised as a result of no longer requiring revenue funding to support the service which had a budget of over £90,000 at the time the decision was made with the MTFP in 2015. All options were presented and costed in the Executive report, see Appendix 1. All the schemes were designed and costed and at the time of the report were correct. There were no proposals that were unsuitable and all designs were considered on a site by site basis with the appropriate type of provision being proposed for each site, all confirmed following Road Safety Audits. The four sites undergoing redesign are as a result of the ward member requesting a different solution, different to the engineers proposed solution.

The cost of the provision of the new crossings was a financial implication but was not the main driver for the decision to continue with the removal of the service. The funding for and delivery of the new crossings had been approved through the Capital Programme budget setting process (MTFP) earlier this year in which a capital bid was approved for a sum of £600,000 to deliver the new or improved permanent crossing facilities at the schools currently service by SCP. Consequently, there was no constitutional need for the Executive to agree in July to provide the crossings or their funding as this decision had already been made. However, this was relevant to the decision that Executive was being asked to make which was the removal of the SCP service as the crossings facilitated continued safe crossing of the highway. The delivery of this programme of works was initially programmed to be completed by the end of the school summer holiday period to coincide with the end of the SCP service should the decision be withheld.

Reason 2:

The decision has contravened Section 1.4.2 b) of the Council's constitution, in that due consultation and the taking of professional advice from officers has not occurred.

- a. The consultation was not complete at the point at which the decisions to withdraw the service were made**
- b. The consultation was not carried out at an early stage, and was not meaningful, and contravened the council's own rules on consultation**

- c. **There was no consultation with ward members on the withdrawal of the service**
- d. **The replacement crossings were designed without reference to ward members and before the consultation was carried out — and ward members were not even informed until mid-July that the crossings had been designed.**
- e. **Wokingham Town Council's consultation response has not been taken into consideration**
- f. **Letters putting the staff at risk of redundancy were issued before the consultation was concluded**
- g. **The council's responses to the points made by respondents are inadequate**
- h. **At least one crossing was scheduled to be installed before the decision on the outcome of the consultation took place**
- i. **At least one safety audit was done during the school holidays**
- j. **No Equality Impact Assessments accompanied the information in the report**
- k. **It is not clear that all guidelines for the provision of safe crossings have been observed It is not clear that all the evidence comparing the appropriateness of SCP versus crossings has been taken into account**

Response:

The decision to end the school crossing patrol service was made when the funding was removed from the MTFP in 2015. However, in line with the constitution on any changes that result in a reduction on service the council undertook consultation. The consultation started 22 January 2018, five months before the school holidays during which the new crossing would be provided; The consultation closed on the 2 March 2018. The Executive Member for highway and Transport gave approval to proceed on the 23 May 2018. All ward members and schools with a SCP location affected by the proposals were informed of the consultation directly and other ward members along with the wider public via press releases, WBC website and the local schools. All comments received prior to the close of the consultation were considered, including those received from Wokingham Town Council.

All comments have been considered and a summary of the key issues raised along with the response were contained in Appendix 2 of the Executive Report. The redundancy process did not start until the consultation had concluded and the results of which were reported to the Executive Member. The School Crossing Patrollers were advised that they were at risk of redundancy at a meeting on 11 June 2018.

It is therefore considered that due consultation had been completed with regard to the proposal to continue with stopping the provision of the discretionary service to provide school crossing patrollers as instigated following the withdrawal of funding in 2015.

In order to achieve delivery of any new crossings on school routes all works have to be done during the School Holidays. In order to achieve this timeframe crossing had to be designed and programmed with the contractor in advance and in this case without prior involvement with ward members. A few of the SCP locations have been vacant for a long period and recruiting a SCP has been unsuccessful. Delivery of these sites were scheduled to be done first as the outcome of the decision had no impact on any existing

staff and ensured the delivery of the remaining sites during the school holiday period was achievable.

Reason 3:

The decision has contravened Section 1.4.2 d) of the Council's constitution, in that openness has not been observed.

- a. At least one of the crossings was designed in February but this was not communicated to ward members**
- b. Some information was shared with individual members of the public but was not made available to all**
- c. No overall plan for the removal of the SCP was made available following the removal of the funding in the 2015 Medium Term Financial Plan**

Response:

The council has been open in all aspects of the decision relating to the Executive Report considered 26 July 2018. The proposals were consulted on publically from 22 January 2018 to 2 March 2018 and the results were presented in the report to the Executive. The petition on the subject was considered at full council on the 19 July 2018 with the decision being made at Executive Meeting held on the 26 July 2018.

With regard to the delivery of the new crossings, in order to achieve delivery of any new crossings on school routes, all works have to be done during the School Holidays. In order to achieve this timeframe crossing had to be designed concurrently with the ongoing consultation. It is again emphasised that the funding for and delivery of the new crossings had been approved through the Capital Programme budget setting process earlier this year in which a capital bid was approved for a sum of £600,000 in 2018/19 financial year to deliver the new or improved permanent crossing facilities at the schools currently service by SCP. This did not therefore need to approval of the Council Executive in July. The delivery of this programme of works was programmed to be completed by the end of the school summer holiday period to coincide with the end of the SCP service should the decision be withheld. However, in order to not influence or predetermine the outcome of the consultation the detail of the agreed schemes were not formally issued until after the consultation had concluded and the comments reviewed. Details of the schemes were shared with the Executive Member on the 25 May 2018 who then subsequently shared the details with the affected ward members.

Reason 4:

The decision has contravened Section 1.4.2 e) of the Council's constitution, in that clarity of aims and desired outcomes has not been achieved.

- a. The recommendations refer to a proposal for provide safe, permanent crossings — yet 4 of the crossing proposals have been rejected in the period shortly before the Executive meeting that made the decision, and therefore the decision has been made before there is certainty that the recommendation can be delivered, or that the costs are as given in the report**
- b. Due to the failure to present all costs for all options it is not possible for the Executive to have come to a properly informed decision**
- c. Due to the failure to present the Equality Impact Assessments it is not possible for the Executive to have come to a properly informed decision**

Response:

As previously stated the proposals are in response to the council's decision to remove funding for the school crossing patrol service made in 2015. The funding for and delivery of the new crossings had been approved through the Capital Programme budget setting process (MTFP) earlier this year in which a capital bid was approved for a sum of £600,000 to deliver the new or improved permanent crossing facilities at the schools currently service by SCP. The construction of the crossings themselves do not need the approval of the Council's Executive once the budget is agreed.

There were no proposals that were unsuitable and all designs were considered on a site by site basis with the appropriate type of provision being proposed for each site, all confirmed following Road Safety Audits. The four sites undergoing redesign are as a result of the ward member requesting a different solution, different to the engineers proposed solution.

The decision made by the Executive was to continue with its proposal to provide safe, permanent crossings and following their installation, remove the school crossing patrol service once the permanent crossings are complete. It is therefore incorrect to challenge the decision on the grounds that clarity of aims and desired outcomes has not been achieved since the work on the crossings has not been completed yet and as per the wording of the decision 'No SCP will be removed until a new formal crossing is completed, including any that are being redesigned as a result of requests from ward members for a different form of crossing'.

Reason 5:

The decision has contravened Section 1.4.2 f) of the Council's constitution, in that the details of all the options and reasons for the decision have not been recorded

- a. A set of options has been laid out but it excludes the most obvious option of replacing some but not all of the patrollers with crossings**
- b. There was a presumption that no funding was available for the service to continue, whereas a supplementary estimate could have been used to find the money, but was not considered**

Response:

The report clearly states the reason for the proposal and also identifies all the possible options available and subsequent financial implications in Appendix 1. This includes the option to remove some but not all patrollers (options 3 & 5). Since the recommendation did not include this there has been no reference to supplementary estimate in the body of the report.

FINANCIAL IMPLICATIONS OF THE RECOMMENDATION

The Council faces severe financial challenges over the coming years as a result of the austerity measures implemented by the Government and subsequent reductions to public sector funding. It is estimated that Wokingham Borough Council will be required to make budget reductions in excess of £20m over the next three years and all Executive decisions should be made in this context.

	How much will it Cost/ (Save)	Is there sufficient funding – if not quantify the Shortfall	Revenue or Capital?
Current Financial Year (Year 1)	(£6,000), £361,000*	Yes	(Revenue) & Capital
Next Financial Year (Year 2)	(£44,600)	Yes	(Revenue)
Following Financial Year (Year 3)	(£44,600)	Yes	(Revenue)

Other financial information relevant to the Recommendation/Decision

Reference £361,000 above in Capital funding, value based on original proposals for new crossing however following requests for a change of provision by ward members the costs will change. It should be noted that a capital budget of £600,000 has already been agreed and committed to the works and any redesign can be accommodated within that budget.

Cross-Council Implications (how does this decision impact on other Council services, including properties and priorities?)

Reasons for considering the report in Part 2

N/A

List of Background Papers

Contact Matthew Gould	Service Place
Telephone No 6460	Email matthew.gould@wokingham.gov.uk
Date August 2018	Version No. 1

This page is intentionally left blank

Overview and Scrutiny Management Committee – 3 September 2018

Call-In of Executive Decision - School Crossing Patrol Service

Response to Information Request by Councillor Imogen Shepherd-Dubey

1) A copy of Wokingham Borough Council's Consultation Policy

See main Agenda.

2) All of the information given to all the Ward Councillors on the School Crossings, such as the consultation, withdrawal notice, plans etc. for the crossings that were due to be built to replace the SCPs.

An email with details of all the proposals were provided to the Executive Member for Highways and Transport on 25 May 2018. This was then shared by the Exec. Member to all effected Ward Members

The Notice of Intent was advertised both in the Wokingham Paper and on street at the locations as well as being made available on our website at the address shown on the NOI. These were also sent to the Police and ward members. Attached NOI and location plans.

3) A copy of the consultation that was sent out or posted online, including the questions, details of where it was advertised and details of any communications that were made with the schools, parents and local residents.

Attached separately:

- The notification sent to affected schools and town and parish councils alerting them to the consultation on January 22, 2018;
- The media releases issued at the launch of the consultation and to announce the decision;
- The consultation questions;
- The notification sent to ward Members and schools to inform them of the decision.

4) A full and detailed list of the Consultation Responses.

A full list of consultation responses by school - attached.

5) A copy of WBC's Consultation Response report for both 2017 and 2018

2017 – See attached report.

2018 – Refer to July Executive item 25 on the agenda.

6) The Business Case for the withdrawal of the SCP Service.

Refer to July Executive Report item 25.

7) The job description and salary of the SCP manager. The percentage of time that they spend as the SCP Manager for the years 2015, 2016, 2017 and 2018. Also, has this post been completely filled during these years?

JD for SCP organiser attached [4458 School Crossing Patrol Organiser [updated Nov 09].doc] Salary budget for role 2017/18 = £11,091
SCP organiser was employed to undertake this role 100% of his time and has been in post for all the years referred to.

8) How much has each SCP been paid in total for this financial year and separately for the last financial year? How much does each SCP actually cost in total. How much is paid each year for uniforms, training etc. and please specify the individual costs for equipment and training.

Information attached separately.

9) How much does it cost to build Pelican, Tucan, Zebra crossing – please separate the design and the construction costs.

Unfortunately there is no fixed cost for a Tucan or a Zebra. The cost of a crossing will vary depending on location and specific site issues e.g. width of road, location of nearest power supply, are there any utility divisions required etc. The design of the crossings is covered within the scopes of works WBC have with the consultation as part of the Highway Alliance and is, therefore, not itemised.

10) How much does cost to maintain each type of crossing? What is their life expectancy? I.e. how long do they last in Wokingham, not the industry average. How often are these crossings updated?

As for maintenance of traffic signal controlled crossings - the new lights will all be LED so power consumption and therefore cost will be very low and in terms of faults the most common fault is bulbs requiring replacement. With LED this is no longer a regular issue. For new traffic signal installations the approximate annual maintenance cost is less than £600 per site. For Zebra crossing maintenance is less.

Traffic signal crossing have an operational life expectancy of 15-20 years before they would need routine replacement, in Wokingham we have aspired to refurbish our sites before they become 20 years old. Our Highway Asset Management Plan and investment strategy is based on this life span however it is worth noting that some of our locations are still operating beyond the 'expected life span'.

11) Looking at the July 26th Exec Agenda – Page 20 – Specific Risk Assessments – How often is a new risk assessment carried out for a school crossing patroller and for a school crossing and what are the costs?

Individual SCP locations and SCPs have site-specific risk assessments carried out yearly. The assessments are covered within the scopes of works WBC have with

the consultant as part of the Highway Alliance and is therefore not itemised. However for the purpose of the report a value of £600 per site was assumed.

12) Looking at the July 26th Exec Agenda – Page 21 - The Cost of Crossing Facilities is 361k – Which specific budget line does that money come from and how is it funded?

Extract from MTFP below as agreed at the Executive Meeting in Feb 2018. The majority of the capital funding has been allocated from grants with a very small proportion from developer contributions.

Country Parks and Play Area	Wokingham Borough Pedestrian Network	The provision of these crossing facilities will facilitate the full withdrawal of the SCP service	KB	600	00	600
-----------------------------	--------------------------------------	---	----	-----	----	-----

13) Where are the Equality Impact Assessments for all of the new crossings?

Equality Impact Assessments have not been done for each individual site only on the overall proposal to remove the SCP service – attached.

14) Can we please have copies of the Risk Assessment for each crossing location and the Safety Audits that should come with them?

Road Safety Audit for each new site - attached.

15) Please can we have the Full schedule of plans and construction works to implement the crossings as were on Thus 26th July?

Attached separately.

This page is intentionally left blank

Good Afternoon Keith,
Please find below details of our proposals for each of the SCP locations should the decision to proceed as proposed is given.
Very happy to talk through this with you on my return from half term.
Regards,
Matt

From: Tim Allen
Sent: 24 May 2018 11:30
To: Matthew Gould <>
Cc: Grant, Samantha) >; Ham, Mathew <>
Subject: Re: School Crossing Patrol sites 2018/19 - design review ~[UNCLASSIFIED]~

Matt,

Please find quick overview below for your conversation with Cllr Baker;

Site: Norreys Avenue, Wokingham
Description of works: Upgrade on the uncontrolled crossing point
Is scheme works in same location as SCP: yes

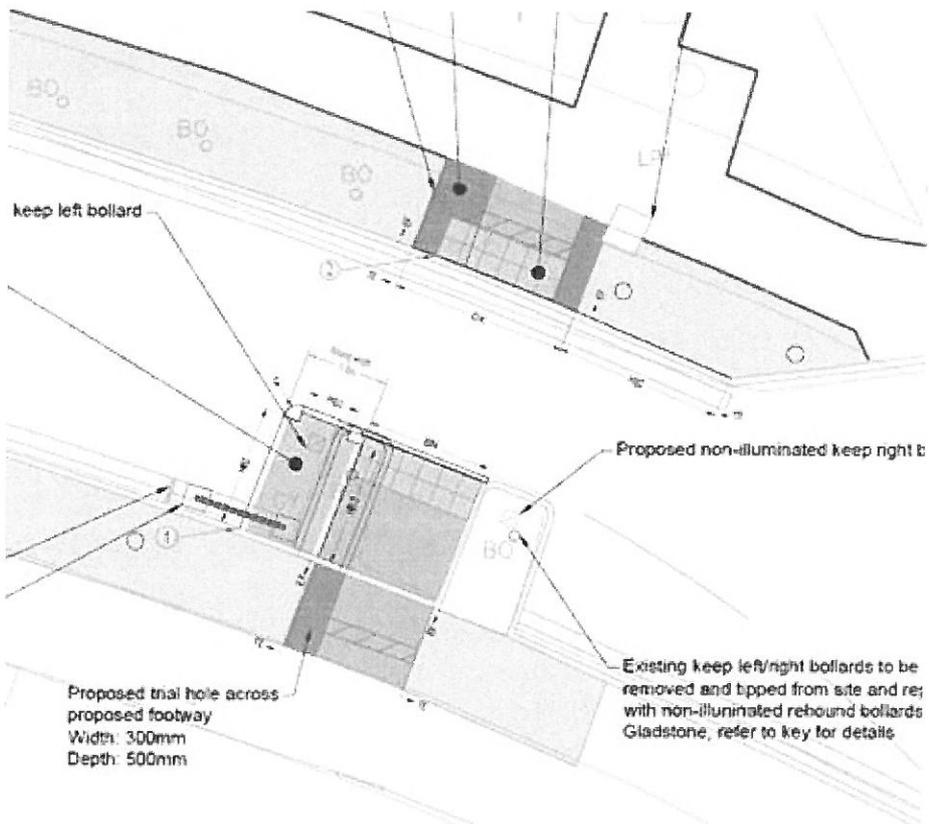
Location:



SCP site location:



Scheme design:

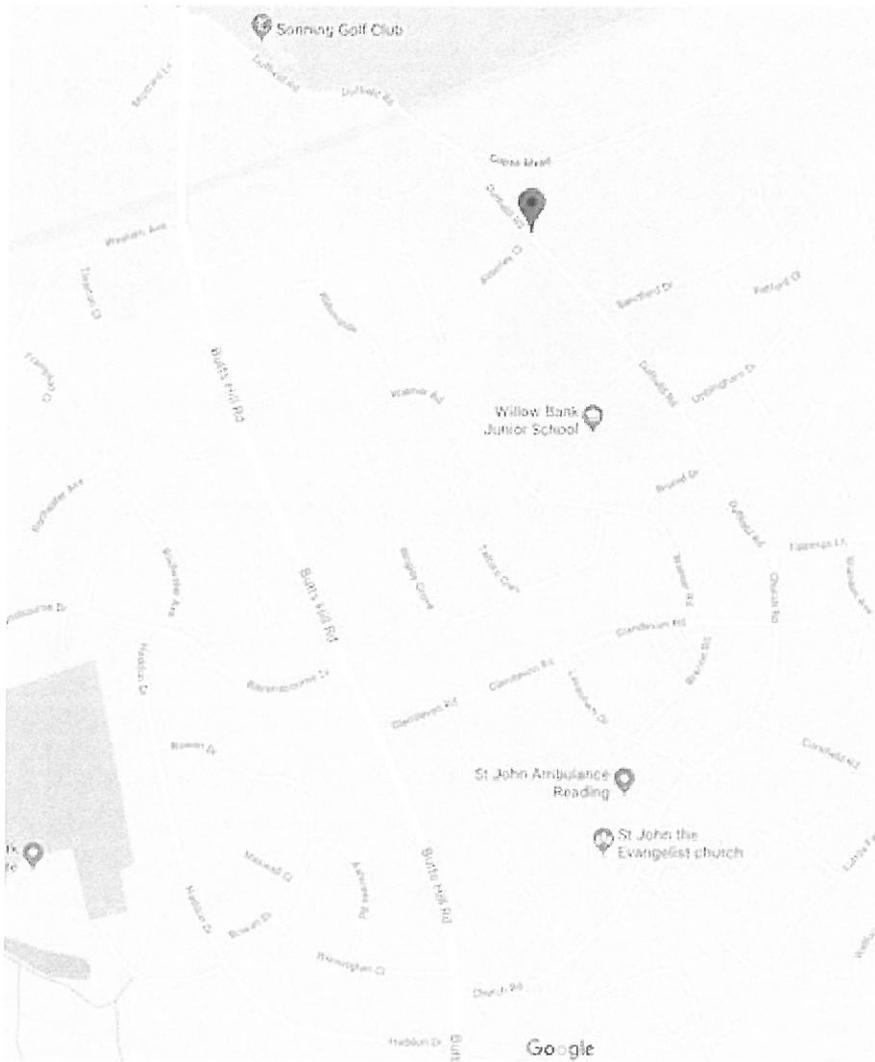


Site: Duffield Road, Woodley

Description of works: Zebra crossing installation

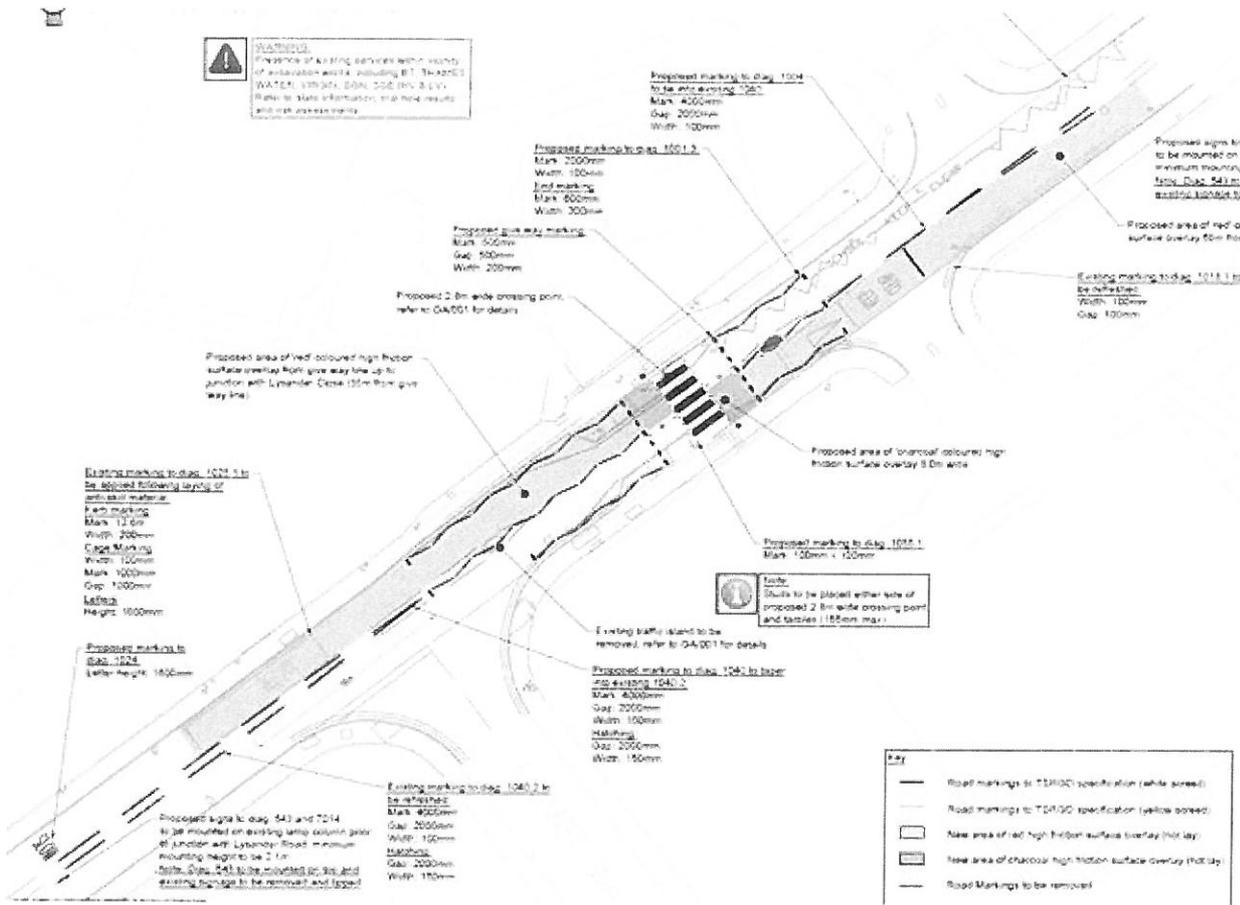
Is scheme works in same location as SCP: Yes

Location:



SCP site location:



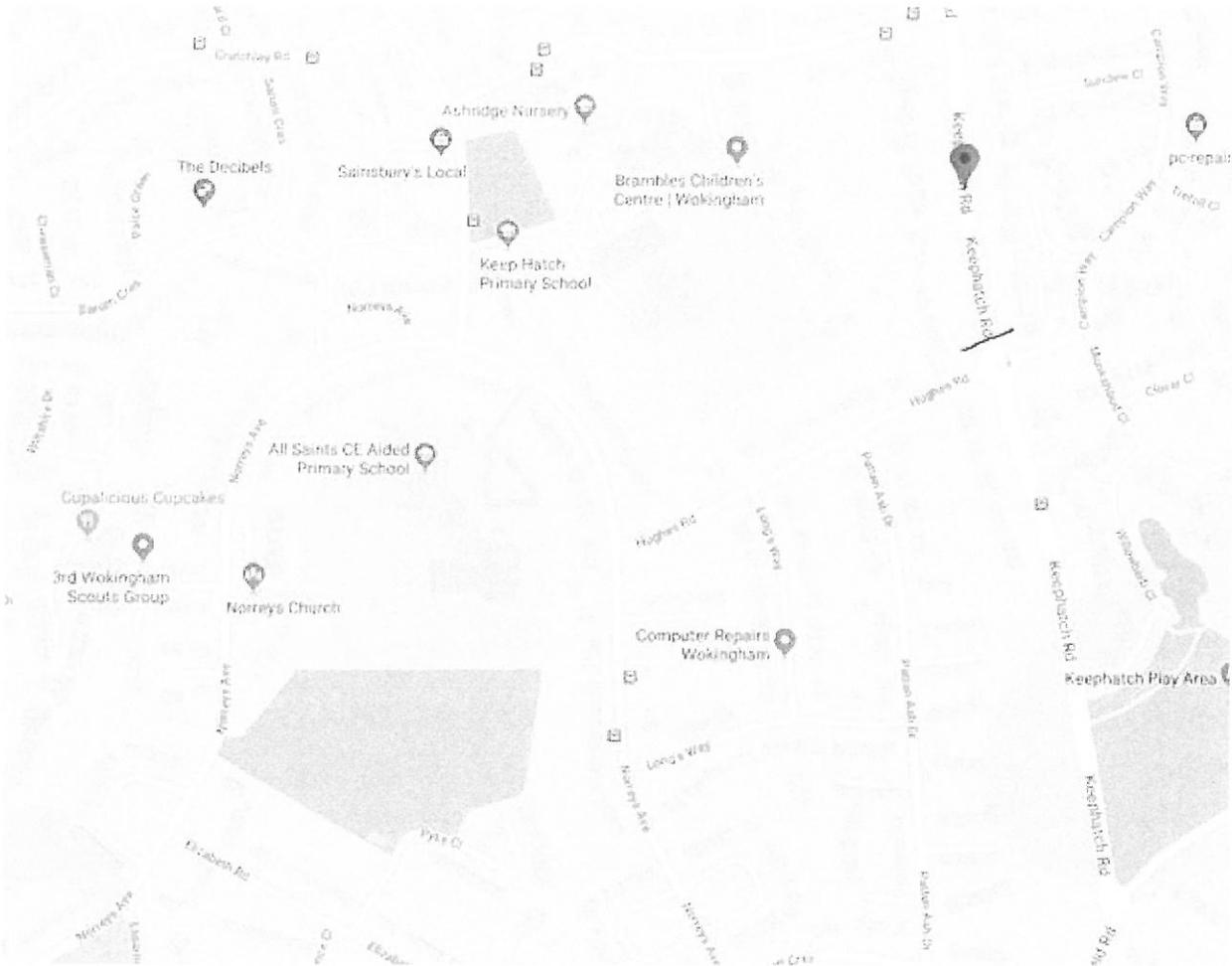


Site: Keephatch Road, Wokingham

Description of works: Zebra crossing installation

Is scheme works in same location as SCP: No – Existing location was not suitable for a controlled crossing

Location:



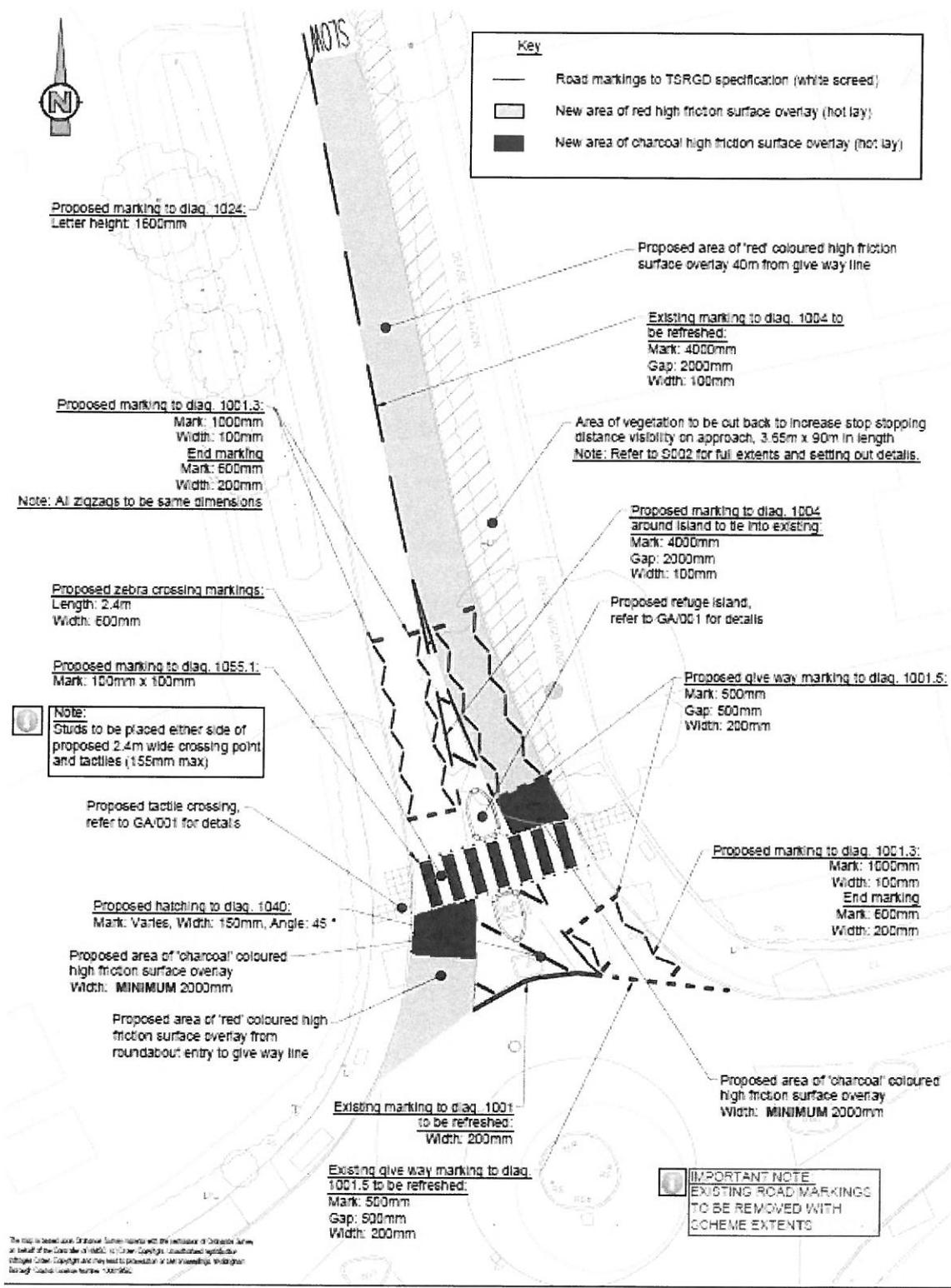
SCP site location:



Design location:



Scheme design:



Site: London Road, Twyford
 Description of works: Signal crossing installation
 Is scheme works in same location as SCP: Yes

Location:



SCP site location:

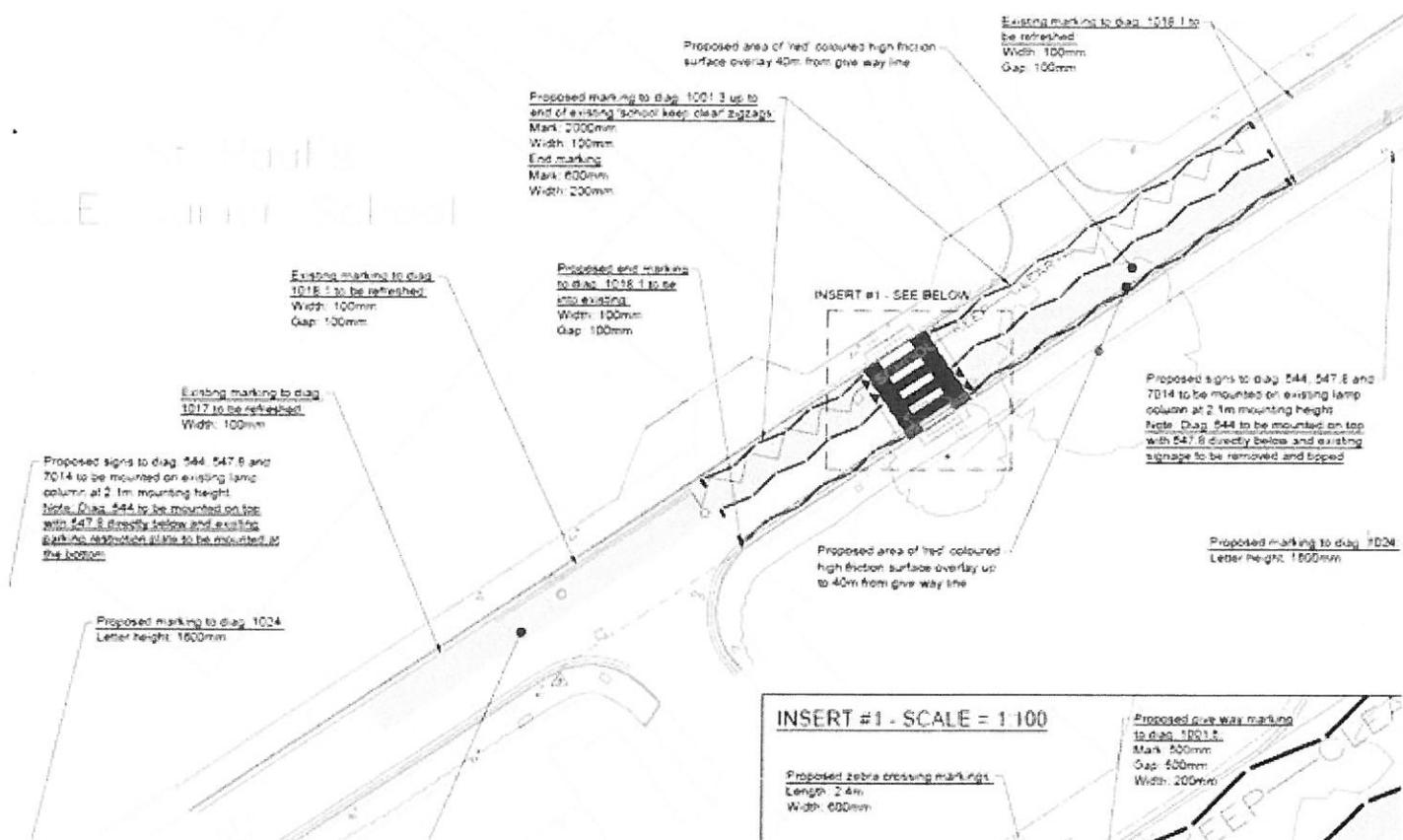


Scheme design:

SCP site location:



Scheme design:



Site: New Wokingham Road, Crowthorne

Description of works: Signal crossing installation

Is scheme works in same location as SCP: No – Existing location was not suitable for a controlled crossing at the entrance of two side roads

Location:



SCP site location:



Design location:



Scheme design:



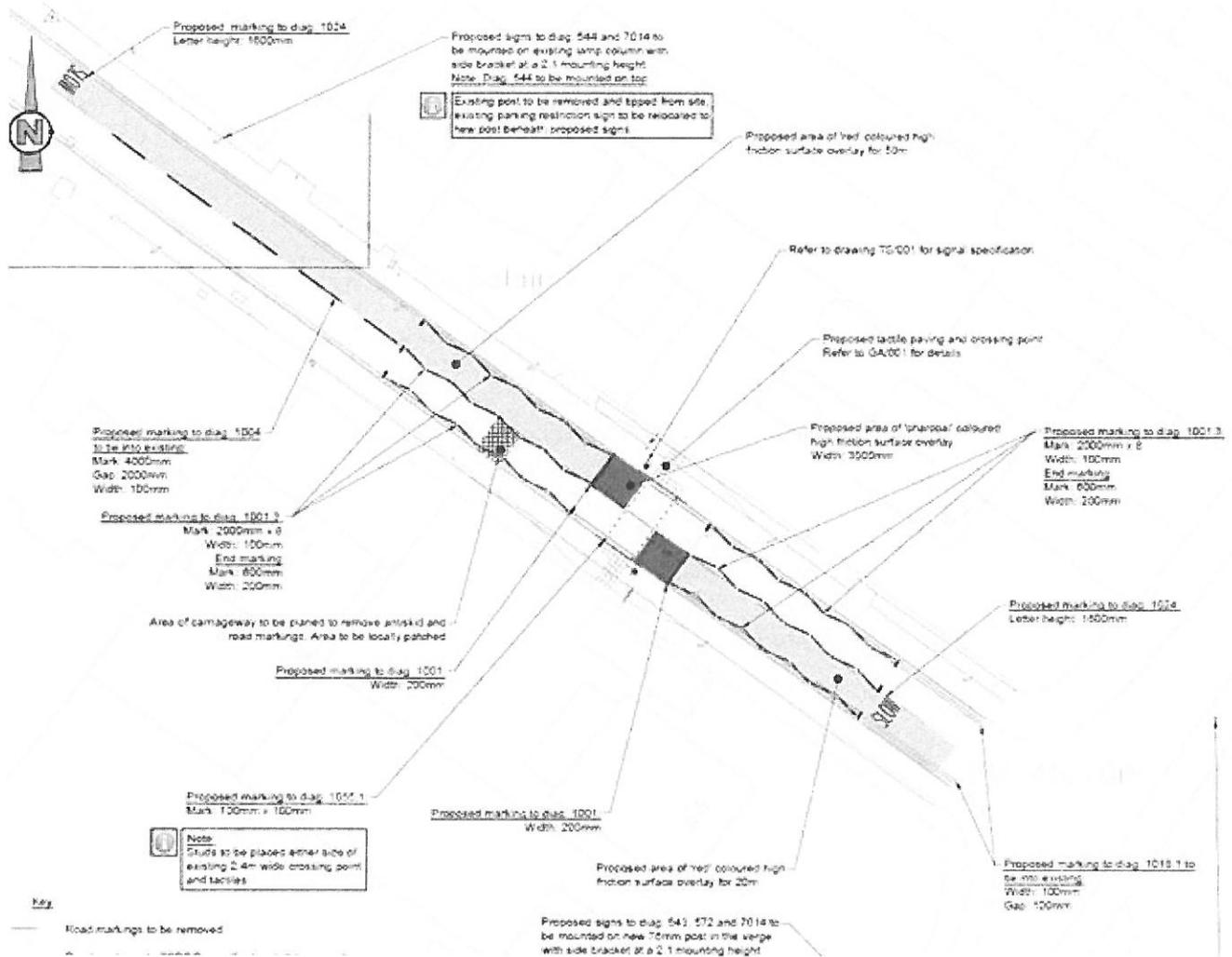
SCP site location:



Design location:



Scheme design:



Tim Allen
Associate

E | tim.allen@wokingham.gov.uk
T | 0118 9088309
M | 07500 122971

Wokingham Highways Alliance



One team delivering excellent services

Wokingham Borough Council
Highways & Transport, P.O. Box 153, Shute End,
Wokingham, Berkshire, RG40 1WL

E | wokinghamdirect@wokingham.gov.uk
T | 0118 9974 6000
W | www.wokingham.gov.uk

This page is intentionally left blank

Overview and Scrutiny Management Committee – 3 September 2018

Call-In of Executive Decision - School Crossing Patrol Service

School Consultation Comments

Comments relating to a pupils at All Saints Primary School
<p>The value of Mrs Burch on Murray Road is huge. Not only does she keep every family safe, help to manage frequent gridlock situations and give everybody a warm greeting to start their days, she also encourages safety awareness to the children before they cross, such as walking scooters across the road and stopping a good distance from the roadside to wait to cross. She is an excellent role model to the children teaching them safety and friendliness to others. At All Saints where my eldest is in year 6, the issue is more the speed at which cars often travel down Norreys Avenue, which would be dealt with by a permanent crossing. However, the concern would be that much needed parking spaces would be lost in the construction of a crossing, with parking already in short supply, causing regular gridlock (meaning people mounting the curbs causing danger to pedestrians) and people’s driveways being blocked etc.</p>
<p>Crossing attendants should not be reduced, especially as traffic has increased. If anything, another attendant is needed on Keephatch Road near Moore’s Green and Dowles Green.</p>
<p>The lollipop lady keeps both children and adults safe on a very busy road. Cars regularly exceed the speed limit and barely slow for the roundabout at the top of Hughes road. The lollipop lady also helps traffic flow as she judges when best to halt the traffic rather than crossings which often stop traffic after people have chanced it and already crossed, or take too long to change</p>
<p>Rather the money was spent elsewhere. Parents can cross the road.</p>
<p>A pelican crossing won’t stop speeding traffic right outside my children’s school, it’s already bad enough having to walk down rose street, towards Wiltshire road, this part is so dangerous as the traffic comes so close to the pavement, and sometimes mounts the pavement, especially the lorries. There should be bollards at this point, overall I would rather someone stopping the traffic, then having to worry about being run over with my children. Please don’t do this!</p>

Comments relating to a pupils at Keephatch Primary School

I have serious concerns about the proposal to remove the crossing patroller from Keephatch Road as I believe it will put children at risk on a busy road. The crossing is used by children travelling to Keep Hatch and All Saints schools and I am not convinced that a pedestrian crossing would be deemed appropriate on the roundabout. The fact that there are regular police speed checks on this road shows that there is already an awareness that vehicles drive along the road above the speed limit. When there is no crossing patroller present, cars and other vehicles speed across the roundabout. Families walking to school would definitely be ignored and I think that children walking alone to school would be placed at a much higher risk of an accident. Whilst it might be a useful addition to the junction, I don't believe that a zebra crossing would entirely mitigate this risk. This is a very difficult crossing with poor visibility in more than one place. I understand that this is a financially motivated decision but I would have thought that the increased council tax income from all of the building projects now sprawling across our once lovely town would enable the continuation of a much appreciated part of the community and a service that is valued by many children and families. Safety of our children is paramount and these proposals put that safety at risk.

If getting rid of Jo the lollipop lady then a traffic light pedestrian crossing needs to be put in where we cross along the Keephatch Road with jo as cars and lorries come so fast along that road a zebra crossing won't be efficient enough and they won't always stop at least with traffic lights they will have to stop and will be a way to slow the traffic down as without anything you will be putting a child's life in danger if nothing else is put in place and should be put in place before our lady goes.

If we lose the controller on Keep Hatch RD it's only a matter of time before there is an accident. Children from Keep Hatch, All Saints, St Crispins and Ashridge Nursery use this crossing.

I am very concerned about the withdrawal of the crossing lady on Keephatch road whilst the development at Mulberry Grove (Kentwood Farm) is happening - there are lots of construction lorries in the mornings. Also, previous traffic surveys has shown the volume of commuter traffic along Keephatch Road and Binfield Road is high - hence the distributor road plans - however, this road is not built yet

Do not understand why WBC wants to discourage walking to school! You have approved planning for thousands of houses without building any new road network, Keephatch road is constant flow of heavy haulage and "white van man" not residential traffic, manned crossing during school periods contains the risk - replacement with unmanned crossing is both high cost and high risk to the safety of pedestrians. You need to introduce 20mph limit and 7.5T weight limits if you take away the manned crossing.

The road is very busy and crossing further up by Dowles Green has made it harder to cross there. The crossing needs to be manned in my opinion for the safety of the many families and children that cross on their own

I do not agree with removing the lollipop crossing patrol on keep hatch road. This is a very busy and large road which is difficult to cross, there is a roundabout so has traffic coming from three directions, and they range from commuters to vans to tractors and of course large lorries going to and from building sites. This is a difficult to cross and dangerous main road, we need this lollipop lady so please do not remove her. I hate to think of people crossing with children without help and also what about the year 6 children who don't even have an adult with them - it is not safe for them to cross alone.

The lollipop lady on Keephatch Road provides a necessary valuable service to the safe keeping of our children. It is an extremely busy junction and drivers appear reluctant to stop even for the lollipop lady. I fear without her help, even with a permanent measure such as a zebra crossing, drivers would be less willing to stop. This is evident just further down the road by the zebra crossing at beanbag Road. Drivers often do not stop here. Therefore to remove the crossing lady at Keep Hatch would seriously impact on the safety of our children and make the possibility of a serious accident highly likely. Year 6 children are encouraged to walk to school by themselves and I believe this would not be possible with her to help the children across. She is a hugely valued member of the school community within children from St Crispins also making use of her services. I fear her loss would inevitably result in the fatality of a child.

I am very concerned about the withdrawal of the crossing lady on Keephatch road whilst the development at Mulberry Grove (Kentwood Farm) is happening - there are lots of construction lorries in the mornings. Also, previous traffic surveys has shown the volume of commuter traffic along Keephatch Road and Binfield Road is high - hence the distributor road plans - however, this road is not built yet

It's a terrible idea (!) and will affect us on a daily basis! The cars and vans all drive far too fast and it is a real danger to all our children! Am sure you can cut costs elsewhere!!!

Keephatch road has become a raceground - the Lollipop lady is essential for child safety at the Hughes Road roundabout. Furthermore, extra provision is needed at the junction with Moore's Green. Children are unable to walk safely to the Hughes road crossing as there is no footpath. The road narrowing has made the crossing more dangerous as cars speed up to avoid waiting and also it is impossible to see whether it is safe to cross when the vehicles are queuing. The blind corner from the roundabout on the new estate has caused several near misses with vehicles attempting to leave Dowles Green and it is only a matter of time before there is a serious accident there. A fixed crossing is needed at the Moore's Green junction in addition to retaining the School Crossing Patroller at the Hughes Road Roundabout.

We want the lollipop lady who really helps the children cross the road.

The road the lollipop lady helps us on is ridiculously busy. Cars speed. Lorries to the developments speed. Police intermittently put a camera trap along this road.

Taking the lollipop lady away would be an accident waiting to happen. I would like to invite you to come and walk that route with your own children and see what you think. I simply cannot fathom any sense of those thinking of making this decision!! A traffic control failure has been put in place further along the road but in my opinion this is in the wrong place. You need something on the other side of the roundabout. I would love to know what Wokingham council spend money on??? It's certainly not roads, road safety or recycling!!

Keep the lollipop lady on keep hatch road. It is an extremely busy road. Particularly due to all the lorries travelling to and from the new developments.

Keephatch Road is extremely busy Road at all times particularly school hours. The roundabout from Campion Road to Hughes Road is slightly set back making visibility to cross the road difficult at the best of times. Without a crossing lady and with the number of children crossing there to attend not just 2 primary school but also a Nursery for 3 year olds!! Many parents crossing the road have several children in tow. In addition several children who are Y6 and then pupils attending St Crispins cross the road would be unaccompanied. I feel strongly if you take away the crossing lady it is an accident waiting to happen!

The crossing at the roundabout of Keephatch and Hughes Roads is incredibly busy before and after school. Given the number of vehicles on the road at that time, putting in a crossing would not be sufficient. The traffic calming measure added further up Keephatch Rd has not prevented speeding vehicles apart from the point of the measure - people speed towards it and slow down only if required. The removal of the regular police enforcement vehicle has made the situation worse. Taking away the lollipop lady seems the wrong choice, will add to the general road disruption all road users are suffering, and surely is not cost effective.

I feel that children crossing the road at the roundabout will be put at risk if the lollipop lady is taken away. It is a very busy road and cars won't stop unless she is there to make them.

I object to getting rid of this service, I walk this way with my 2 children every day both to and from school and consider it vital for this dangerous road we have to cross to get to school. There is more and more traffic in Wokingham and on this road particularly many cars, lorries and vans driving at high speeds even on the approach to the roundabout. To keep our children safe it would be ludicrous to lose the service of lollipop men/women.

I think not having a lollypop lady on Keephatch road will be a massive mistake, it is such a busy fast road. Seen lots of near misses there. Please please keep her. Accidents will happen if you don't...

our lollipop lady is irreplaceable and should not be replaced with a zebra crossing

With all the building work going on with their trucks and white van drivers this road is becoming a liability. Removing this service will be an incident that will happen. I suggest you sit and monitor these roads for 2 weeks and you will see.

Keep Hatch Road is extremely busy now with all the new houses being build and the increased through traffic it has caused as a result! This is more reason than any to have a person ensuring children are crossing safely.

I think a move to a permanent crossing would be ok. It would need to be located right on the roundabout with Hughes Rd exactly where the current lollipop lady is because you know what people are like they take the shortest and quickest route. Due to the age of the children crossing there I also think it would need to be a pelican crossing with push buttons green men because a zebra crossing allows that hesitation factor for pedestrians and drivers. With a pelican crossing the decision is taken away from both sides making it much safer. If a pelican crossing with traffic lights was going in then I would support losing our lollipop lady if not then I'd want to see her remain. Personally I have lived round here so long that it will be a shame to see a smallish country lane turn into a major road with lots of road furniture but I guess that is progress with all the houses being built around, the pinch point further up, the new junction round by Warren House and Ashridge.

This is a busy road, especially with the increase in large vehicles for the housing and toad construction. It is really important we have crowding assistance, especially to help those year 6 children who traditionally walk on their own before starting secondary. I am sure it will cost children's lives if you move the crossing patrols.

It needs someone to help them to cross the cars and lorries go so fast down this road

Keep Hatch Road is too dangerous with the current levels of traffic on it for children OR adults to cross. There are so many heavy lorries going back and forth with yet another set of housing estates being build - Cantley area has a high volume of traffic to by-pass the 'closed' town centre roads. I also use the zebra crossing on the Binfield Road and the amount of times I have had to hold my child back before stepping out because yet another car has not stopped for us to safely cross - I have run out of fingers and toes to count on! The safety of the children and adults using this crossing goes to prove that we really need a crossing attendant for the Keep Hatch Road. It's not just used by children going to Keep Hatch either, it's used by older children going to St Crispins and we all have a duty of care to protect them as much as we can. That includes the local council as well as car drivers and parents. Please, please please do not remove the crossing attendants (including for All Saints as we sometimes walk along Norreys Avenue too).

The manned crossing on Keephatch Road is very important. Vehicles travel very fast along this road, often breaking the speed limit. With the development in the north east of the town, there has been a huge increase in lorries and other HGVs using Keephatch Road, and they also break into the speed limit. Replacing the roundabout at the junction with Hughes Road by four-way traffic lights would negatively affect the smooth flow of traffic. The crossing has to be at the junction, and that is not a suitable location for a Pelican crossing.

Children safety is paramount and I have experienced some reckless drivers at the roundabout myself. Most important thing to mention is that everyone will cross the street at the roundabout. Planning a zebra crossing or pelican crossing further down the road will not be used and will put children at risk. Any planned measure

should be considered alongside calming the traffic down at this point, as this really is a high speed road.

Crossing attendants should not be reduced, especially as traffic has increased. If anything, another attendant is needed on Keep Hatch Road near Moore's Green and Dowles Green.

Keep the school crossing patroller for Keep Hatch School because: 1. she helps year 5 and 6 children who are unaccompanied to cross a very busy road where drivers are inconsiderate & already even with a crossing patroller start to go again before children have crossed the road, 2. this is an extremely busy road and is used as a rat run even before the town centre road closures 3. children walking both with and without parents are often too busy chatting to fully observe the traffic 4. my own son walking with myself has stepped out onto the road to cross when traffic is coming on occasions & the patroller has pre-empted this 5. The school crossing patroller helps parents with children who scoot/walk/run ahead by pre-empting them coming up or down the hill by the roundabout and crosses over to ensure that children stop to cross 6. I would not be happy for my children to walk to school on their own crossing that road as often cars speed up when they see you crossing and I would not feel comfortable that traffic would stop for a pelican/zebra crossing as drivers are so intent on getting to their destination they speed along the road and also the round up and down the hill where children walk to cross is very blind for a driver to see what is coming. 6. already at the zebra crossing further down keep hatch road towards St Crispins cars often do not stop even when there are children 7. Already with a crossing patroller cars are trying to start on their journeys again before children are fully across the road and I feel that even a pelican crossing drivers would be going through amber lights as they are so impatient and with a physical school crossing patroller they are unable to do this. 8. The crossing patroller also helps to teach children about road safety especially when they are starting to walk on their own as they are busy talking or messing around & she ensures they cross safely & concentrate

Terrible that Wokingham Council think that little of children's safety.

Keep Hatch School: 1. she helps year 5/6 children who are unaccompanied to cross a very busy road where drivers are inconsiderate & even with a crossing patroller start to go again before children have crossed 2. this is an extremely busy road & is used as a rat run even before the town centre road closures 3. children walking both with & without parents are often too busy chatting to fully observe the traffic 4. my own son walking with me has stepped out onto the road to cross when traffic is coming & the patroller has pre-empted this 5. The patroller helps parents with children who scoot/walk/run ahead by pre-empting them coming up or down the hill by the roundabout & crosses over to ensure that children stop to cross 6. I would be unhappy for my children to walk to school on their own crossing that road as often cars speed up when they see you crossing & I would not feel comfortable that traffic would stop for a pelican/zebra crossing as drivers are so intent on getting to their destination they speed along the road and the lead up & down the hill where children walk to cross is very blind to drivers 6. already at the zebra crossing further down keep hatch road towards St Crispins cars often do not stop

even when there are children 7. Cars already try to start on their journeys again before children are fully across the road & even with a pelican crossing drivers would be going through amber lights as they are so impatient & with a physical school crossing patroller they are unable to do this. 8. IF the crossing isn't in the current location then it is unlikely children will walk further away from the roundabout to cross the road as it will take longer especially those walking on their own. 8. The crossing patroller also helps to teach children about road safely especially when they are starting to walk on their own as they are busy talking/using mobiles/messing around & she ensures they cross safely & concentrate

I strongly oppose the decision to remove the crossing lady from Keephatch Road. The roundabout located on Keephatch Road crossing from Campion Road to Hughes Road is slightly set back and depending on which side you cross over the road, it can be very dangerous due to lack of visibility until the last minute. There is a particularly high volume of traffic using Keephatch Road - which will only increase due to the additional housing developments being built. Children of all ages use this particular crossing point not just for Keephatch Primary School, but All Saints Primary School, Ashridge Nursery (children 3 years old!!!) and older children on the estate cross there to walk to St Crispins. It is an accident waiting to happen should you take away this facility, which quite frankly would generate a minimal saving for WBC!!!

The Keephatch Road has become a very busy/ fast road with all the new developments going in and some form of safe crossing needs to be provided on the roundabout as it is so very dangerous to get across with small children. Also I'm in total disbelief that the lollipop helpers are going as this will only result in more accidents!!!

The lollipop lady keeps both children and adults safe on a very busy road. Cars regularly exceed the speed limit and barely slow for the roundabout at the top of Hughes road. The lollipop lady also helps traffic flow as she judges when best to halt the traffic rather than crossings which often stop traffic after people have chanced it and already crossed, or take too long to change

I'm not supportive of the proposal. Drivers, building work lorries and Vans drive down that road and a speed far more than the speed limit. The children are at a higher risk of an accident without the human there to support them safely crossing the road. There are also a large number of secondary children that walk down that road and without looking probably cross the road.

Keephatch road is extremely dangerous with most drivers definitely not keeping to the speed limit (hence a temporary police speed camera nearby on a regular basis). The Lollipop lady gives many parents with their children peace of mind especially if they are walking by themselves. And the amount of huge lorries using this road with all your new housing in the area increases the risk of a terrible accident. The thought of my children or any of their friends getting seriously hurt or dying because you are saving 2 hrs a day of not paying someone is hardly worth someone's life.

<p>My daughter will start walking to school alone in a couple of months and I do not feel comfortable with her crossing the very busy road, which cars don't stick to their speed limit on. Jo the current traffic controller is essential in my eyes towards our children's safety.</p>
<p>So for both smooth transition of kids and cars, it's vital to have an assistant to help cross the road. An un-aided crossing would make an already overburdened road and route a lot worse, with more housing and more cars being put on these roads and in turn more children this could totally bloke these roads and cause danger at other junction points. Finally the social and neighbourhood element adds to a community and having a welcome site for one and all adds to the children's comfort and safety. Nothing is more frustrating than children continually crossing the road in an uncontrolled manner and playing about as they cross a busy road.</p>
<p>Keep the lollipop people</p>
<p>Would have to be replaced by a Zebra crossing. If you just take away the lollipop lady it's an accident waiting to happen.</p>
<p>The cars driving on the roads around this school do not stop at pedestrian crossings and this is evident in other locations around Wokingham. A crossing person is visible has presence, provides an extra set of eyes on the road safety of school children of all ages. Not only this but they are also an integral part of the community who kids trust in and confide in. Cost savings on road safety should not be placed above safety of our children. I am very against this idea</p>
<p>Disgusted that you are considering doing this... A child's safety should be your utmost priority ... Just for once!!! Do something right and leave it be...</p>
<p>KeepHatch road is busy in the morning, but also many times I have witnesses to cars not paying attention to children crossing. My daughter walks by herself and so the support of the lollipop lady is essential</p>
<p>The lollipop- safe crossing is a must to allow children to walk to school</p>
<p>Some cars travel very fast down Keephatch Road and there is always lots of children crossing at school times. Having someone at the crossing points gives parents peace of mind knowing there is someone keeping the children safe and teaching them rules of the road.</p>
<p>The lolly pop lady provides a safe place to cross and a familiar face. She often stops children who have nearly run out in the road, which a zebra crossing would not achieve. It is a very busy road with many big lorries, especially due to the amount of building work going on around us.</p>
<p>We love the lollipop lady and it would be a real shame if she wasn't there anymore</p>
<p>Far too much building traffic goes down the road and the lollipop prevents nasty child accidents</p>
<p>I am very very concerned how dangerous Keephatch road has become and how dangerous it will be to young children if the lollipop lady is no longer present. I've witnessed several near misses with the lollipop lady present in which she was able to keep the children safe from an accident. I'm also concerned as to how much more dangerous the road will become with the additional housing being built on Keephatch Gardens, Elridge Park, mulberry grove and Matthews green which all cause more traffic and more deliveries. I saw a near miss last week where a</p>

<p>delivery driver did not stop at the crossing. I have had to walk to school for the past 2 years with a buggy to collect my eldest and have found that cars don't ever stop for me on my own with my young children. It is a very dangerous crossing and I would really like you to consider keeping the lollipop lady for everyone's safety.</p>
<p>This is a big step backwards. Meanwhile our roads are more busy and with more houses being completed in the area there will be more traffic. This cutback is senseless! Children should be able to walk to school in safety.</p>
<p>Children's safety is the 1st priority before WBC spending more money on property and getting greedy from the developers</p>
<p>Keep our lollipop lady!!!!!!</p>
<p>A permanent crossing on Keephatch Road/Binfield Road would definitely be needed so many children use this road daily getting to school and it's quite dangerous</p>
<p>My daughters have been helped across the busy road by our lollipop lady for years. She has always gone beyond to help them understand the reasons for safe crossing. Although my daughters are leaving primary this year I know a lot of St Crispins children also benefit from the crossing and it is such a busy road. Teenagers are often even less likely to cross safely as they are talking or listening to music. It has also been good for parents to have the lollipop lady as a point of contact half way to school so we know they don't have to make the whole journey alone. So many benefits from one small and simple job.</p>
<p>It would be terrible for us to not have anybody helping with crossing at the incredibly busy Keephatch Road. So many big trucks going to the building sites at the end of Keephatch Road. People driving so fast here.</p>
<p>I am 'commenting on for other reasons' as described in Qu1. My children no longer attend Keephatch primary, but the school crossing patroller on Keephatch Road has been invaluable in providing a safe place to cross for children going to Keephatch and All Saints primaries. I believe that this road does need to be managed by an individual due to the increased volume of traffic on this road in the last couple of years, due to the new housing developments. I regularly drive along this road to work and have witnessed the increased traffic, including the large number of trucks heading to the building sites. Please re-think this proposal.</p>
<p>Keephatch road is very busy with traffic, with a number of vehicles driving over the speed limit. Currently the crossing patroller can anticipate vehicles that may not stop in time and can prevent children stepping into the road at the wrong moment. An unmanned crossing does not give the same level of safety. Vehicles can jump the lights or leave it too late to break. As a parent I talk to my children about not just relying on the 'green man' safe to cross light and checking both ways but it can be tricky for children to judge. Children in years 5 and 6 want to be independent walking to school, but on a busy road without a crossing patroller it is an accident waiting to happen. Please keep the crossing patrollers, they do an amazing and extremely important job keeping our future safe.</p>
<p>I am deeply concerned about talk of removing the current lollipop lady. There are lorries thundering down Keep Hatch road in the mornings, plus of constant stream of traffic reluctant to stop. So many children cross the road, including those going</p>

<p>to All Saints and St Crispins as well as Keep Hatch. You would need to have safety barriers and a traffic light crossing right at the roundabout if there was no-one there to man it, but even then it is dangerous. Putting in a crossing away from the roundabout would just tempt children to cross at the roundabout again. Having a lollipop lady is seriously important to the safety of our children</p>
<p>It would be terrible if you took the lollipop lady away from the Keep Hatch roundabout. The traffic is awful in the mornings and someone will get hurt sooner or later</p>
<p>The flow of small children in the morning, sometimes running ahead of parents, would make road crossing very unsafe. Children of primary school age are not tall enough to be seen from larger vehicles.</p>
<p>Utterly ridiculous and completely unsafe for children</p>
<p>The school crossing lady plays a vital role in protecting all the children who have to cross Keep Hatch Road. There is a slight bend in the road approaching the crossing which children would find difficult to judge without the lollipop lady to guide them.</p>
<p>Keep Hatch road can be very busy. Having a human manned crossing really helps with aiding the children to cross safely. On a number of occasions I have seen cars exceeding the speed limit along this road. Having the assistance of a crossing patrol really helps manage the flow of traffic</p>
<p>A 24 hour road crossing would be safer</p>
<p>The lollipop lady is essential. The speed cars and lorries drive down Keep Hatch Road is ridiculous and very dangerous. The road cannot cope with the size of the lorries.</p>
<p>I would like my son to walk to school when he is older and a safe crossing will be part of this decision. I consider a school crossing patrol to be the safest way of delivering this across the busy Binfield Road which is a 30 mile per hour road with many lorries and trucks travelling. This survey by Direct Line highlights a large number of motorists that do not know the Highway Code in relation to zebra crossings which concerns me. Please read the article link https://www.petrolprices.com/news/how-to-use-a-zebra-crossing.</p>
<p>Keep Jo the lollipop lady</p>
<p>Much prefer the current 'lollipop lady' arrangement. It works very well and the children are safe. Should this not be possible in the future then it is very important that a pelican/zebra crossing is installed at the junction of Keep Hatch Road and Champion Way. Keep Hatch Road beyond Champion Way (the Cantley side) is a blind corner for those crossing Keep Hatch Road from Champion Way. A pelican/zebra crossing provides some safety although less than the current arrangement.</p>
<p>you can't get rid of Jo she being there helping both my daughters over the years one daughter is in year 9 and still helps my other daughter cross the roundabout to make sure she is safe crossing , If Jo was to go I would be more worried about her walking to school as the traffic is speeding , in the pass even on a zebra crossing at Beanoak I and my kids have been nearly run over</p>

<p>We need more lolly pop ladies not less. Too many cars on the road and driving too quickly so not particularly safe for young kids crossing</p>
<p>Without the crossing lady, the sheer amount of traffic now in the road with the new builds in the area, would be uncontrollable. They are essential to child safety.</p>
<p>Please don't leave us with nothing to help us cross as it is a very busy road during the school run. In addition some drive very fast and it is very dangerous.</p>
<p>It would be irresponsible to remove the Safe School Crossing on Keep Hatch Road and to not replace it with at least a zebra crossing. There is currently no crossing point along the road despite the length and considerable use of the road. Every few months a police van is stationed along the road to measure vehicle speed so there is an obvious issue with speeding along the road, evidenced by the new traffic calming measure installed near Matthews Green on the route. Do not put child safety at risk for the sake of saving a few pounds!</p>
<p>I feel it is critical to keep the lollipop lady in place on KeepHatch road. Safety will be compromised if this service is replaced by a pelican/zebra crossing, as cars do not always stop at such crossings, even though they should.</p>
<p>Removing school crossing will endanger lives. It is as simple as that.</p>
<p>I am disgusted that a relatively low cost service is even being considered for removal. The school crossing safety worker is not only an important safety measure in an area which has already seen hundreds of new homes already built, (with more to come), generating more traffic, but is also "part" of my child's experience going to school. I would appreciate the council making public how much cost could be saved implementing the proposed changes as I struggle to believe that paying the wages of such safety persons are more than the cost implementing a pelican or zebra crossing. With additional homes planned and in planning around Keephatch, the road between Keephatch and the school is now busier than ever with more lorries, traffic and children using the crossing. The option should NOT be one or the other - safety shouldn't come at cost. If an additional crossing is needed then this should be in addition to the existing service. I left Wokingham in 2011 after living here for 6 years. I returned in July 2016. Increased council tax, less services, busier roads, a town that can't be used, more houses. Wokingham IS NOT what it used to be and compared with Binfield and Bracknell, the quality of life in Wokingham is in decline. With more homes your budget should be increasing. I welcome the opportunity to have my views on this site, but also wonder why such a small box is used for comments. Keep up leaves, keep the streets clean, finish the town centre, stop cutting low cost services like a lolly pop lady and start thinking about how you could make money from additional recreational services instead of cutting back</p>
<p>School crossing patrollers provide a vital service in keeping our communities, children and roads safe. The council would be extremely short sighted in removing these. The road restrictions that have been put in place on Keephatch Road are not an improvement but more of a danger to those using the road and inserting another permanent road restriction will only endanger those walking to school more. The school crossing patroller also gives parents a peace of mind when they have children walking to school on their own. Ours knows our parents and children</p>

<p>and is kind and caring and looks out for them. This is an additional safeguard for our children</p>
<p>It is madness that WBC are considering removing the road crossing patroller at Keephatch Road. The volume of traffic is too heavy and too fast for parents and children to cross the road from the Keephatch estate safely. Drivers are rushing to get to work and barely have the patience to wait for the current patroller, Jo Robinson, to enable parents and children to cross - how will this happen without Jo. Also when Jo does stop the traffic for crossing, a queue quickly builds demonstrating how many vehicles use Keephatch Road as a rat run - this is just being exacerbated by the new housing estates. Given that there have been many near misses over the years, this is a serious accident waiting to happen - do WBC want 'blood on their hands'?!</p>
<p>My son will walk to school on his own from September - a school crossing patroller is invaluable in safe road crossings</p>
<p>It would be a sad day indeed if the remaining 'lollypop' People were to be dismissed. They provide an invaluable service to our community particularly with regard to helping alleviate traffic congestion.</p>
<p>This service should 100% not be removed as the road is so dangerous and jo does an amazing job of protecting the hundreds of children everyday!</p>
<p>Less safe environment for our children to walk to school, can't imagine any permanent crossing, such as pelican etc. will happen in the near future</p>
<p>The lady who manages the school crossing does an excellent job managing a very busy crossing point and directing traffic during congestion. Replacing her with a zebra crossing or traffic lights on that busy road would endanger the families and children attempting to cross.</p>
<p>The current crossing is extremely dangerous. Children left in their own to cross the Keep Hatch Road from the Keep Hatch development are crossing right next to a roundabout, with cars approaching from 3 ways. I currently walk my children to school and many a time cars have not stopped to allow us to cross without the lollipop lady having to stop the flow of traffic. My eldest will be walking to school on her own in a few months and she will be affected by this should you replace the crossing controller with a crossing. Nearly 500 cars pass through this crossing during the school run and with the new housing developments happening at Mulberry a Grove the traffic is becoming heavier with not only cars but lorries and heavy goods vehicles. This road is also a well known rat run for those who wish to bypass other roads when traffic is heavy. I urge you to reconsider this notion and think about the safety of the children especially when there is a huge drive to get children active and walking to and from school.</p>
<p>Yes I feel that the proposal would be harmful to local children of Keep Hatch, the current controller does a superb job of calming traffic and ensure a safe crossing place for children and their families the removal of the controller would be detrimental to the safety of the children.</p>
<p>Keephatch Road is extremely busy, especially with all the lorries thundering past with the new houses being built further down Keephatch Road</p>

I believe removing crossing patrollers and replacing them with zebra crossings will REDUCE safety for children without significantly increasing safety 24x7 for all. Removing them is a bad idea.

It is such a dangerous crossing if there is no lollipop lady. It is rare for cars to stop if she's not there.

The presence of the crossing lady on Keephatch Road does cause cars to slow down when they approach the point of crossing. A zebra crossing or a light controlled crossing may not have the same effect.

The volume of traffic on Keep Hatch Road will continue to significantly increase over the next few years as traffic from the Warren House development use the road to access London road and then the 329M. Relying on local drivers and non-local lorry drivers to be considerate and aware of children walking to school unaccompanied is a high risk. Unfortunately many drivers are inconsiderate to other road users, never mind pedestrians. I have witnessed parental and crossing patrol personnel stopping and warning speeding drivers on this road.

I have a real concern that you will actually be endangering lives by removing the Keephatch Road crossing patroller. It is an extremely busy road, especially at the times of day when children are crossing it. Please rethink for the sake of our children's safety.

The School crossing patroller provides a sense of safety and security for the many young children and parents crossing Keep Hatch Road which a static zebra crossing could never replace. She is highly visible and frankly many road users seem to be unaware of the rules for a zebra crossing, as shown by the recent Direct Line study here <https://www.petroprices.com/news/how-to-use-a-zebra-crossing>. I fully support installing a zebra crossing given the increasing number of vehicles, including some very large lorries due to the extensive construction at Mulberry Grove. However this should not be as an alternative to the School Crossing Patroller who provides a different yet complementary service. For a relatively small annual cost, please let's not compromise on the safety of our children.

Short-sighted to remove this valuable asset

Comments relating to pupils from Polehampton Infant School

I believe that school crossing patrol officers do a vital job in keeping children safe and teaching good habits. I am strongly in favour of them remaining.

This is a big step backwards. Meanwhile our roads are more busy and with more houses being completed in the area there will be more traffic. This cutback is senseless! Children should be able to walk to school in safety.

A pelican crossing would be better than a zebra crossing as many cars approaching the area don't even see the lollipop lady that's currently there. It is a very dangerous stretch of road I worry about my older who walk to the secondary school on their own.

Do not change. The road is quite unsafe with a great deal of fast moving vehicles. The lollipop person changes this effectively and controls this dangerous spot. Changing this would be a grave mistake

A permanent safe crossing point outside Polehampton Infants on London Road, would be preferable to the Lolly Pop service currently in place. Please could you also investigate a permanent safe crossing point outside of Polehampton Junior school at Loddon Hall car park. This is a very busy and dangerous crossing, with blind spots, especially as the junior school age children are frequently crossing this point without an adult.

Twyford Parish Council object to the withdrawal of the school crossing patroller role.

Drivers are consistently more aware of lollipop men/women than of a fixed crossing, so I feel retaining the use of a lollipop man/woman would be beneficial.

I support pelican crossing with lights outside Polehampton Infants. I think the road is too fast for a zebra crossing and that motorists take more notice of lights.

A zebra crossing is definitely needed.

Retaining the Lollypop person would be better than a fixed crossing. Sometimes the traffic has to be forced to stop.

Although it is a reasonable idea to put in place a pelican crossing that would apply 24 hours, we feel that a lollipop lady is safer. The infant school children are too young to judge the traffic. The lollipop lady also gives a lovely tradition feel to the school and community which I feel is nice as it reminds me of my childhood. We understand the reasoning behind a pelican crossing but it is a shame.

Comments relating to pupils from St Paul's Junior School

The roads are too congested and without a crossing person it becomes chaos

The traffic around Walter/St Paul's is awful. Without the crossing lady there would be some serious accidents and child's lives put at risk

Child safety is the most important thing. If you want to cut back money, stop spending thousands of pounds on public notices in local newspapers each week

The roads are too congested and without a crossing person it becomes chaos

With three different school facilities on Murray Road and an extremely busy and often congested route, it is essential that we can retain our well liked and efficient crossing patroller

Given the dangerous driving and inconsiderate parking by some people by these schools, a patrol is definitely needed to protect everyone who walks to the school.

It will be more dangerous - the lollipop lady regularly negotiates conflicts - and there is no need for a formal crossing at other times.

The idea of removing this patroller, on a busy rat-run road is crazy. You need to perform a needs analysis and see that road. I suggest a busy Friday, rainy afternoon at 3.30pm when the road outside is often gridlocked. Then you'll see how important this role is in ensuring child safety. This decision will also increase traffic as parents, will be unable to let their child safety walk to school.

I would not feel safe at all letting kids cross the Walter Road without a crossing patrol - volume of traffic would make it totally unsafe.

School crossing patroller is vital to safety outside the school. Very dangerous and busy road which is constantly getting gridlocked.

■■■■ the school crossing patrol is doing such a fantastic job. She always ensures that parents and children cross the road in the safest way possible and she would challenge anybody or any cars compromising the safety of others. The roads near Walter Infant and St Paul's CoE junior schools are dangerous because of heavy traffic and drivers being in a rush. Also, Oxford Road has narrow pavements which always causes concern for the children's safety, and some very local children walk to school on their own. I have witnessed few near misses around the school area so I would feel very concerned if the crossing patrol was not there anymore.
Envoyé de mon iPhone

Murray road is extremely busy, therefore it would be putting children's lives at risk if the school crossing patroller was taken away.

It is absolute madness to remove School Crossing Patrollers when our children's safety is paramount.

Puts our children at risk by removing - Meadow Road is a rat run

Mrs ■■■■ prevents accidents - removes Lorries that have stopped in dangerous position - she is an asset the school SHOULD NOT LOSE !

It won't be safe as there are many undisciplined motorist speeding, rushing and not giving way to children with parents waiting to cross.

<p>The benefit to the school community and parents is immeasurable. It is a false economy to stop such services. The traffic in Murray Road is horrendous and adult supervision desperately required by retaining the school crossing patroller. Cars are often mindless towards the children which occupy the narrow pavements and start and end of school. I am disappointed in the council leadership to consider any reduction causing danger to children against the small cost.</p>
<p>The roads around Wokingham are busy and chaotic. I think it's outrageous that children's safety is put at risk for the minimal cost savings getting rid of no doubt minimum wage crossing patrollers would achieve. Maybe take a look at savings to be made higher up the ladder....</p>
<p>I think it is a big mistake to remove school crossing patrollers. I have seen the crossing patrolled outside of St Paul's junior school prevent many accidents and she is always on the look out to keep the pupils safe.</p>
<p>We not only need the patroller to give safe crossing but also to insure traffic flows smoothly</p>
<p>Our schools need our crossing patroller. The roads around Walter and St. Paul's are so busy with traffic being near the town and station the patroller does an amazing job of directing the flow of traffic, and stopping inconsiderate drivers from parking and endangering the lives of children. She keeps not only the younger children safe but also secondary school girls from the Holt or children from Emmbrook safe who also use the crossing especially when they are often distracted by mobile phones. Having pelican crossing would be a nightmare on Murray road during the peak times as will constantly being pressed whereas the patroller is able to recognise when traffic needs to flow. She is essential to our children's safety.</p>
<p>This would put hundreds of children's lives at risk. Our lollipop lady ensures our children's safety and even helps with the congestion. She always makes people move from parking on double yellow lines and disabled parking. I have children with special needs and one of my children went to run into the round and the lolly pop lady stopped him from doing so. We cannot get our children to school safely without her. Sometimes even with her it's extremely dangerous. Without her I'm positive there would be casualties.</p>
<p>The crossing patroller needs to stay, she does an amazing job & makes crossing 1000 times safer for the children</p>
<p>Replacing lollipop lady with traffic light will cause even more chaos in the already congested Murray road. That means more reckless drivers and I fear for the kids' safety.</p>
<p>Yes, I don't want the patroller to be removed as Murray Road is extremely busy with cars and pedestrians and narrow pathways. Many need to cross to walk in all directions to homes, childcare providers or where cars have been safely parked a way away. Removing this controller would vastly increase the potential for accidents and problems as there can be much aggressive driving here as parked cars mean it has enough room for only a single car. The patroller directs the traffic and keeps us all safe.</p>
<p>Do not remove the current service</p>

Removing the patroller at this school would be a great loss to the children, school and community. Not only does she ensure children and adults can cross safely she also diverts drivers trying to park on double yellow lines and ensures Murray Road doesn't come to a halt. The zebra crossing that is already in place is often driven across with people waiting to cross therefore would be unsuitable for children to use safely without the patroller there.

Our crossing patroller keeps [REDACTED] safe and regulates traffic at the same time. No one messes with her. Changing the kids routine will lead to some accidents as they will not be sure what to do for a while

I believe it's outrageous the Council try to save money which children safety. Installing bumps/traffic lights like in Emmbrook is going to cost money as well

It feels unsafe without the crossing patroller as there are drivers not following the rules.

Murray Road is used as a rat race by cars every morning and evening. They block the road, ride on pavements and a few incidents have happened over the last few years. Children will be at an increased risk of being run over on at non patrolled crossing near our school. This is really a bad decision. You cannot remove patrollers near schools where the traffic is so intense and not consider how to reduce the traffic caused by people who just use Murray Road as a short cut to avoid the station area and drive inconsiderately despite seeing all the children and adults walking on the pavements at school time. The situation gets so tense that over the past few years, we have had quite a few visits by the police at times where the patrollers are there in order to give them support. The patroller near our school does a fantastic job at keeping our children safe and we don't feel an unmanned zebra crossing will keep our children as safe as the patroller does.

Our crossing patroller is essential to help families navigate Murray Road at peak times. She is highly regarded by the school community and neighbouring houses. Her efforts to prevent reckless driving and potential accidents undoubtedly keeps our pupils safe. I have personally witnessed many occasions where her intervention has been essential. The proximity of our schools to the train station means that local roads are extremely busy, particularly at the start and the end of the school day, and parking issues further exacerbate this problem. Our controller directs traffic daily, preventing the roads around our schools from grinding to a halt. A pelican or zebra crossing would be utterly ineffective at these times and in these situations.

Our crossing lady keeps everyone safe by reinforcing parking and road rules in addition to crossing the children safely. Please reconsider this proposal.

The School Crossing Patroller is the significant contributor to help parents and children remain safe while reaching school as fast as they can. It also ensures that commuters remain compliant and less stressed while using this road. If School Crossing Patrollers are removed then this will significantly increase the risk for children and might create conflicts with commuters.

These crossing people are of immense importance

Disgusting. Lots of traffic come this way and isn't any safe place for children to cross !!!

Road is too busy to not have a lollipop lady
Really like it
Removing the crossing supervisor would reduce safety in the area around the school
Terrible and dangerous idea to remove the crossing patrol person on Murray Rd! Not only does she keep everyone safe, but also monitors the traffic and moves people on. For parking/waiting dangerously!!!
To encourage children to walk to school, we should have more crossing controllers.
My son does not need to cross the road - however, the crossing patroller does more than just help. People cross. She co-ordinates the traffic and ensures safe parking around the schools. She really is indispensable and I would have grave safety concerns if the area was not patrolled by a person at the key times of day.
Please do not take away our crossing patrol, we need it due to an extremely busy road, keep us safe
The school crossing lady at St. Paul's is invaluable - she keeps the traffic moving, watches out for the children, ensures their safety and stops drivers parking where they feel like. I really do believe it wouldn't be long before a child is hurt trying to cross the road without a crossing patroller - it's a busy stretch of road, parents drop their children where they like, regardless of road markings and everyone is in a rush. Some services need paying for and this is one of them.
We need a controller as she also manages the traffic which is often gridlocked and cars often drive on the pavement which is very unsafe for the children walking to school
We need the crossing patrol as the Murray road is too narrow and in her absence the traffic becomes chaotic, which is extremely dangerous for kids and also drivers.
A child's safety and welfare is paramount. Around St Paul's, roads are used to access the train station and town. On numerous occasions I have witnessed inconsiderate drivers, who are speeding and seem to have no awareness of young children crossing the road. The area is congested with parked cars on the road and it's very tricky to cross when the flow of traffic is busy. I do drive my son to school but it's such a relief and peace of mind to know we have a designated School crossing patroller. Without her the pupils' safety would be at risk and the traffic would probably come to halt as she helps to direct the traffic.
We want the lollipop lady.
The school crossing controller should NOT be removed - it is a very dangerous road!! Drivers speed down the road and if the controller is removed it is a major accident just waiting to happen!!
I believe the school crossing controller at St Paul's does much more than ensure the children cross the road safely. She also directs traffic and prevents traffic gridlock and frayed tempers at the busiest times. I also believe that having a crossing patroller is much safer than an unmanned fixed crossing. Pelican crossings are dangerous and in the traffic chaos of Murray Rd it would be easy to miss a child stood at the crossing which could lead to a terrible accident. The

zebra crossing in Oxford Rd has its lights jumped regularly and I believe the same thing would happen in Murray Rd. I absolutely believe that if the school crossing patrol is removed there will be a serious accident on Murray Road and the traffic gridlock will be even worse.

Murray Rd is a nightmare at school start/end times. There are two factors affecting this: Insufficient local parking and belligerent drivers - a problem exacerbated by the new Wokingham road lay outs which now make Murray Road a main thoroughfare for all road users, instead of a back road. Regularly now, there are aggressive confrontations and the traffic becoming gridlocked by drivers not giving way to right of way traffic. The parking means only one way flow of traffic at school day beginning and end, with drivers not being courteous and respecting this. And with a queue of cars meeting in a deadlock, cars often unexpectedly charge down the road if they see the other queue backing up. It's not safe for children, and without someone on patrol it seems a child-related accident will become inevitable. The current school crossing patrol lady not only manages a safe school crossing, but directs the traffic as well. If you remove the school crossing patrol lady, you need to look at installing traffic lights on this section of road to manage the traffic. Or to direct non-school traffic to the Caroline Drive option as a thorough fare to bypass the school and reduce the current road rage. Installing traffic lights might help to achieve this. You could also look at increasing parking areas around the school. And finally, parents and staff are not legally enabled to manage traffic. Drivers get very frustrated and angry. We need someone with a semblance of traffic authority to manage the situation.

The crossing patrol is a vital part of school pick up and drop off! This area is very busy during school drop off/pick up and each person values the crossing patrol. Not only do the infants and juniors benefit but also holt and Emmbrook schools. She does a great job at traffic management too.

Murray Road is a dangerous, busy road to cross at school pick up and drop off times. Some parents do not park sensibly or supervise their children which means the human pair of provided by the school crossing patroller are vital. St Paul's and Walter Infant School only have one exit point each this means huge numbers come out at the same time. The path is very narrow. I have often seen the patroller using her stick to ensure the large numbers do not spill out onto the road.

Murray Road is a significant cut through and is exceedingly busy during the rush hour. Cars routinely drive on the pavements due to lack of giving way to oncoming traffic and gridlock occurs at least once a week. The safety of the children would absolutely be affected if the crossing patrol lady was to be removed.

I think the lack of a school will make the walk to school unsafe. Currently the school patroller ensures the safe flow of traffic which is difficult due to the high volume of traffic trying to go down Murray road. The area is very difficult both before and after school and needs ADDITIONAL safety measures.

I don't believe that safe crossings will be sufficient for this crossing due to the risks associated with the very narrow path outside St Paul's. This crossing patroller does an excellent job of keeping people moving and more importantly the children

safe. I also observe her making sure that people do not park in a dangerous way next to the crossing.

Murray road is extremely busy every morning and the removal of Mrs Burch would lead to a very dangerous situation; commuters and parents regularly have disagreements and the bottleneck beside the school is extremely dangerous - without a patroller I fear a serious accident would occur quite quickly. I live opposite the school and use the traffic lights pedestrian crossing and even here we often have drivers miss the red light and go through; distracted either by their commute or run to school. Removal of the controller on Murray road would be a disaster.

This is silly we need the help to cross the road Murray Road gets so so busy because of reckless drivers I have seen loads of near misses when there hasn't been the crossing lady that. We need her there will be a child killed if she goes

Having a school crossing patroller at St Pauls really helps us to use Year 6 as a transition period for our daughter to get used to travelling independently in readiness for secondary school. We know from all the times we accompanied our children to and from that school when they were younger, that the school closing patroller not only directs traffic but also coaches children in staying alert and being safe around traffic. It's very busy at the site of 2 schools and a nursery at the start and end of the day, and I feel families need more than a zebra to keep them safe.

Please retain the school crossing patroller. As you may know there are 3 schools that are being monitored by a single patroller and I think the signal crossing may not be effective in this case particularly given the vicinity to the train station, it gets very busy!

I am disgusted that Wokingham council would even contemplate this action which will endanger young children.

Please do not remove the lollipop lady on Murray Road, outside Walter Infant and St Paul's. Her presence keeps our children safe. The footpath outside the schools are very narrow and with hundreds of children and parents walking along this road and on this footpath, it would be incredibly easy for children to spill onto the road, without a lollipop lady. There is no way a zebra crossing will keep the children as safe as the presence of a lollipop lady/traffic controller. Please do not remove this valuable and important service for the sake of all the children at Walter Infant and St Paul's.

I believe it is essential to the safety of the children and parents to have a "manned" controller rather than another set of lights. The traffic around the school is really heavy at pick up and drop off times and our current school crossing patrol lady has been invaluable in preventing accidents. I would fear for the safety of the children if she was not there.

Our crossing patrolled does an amazing job ensuring the safety of over 300 children aged three to eleven years as well as younger siblings. She regularly advises drivers who are parking inconsiderately to relieve congestion on an increasingly busy road and has also discussed road safety with both of my children in their nursery and infant years.

Murray Road is dangerous. Removing the Patroller will place all children attending St Paul's and Walter at grave risk.
Our School crossing patrol lady is highly valued and needed to keep our children safe. We would very much like to keep her!
I consider the proposals to remove patrollers to be dangerous and unnecessary. The Walter/St Pauls patroller is extremely valuable and does far more than any static crossing could.
It's a traffic hotspot. Drivers often mount the curb or refuse to give way. Mrs Birch frequently acts as a mediator and defuses these situations. We would've had fatalities without her.
The school crossing patroller is vital in this area when controlling the traffic and protecting the children whilst crossing. A pelican/zebra crossing would NOT provide the same level of protection for the children during peak times. I suspect we would see more frequent traffic gridlock also.
I think this would be a mistake. The crossing patroller on Murray Road does so much more than just help the families cross safely and wish them a good day. It would of course be sad not to have her cheery smile in the morning however Mrs Burch does so much more than this and this is the reason for my objection to this proposal. Mrs Burch ensures the traffic continues to flow safely along the road and to lose this key figure would be a complete mistake. Any days without her there in the mornings are utter traffic chaos and grid lock! Having lived on the road for the last 14 years, the road is mostly very quiet of cars and pedestrians and only busy at these peak times so a zebra crossing just wouldn't be necessary. Traffic lights would stop the traffic when it's already potentially grid locked and just make the situation worse. There are entrances to 2 school and a nursery opposite the entrance to a nursing home, Mrs Burch ensures the traffic can move freely down the road at busy times and so prevents these entrances being obstructed, something vital for the nursing home should an ambulance be required. We need Mrs Burch on Murray Road to continue to do the fantastic job she has been doing for so many years. It would be a complete mistake to have her removed from post.
The school crossing patroller provides a much greater benefit to the local community than just enabling the safe crossing of Murray Road. That benefit far outweighs the limited savings that would be made through the abolition of this service. The patroller balances the needs of both pedestrians and drivers by collecting pedestrians and crossing them in groups. With a zebra crossing pedestrians at the busy times would simply stream across the road offering no break to vehicles. This could lead to a great deal of additional frustration on the part of drivers and yet more reckless and inconsiderate driving. A pelican crossing would not take account of the situation on the road and could create more problem than it solves. At busy times Murray Road is effectively just one lane and the school crossing patroller often takes it upon herself to direct the traffic so that everyone can keep moving. She is also very active at calling out people who have parked illegally or inconsiderately. As a local resident who regularly walks up and down Murray Road at different times of the day and week, I would suggest that there would be no real benefit in having a permanent crossing. The road is simply

not busy enough at other times that it is unsafe to cross. Apart from school drop-off and pick-up times there is very light traffic and there is good visibility along the road. For anyone travelling to and from town or the station there is already a pelican crossing outside the schools on Oxford Road which provides an alternative route. In conclusion, I am strongly against the removal of the school crossing patroller for the reason that the benefits to the local community of keeping her far outweigh the costs.

I think the safety of my 3 children and all the children of the school will be compromised. It is a very busy through road, and the parking is shocking. The controller manages the traffic and the safe entrance of my children to school. If she wasn't there, my children would not be safe.

Taken the crossing lady away puts children at risk

I do not want the crossing patroller to be replaced by a pelican/zebra crossing. Murray road is a very busy road and the crossing patroller provides very safe environment for my child to cross the road by directing traffic and preventing irresponsible drivers from taking dangerous means to force their way down the road at the beginning and end of the school day.

In the mornings the road is very busy with local work traffic and school traffic. The traffic concertinas up and the school crossing patroller does an amazing job in keeping it moving safely. With a static crossing this could not happen. There are lots of children crossing the roads and cars parked if they had to wait for an electronic patrol I fear there would definitely be more accidents and road rage which is already bad along that road. I don't believe it is necessary to have a crossing there 24 hours a day as there is little traffic or pedestrians at other time.

I strongly disagree with the proposals. Murray Road is an extremely busy and congested road, especially during school open/close. There are constant issues of road rage and backed up traffic where cars cannot move due to parked cars/congestion. This is very dangerous for children and the Patroller on many occasions helps free up backed up traffic and congestion by talking them through moving. There are two schools and a nursery that use this site so there are so many children who would be put at risk by getting rid of the patroller. The patroller also asks people to move who have parked on the school yellow lines also ensuring safety of children to enter the schools without obstruction. The current traffic light crossing is on Oxford Road, not Murray Road. For children that are on the other side of Oxford Road, coming towards the school from the station, this crossing is of no use so they would have to cross Murray Road anyway. This would be very dangerous with no patrol working as people would be tempted to cross anywhere. Having a patrol person ensures most people cross in safety at the same place. By installing a zebra crossing here, there would be a constant stream of people crossing and traffic would be more snarled up than it is now encouraging more risks by motorists. The patrol worker ensures that people wait for a short time, letting traffic pass and then once there is a small group, cross together. This enables the constant flow of vehicles yet keeps the pedestrians safe as they are not crossing all over Murray Road and it ensures traffic has stopped for them which it would not do necessarily if they were crossing wherever they felt like. If a

traffic light system was installed here, there would be no space for people to wait on the pavement as they volume of pedestrians is so great with siblings and pushchairs too. The patrol worker ensures a perfect balance between numbers crossing and the free movement of traffic, which technology could not respond to in the same way.

Lots of morning congestion around Meadow/Walter/St Pauls and crossing patroller help keep children safe

I am concerned that the removal of the patrol officer will lead to an increase in dangerous driving and inconsiderate parking. The area where the crossing patroller works get really clogged and we will see more cars going up on pavements which is really dangerous with children walking on those pavements. Cars do this because they can get trapped on single lane and row of cars in both directions unable to move backwards or forwards. This happens when there is no one managing who needs to go and who to stay put. The crossing patrol does a lot more than stopping cars to let children cross- she manages the traffic too and tells people to move when they are parking on the yellow zigzags or other equally stupid things that people do when they are in a rush. The school run brings out the worse in some drivers unfortunately and it will be so much worse without someone there prioritising the safety of the children.

During the School Hours traffic comes to stand still on Murray Road and drivers get very aggressive on the road due to one side of the road is being blocked by parents parking their cars to drop off children at Walter Infants & St Paul's, at the moment crossing patrol is the only safety we have for children/parent who walk to school. Myself do not feel safe crossing the road during the school hour, it will be very scary for children and council putting lives at risk with the proposal to remove crossing patrol.

Murray road is extremely busy and dangerous at drop off and pick up time. In the mornings with the proximity to the station, the mix of commuters and school traffic the congestion can be extreme and the school crossing patroller not only gets the children across the road safely, but helps to keep the flow of the traffic moving. There have been incidences of people being hit by cars already and I dread to think what may happens the traffic and children are left to their own devices on the road.

Patrol Officer should stay: Murray road is incredibly busy and traffic often grid locked and arguments between drivers! Human patrol officer as opposed to pelican crossing helps to alleviate this issue as timed and thoughtful intervention and often also ensuring parking and general road safety around the entrance is adhered to.

Please do not get rid of the crossing support as this provides a vital service to the children of St. Pauls in a very congested area. Traffic would ground to a halt without support from the crossing lady in the morning and I would not be happy for my child to walk across the road on their own when they go into Year six because cars come at you from everywhere! Crazy idea to remove this service!

<p>it will make me nervous for the safety of the children if he patroller has gone, the road Gets lots of cars in a very small area. also the workers are valued by parents and children</p>
<p>Murray Road does not have an automated crossing and is very busy. Removing the School Crossing Patrol for Walter\St Pauls is not safe for children and parents trying to get to the school</p>
<p>The crossing patroller Diane 'Mrs Lollipop' to the children is much loved and so integral to our safety. As a teacher and Mum, I appreciate how important it is for children to have a safe place to cross the road.</p>
<p>It's an absolute disgrace to remove the crossing patrollers - these are cut backs that should never be made!</p>
<p>People drive down the road with no consideration for the children trying to cross. When there is no school patrol they don't stop to allow us to cross so she is vital for safety.</p>
<p>The roads around the school (Oxford & Murray) are extremely busy, more so with recent changes to traffic circulation imposed by the council. We made a complaint to the police last year as cars were not stopping at a red light on the crossing on Oxford Road. If they are running red lights, they will certainly not stop on a zebra crossing!</p>
<p>We think the school crossing patroller does a great job and is very much needed by the school. The road is very very busy and the standard of driving is very poor at peak times. We have seen many issues resolved by the patroller. She is needed. A crossing without her would not be sufficient.</p>
<p>School crossing controller is critical to safety in a very busy area where two schools are based and there is a lot of traffic not always related to the schools</p>
<p>I feel that you are endangering our children by removing the crossing patrollers</p>
<p>Removing the lollipop lady will be extremely unsafe for the children of St. Paul's and Walter, with commuters and parents dropping off/collecting children there are too many cars going through Murray road and the lollipop lady is the only one who manages to control this traffic, All the children know her, listen to her and know she keeps them safe, removing her will be an accident waiting to happen.</p>
<p>Really advise keeping the lollipop lady for traffic control</p>
<p>Murray Road is very congested in the morning and some drivers park inappropriately. The school crossing patroller not only helps the local community cross the Murray Road safely, but also works with local police when necessary to prevent accidents. She feeds back to both schools and nursery on Murray Road so they can send parent communication out. All of this cannot be replaced with a traffic light.</p>
<p>Totally disagree on taking Patrol lady off. There is no sufficient parking and on top of it , if the patrol is also taken away, whole thing is going to get into a nightmare</p>
<p>I have been walking my children to school for over 5 years, and I can say confidently that the crossing controller outside Walter/St. Paul's saves injury on a regular basis, in a way that an automated crossing just couldn't do. Let me explain. Murray Road is a complex beast at school rush hour. There are 3 schools (including Meadow Nursery), narrow roads, hundreds of parents, children and</p>

dogs on pavements that are too narrow for the number of people and push chairs, cars zipping in and out of parking spaces, other cars using it as a cut through and residents using their driveways. Sometimes it's the people that get jammed, such that you can't get onto the pavement over the crossing because of the number of bodies in the way. The crossing controller manages this by preventing anyone else from crossing the road and keeping the cars waiting until it is safe. Sometimes it's the cars that get jammed (usually due to inconsiderate, impatient drivers), such that the road gridlocks. The crossing controller manages this by holding the pedestrians at bay and directing the car drivers individually in order to clear the situation. I have seen many incidents that, without human anticipation and reaction, would have endangered children's safety. I feel very strongly that a machine could not do this job, and that a human crossing controller is critical to keeping parents and children safe and managing traffic flow at busy school times. A pelican crossing wouldn't work because the cars would never be able to move. Traffic lights wouldn't work because the pavements are too narrow to hold the weight of people. Please, please, please do not remove the human element from this crossing and introduce extra risk to the children's journeys to school.

Traffic lights will not be safe for such a busy road where cars are always reversing and bumping up curbs and double parking outside the school. how can a year three child walk across that road safely

We NEED to keep our crossing patroller. For the safety of our children. Mrs Burch does an excellent job of controlling the traffic.

I think it would be a huge mistake to remove the schools crossing patroller on Murray Road. Apart from crossing over children from two schools safely, twice a day, I have seen her manage traffic on numerous occasions on what is a very busy, congested and chaotic road. The situation on this road is worse than it has ever been as commuters use it as a cut through now that the new one way system is in place in central Wokingham. Without the patroller, older children will lose their independence as less parents will feel inclined to let their children walk to school alone. You will almost certainly risk an accident at some point due to the busy nature/visibility on the road, including the rage that some people have from waiting in the backed up traffic. Surely there are other ways to cut spend that does not put local children's safety at risk.

Murray Road is incredibly busy at school drop off and pick up times. Cars are constantly parking and leaving and there is no safe place to cross. This road is also used heavily by commuters who are avoiding the busy and congested Reading road. The crossing patroller does a fantastic job of ensuring the children all cross the road in safety. I feel sure that unless she is kept, or a suitable alternative permanent crossing installed, there would be children and parents crossing at multiple points along the road, adding to the traffic chaos, and this would surely result in accidents and the children's safety being jeopardised.

Disappointed that the consultation doesn't encourage as many people as possible to submit comments. There is an assumption that only parents of existing children at these schools will be the ones you would expect to be interested. Would future parents have a view? What about parents whose children have recently or not so

recently been at the school? What about drivers who use this road and welcome the guidance they are given. Having a person in the middle of the road is a powerful deterrent . Will the police be asked for their views? School crossing patrollers provide an external link between the school, the children and who takes them to school. I would like to know exactly what is being suggested, when the report will be produced and how a decision will be made.

I strongly oppose getting rid of the school crossing patrols. Your weasely vague use of 'where appropriate' with respect to alternate crossing facilities makes it apparent that you have few solid intentions to do anything other than stopping funding the patrols. How can you consult in such vague terms - with absolutely no guarantees on what you will do to protect the safety of children in the borough? If you're going to replace the crossing patrols with something else specific - tell us what that is. Surely you've done some sort of road safety audit or risk assessment on this proposal that has led you to a view of options beyond a "maybe we will, maybe we won't, we're not sure, we might tell you what we'll do later". This is no way to statutorily consult on public sector proposals. On a narrower point - I was under the impression that good practice in government consultations suggested 12 weeks as an acceptable consultation period; a minimum even. Is this how long your consultation is running? I'm proud to pay my tax and this proposal is shaming. I have seen a great deal of poor traffic light (and other road traffic) compliance around St Paul's/Walter Schools - people running red lights and nearly mowing down children. The fact that my daughter has been hit by a vehicle at a nearby crossing, by a driver running a red light, causes me to feel intense unease at your proposal. I will actively and vehemently oppose it. I understand the budget pressures you're under but I simply do not believe you are at this point - other things must be cut first: sell your car park(s) for development, do fortnightly bin collections, your parking charges are ludicrously low - raise them, introduce a workplace parking levy.

I would urge a member of the council to visit ST Paul's School at 8.40am and 3.35pm on a rainy day and witness the traffic. I have been walking my children to school at St Paul's for over 10 years and I have witnessed the congestion and road rage become increasingly worse during that time. It is not a rare occurrence to see a motorist speed angrily towards the crossing. Due to parked cars it is not always easy to see the crossing and whether people are waiting. I don't think a pelican crossing would help. I genuinely believe if the fantastic lollipop lady were to no longer be at the crossing regularly it will only be a matter of time before a child or pedestrian is seriously injured or killed.

School Crossing Patrollers take every care to make sure kids cross safely especially when there is increased car traffic at that time.

A fixed crossing will serve no purpose as the road is not dangerous outside of the school runs. During the school runs, the road is congested with traffic using the road as a rat run and navigating parked cars. There are parents, around 650 children, plus siblings, buggies, dogs and scooters all traveling to the two large schools and nursery at the same time. Mrs Burch provides a safe crossing and

<p>commands respect from irate drivers who she calms and often has to direct traffic and eases congestion. Murray Rd would be gridlocked and dangerous without her.</p>
<p>I feel the children would be unsafe with our lolly pop lady there have been so many occasions she has stepped in when children have tried to cross without an adult</p>
<p>I think our school crossing patroller must stay. She does a marvellous job of helping us to cross Murray Road at peak times. Traffic there are extremely busy especially at the start and the end of a school day.</p>
<p>I feel that a crossing patroller in this area is a necessity rather than a luxury. The traffic has become unbelievably busy in recent years and the crossing patroller ensures that children are kept safe when crossing.</p>
<p>It would be difficult to replace the existing Lollipop Lady with traffic lights as she does so much more than just stopping the traffic to enable the children to cross. Murray Road gets so congested in the morning and afternoon school drop off and collection times. She helps to keep the traffic moving, keep it parking in a safe place (not on double yellow lines or in front of the school) and can respond to traffic flows proactively. A zebra crossing would be very dangerous for the children as cars may not stop as the road is so busy. Traffic lights would not be able to respond to the actual conditions on the road and are likely to make traffic worse, or lead to a dangerous build up of children and adults on the school side of the road while waiting for a red light to be able to cross, blocking the path for other pedestrians and increasing risk that people walk on the road or are pushed onto the road. I currently let me Year 6 walk on her own, as I know that the crossing is safe. She is meant to be taking her younger brothers in the Spring term to get them used to walking alone, however, if there is not an attended crossing then I will not let them walk on their own at all, as I consider the risk of accident to be too high. This would be a shame as it gets the children used to walking a short distance on their own, builds up their independence in preparation for secondary school.</p>
<p>Road around the school is extremely busy with cars often backing up creating a danger for the children walking to school. The removal of the crossing support raises concerns for me</p>
<p>The road outside the school is busy and dangerous. I will only let my kids cross with the patrol. There is non-school commuter traffic which zips down there plus there are often angry and frustrated drivers. It's no road for children to cross. It's dangerous. Please don't let an accident or fatality be the reason for bringing it back if you axe this vital service for our children.</p>
<p>The volume of traffic going past Walter/St Paul's is very high and without the school crossing lady it would be very dangerous crossing the road. I</p>
<p>My son will be walking next year on his own and I would not feel happy without this. It will also cause more traffic concerns right outside the school which is already very busy and dangerous.</p>
<p>I think the idea of getting rid of the crossing patrollers is a bad one, as the traffic down Murray Road is crazy during school pick up and drop off. I have seen impatient drivers speeding off with a complete disregard for the swarms of children and families getting to and from school. Mrs Burch does a fantastic job laying</p>

<p>down the law and controlling the traffic, she teaches the kids to wait until its safe and does so with a lovely smile on her face. To get rid of her would be a huge mistake.</p>
<p>Murray road is very busy and cross patroller doing great job if she will be removed kids will be in danger</p>
<p>The driving and parking situation on Murray Road is dangerous. Having a school crossing patroller facilitates safe crossing on this nightmare road. Removing the patroller will make the road even more unsafe for children.</p>
<p>Be in no doubt, if you take away the school crossing lady for this school it will result in an accident and a child being seriously injured. So many times the current school crossing person for this school has sorted out the cars and traffic to avoid serious incident.</p>
<p>The crossing lady does not just make sure children cross the road safely, she provides a whole traffic management system, keeping the flow of traffic moving in a really tricky spot. When she is not there, children, led by their parents, cross wherever they like. Also cars force their way onto the traffic coming up from Meadow Road.</p>
<p>Yes I think that removing the school patroller will endanger children's lives, as the traffic problems on Murray Road are awful at school drop off. Without the patroller controlling not only the crossing but traffic as well I feel this will only get worse.</p>
<p>We would be lost without our crossing patrol, cars regularly pull up on zig zags or up on the curb, and our patroller keeps them all under control, it would be so dangerous if this was to removed and replaced with permanent lights</p>
<p>The Crossing controlled by human has more advantages than traffic lights. I can't imagine traffic/ how many car accidents will occur without our Lollipop Lady</p>
<p>I strongly feel that lolly pop lady should be there as both me and my daughter feel safer while crossing the busy road in front of the school.</p>
<p>The School crossing patrol is vital outside St. Paul's as the roads are residential & very busy. Drivers park on yellow lines & block the view of the road. I would not let my daughter walk on her own if there was no patrol there meaning I could not work at a school as I do now.</p>
<p>Rely on our lollipop lady. Her assistance is invaluable</p>
<p>The road is so busy and would put children in danger without a patrol</p>
<p>I am not happy about removal of the crossing Patrol near St. Pauls Junior and Walter Infant</p>
<p>The Crossing Patroller at Murray Road not only helps the pupils cross safely, but she also directs the traffic ensuring the traffic keeps moving as parking is difficult in that area and can easily grind to a halt.</p>
<p>It would be very difficult to lose our crossing patroller as she is a great help outside school</p>
<p>This is kind of putting risk of younger kinds, future of this Nation, one small incident can impact a lot on children mental health. Please consider this.</p>
<p>I work at St. Paul's and walk to and from school. I can see the difference that Mrs Birch makes, not only to the safety of the children and adults crossing the road, but</p>

<p>also uses her experience and judgement to help the flow of traffic. Traffic lights cannot do this. She is very popular and highly regarded.</p>
<p>I feel it is irresponsible to remove the safe crossing option on such a busy and extremely dangerous road. There have been collisions with people on the pavements before in this area around school pick up and drop off. This proposal is totally unacceptable.</p>
<p>Our crossing patroller has saved the lives of children and parents and managed a number of situations so well that pedestrians have avoided injury. The vehicles using Murray Road do not always stop or slow down, even when pedestrians are part way across the road. There are 3 schools on the site and the majority of the 500 families that belong to the schools are crossed back and forth by Mrs Burch. She provides a VITAL service to all of our schools.</p>
<p>We need to keep our school crossing patrol lady. She is invaluable and plays a huge part in keeping our children safe.</p>
<p>We really can't do without the patrol crossing, it keeps everyone safe as cars speed through this cut through. Also lots of unsafe parking that is kept under control.</p>
<p>We need people to monitor the road! It is dangerous. It would be ludicrous to remove the lollipop lady.</p>
<p>We really need the school patroller. She does an amazing job of keeping our children safe on such a busy road.</p>
<p>It would be so dangerous on such a busy road to not have a crossing lady. There are 3 schools on this site with around 900 children needing to get into the school safely. With all the cars parks and the cars speeding to try and get passed the parked cars before someone comes the other way and causes gridlock it would be so dangerous for the children trying to cross unsupervised. The crossing lady does an amazing job moving the traffic on and stopping cars parking on the zigzags too.</p>
<p>I think the crossing patrollers do a wonderful job keeping our children safe, and cannot be replaced by a formal crossing. The crossing patrol ladies do a wonderful job in not only teaching our children about road safety, but also act as a visible deterrent to speeding cars, or those who might not otherwise stop for traffic lights or a zebra crossing. Removing them will cost lives.</p>
<p>If you implement zebra crossing, then people will be crossing continuously there will be even more traffic jam than now.</p>
<p>With our school's close proximity to the train station the roads are incredibly busy around the school. Our crossing patroller is invaluable and I fear that replacing her with a Zebra crossing, or another type of crossing, will lead to drivers taking less care when passing the school and ignoring the crossing.</p>
<p>Would be extremely dangerous to have no crossing patrol person outside our school due to volume of traffic and the need for frequent intervention due to nature of parked cars and flow of traffic.</p>
<p>Yes. Our crossing patroller does so much more than help the children cross the road. She helps us all to navigate the mayhem of Murray Road every morning and afternoon. Since the rerouting of the road system around the railway crossing, Meadow Road and Murray Road have become thoroughfares for commuters trying</p>

to bypass the traffic build-up at the crossing. The high volume of cars past the school can lead to congestion, which our crossing controller has become adept at managing. She regularly directs traffic to keep vehicles moving, while also keeping watch to prevent cars parking on the yellow zigzags which would further exacerbate the problem. Her efforts to prevent reckless driving and potential accidents – and therefore keep our children safe – is invaluable.

Please do not get rid of the school lollipop lady at Walter / St. Paul's schools. The road is very dangerous and she does amazing job of ensuring the children don't get run over.

The school crossing patroller in Murray Road provides an essential service for pupils and their families trying to cross Murray Road at the start and end of the school day, when the traffic is extremely busy (due to commuters, school traffic and Wokingham station users). Not only does the patroller ensure the road can be crossed safely, but she also regularly has to direct traffic when the road becomes gridlocked, and she prevents drivers from parking illegally near the schools, and prevents impatient drivers from driving over the crossing when pedestrians are waiting to cross. These latter 3 actions cannot be undertaken by an automated crossing system, and they provide an invaluable service for the local community as well as ensuring the safety of the school children and their families.

I would like to lodge my objection to the removal of the School Crossing Patroller for St Pauls Junior School and Walter Infant School. Murray Road is a particularly busy road during the school run and "rush hour" periods, not only due to the presence of two schools, but also due to the increase in commuter traffic, using Meadow Road and Murray Road to bypass the station area gyratory traffic system and level crossing. Our current School Crossing Patroller does a fantastic job of controlling traffic flow and preventing potential accidents; an unmanned crossing would not provide the same level of safety.

It is paramount to keep the school crossing patrol for St Paul's, Walter infant and meadow nursery for the safety of all. Mrs Burch does a sterling job at managing the traffic to ensure all can cross safely. This is further complicated by pushchairs, bicycles, scooters, dogs, etc. Without her, things would be much more dangerous and she adds real value to the overall safety of the area.

It is very hard for me to hold 3 children's hands at once to cross. I just hope and pray no child gets hurt if we lose the lollypop lady

Short-sighted to remove this valuable asset

As a teacher at St Paul's C of E Junior who is on the gate at the end of every day, I can see how important it is that the children have the Safe School Crossing. There are a large number of people and a large number of cars combined with many children who are having their first experience of walking home alone. We need to make it as safe as possible for all around the school premises.

Yes, I work at St Paul's Junior School and cycle to work every day. Our school crossing lady not only ensures the safety of school children, siblings and parents when crossing the road but she also advises cars when they are parked inappropriately and illegally. The role is more than just ensuring the safety of people crossing the road and this role cannot be replaced by a zebra/pelican

crossing. Mrs Burch ensures that the busy road remains running, preventing gridlock and at times road rage. She is a member of our school community and in an age where many things are automated, to have a person who always has a smile and wishes you a good morning or good afternoon is a valuable learning tool for our pupils and sets a wonderful example. I urge you to take on board the comments from our school community and rethink your proposal.

I am the school crossing patroller at Murray Road (St. Paul's Junior, Walter Infant and Meadow Nursery schools with approx. 800 children). My job role has evolved since I started in Sept 2010. Although I realise I am there to stop traffic so parents and children can cross the road safely - that does not mean I will step out into the road as soon as someone wishes to cross. As it can 'bottle neck' I am mindful of how long I hold up traffic to enable pedestrians to cross. Being so close to the station, it is used as a 'rat run' to avoid the level crossing and at the start and end of school is incredibly busy, especially in the mornings. I use my intelligence regarding the volume of traffic and whether allowing cars to go first might eliminate a potential jam. (Yes, I spend a lot of my time counting cars and calling across to users; "Won't keep you long!") Drivers travelling up the road (and have cars parked their side) do not always give way to those travelling down the road and this will always impact on whatever crossing is there. I'll ask parents who park on the double yellow lines to move (if I am able to) so that Blue Badge holders are able to be there (currently 3-4 parents/grandparents). During this 6 weeks consultation period, from a point of safety have shouted at one girl who was so engrossed on her phone that she didn't check I was still holding up traffic when she started to cross, as well as stopping a 2/3 year old from running across the road (not holding his mum's hand). Having a patroller on this particular site is probably more efficient than a crossing is likely to be. The suggestion that 'other road users could cross safely with a permanent crossing' is highly debatable: during school times it is incredibly busy but at other times, it is almost deserted. There are a couple of vulnerable girls from The Holt who also cross with me and Year 6 pupils will start doing their journeys without parents shortly in readiness for Senior School.

Comments relating to pupils from Walter Infant School

Murray Road is absurdly busy and chaotic at school times with badly parked cars and other vehicles using it as a rat run to avoid the level crossing. There needs to be a safe place for these very young children to cross.

The roads are too congested and without a crossing person it becomes chaos

The traffic around Walter/St Paul's is awful. Without the crossing lady there would be some serious accidents and child's lives put at risk

Child safety is the most important thing. If you want to cut back money, stop spending thousands of pounds on public notices in local newspapers each week

Please do not get rid of the crossing patroller. She is excellent at her job and makes the road safer for us all

Please keep our lollipop lady, she's so helpful at helping the children cross a busy road, and helps to slow the cars down.

The roads are too congested and without a crossing person it becomes chaos

The school crossing is a valuable and essential safety role to enable us to walk our children to school. We would take the car if she were not there.

With three different school facilities on Murray Road and an extremely busy and often congested route, it is essential that we can retain our well liked and efficient crossing patroller

Given the dangerous driving and inconsiderate parking by some people by these schools, a patrol is definitely needed to protect everyone who walks to the school.

The road outside Walter/ St Pauls school is exceptionally dangerous and hazardous due to the size of the amount of cars and vehicles that use the road as a cut through in the morning. I have observed cars driving on the pavement and on a daily basis i have witnessed vehicles inches away from hitting small children. I believe that if the crossing scheme is removed from outside St Pauls/ Walter Infants it will be only a matter of a time before a child is seriously if not fatally hurt and the Council will be whole heartily responsible for this. At the moment the crossing scheme is the only management of a dangerous level of traffic around both schools which with the numerous future housing schemes being completed this will only become a greater problem. I believe that greater and further provisions for safe school crossings should be given to these schools, not to reduce and remove the only resource of safety for our children to get into school.

I would not feel safe at all letting kids cross the Walter Road without a crossing patrol - volume of traffic would make it totally unsafe.

School crossing patroller is vital to safety outside the school. Very dangerous and busy road which is constantly getting gridlocked.

Diane the school crossing patrol is doing such a fantastic job. She always ensures that parents and children cross the road in the safest way possible and she would

challenge anybody or any cars compromising the safety of others. The roads near Walter Infant and St Paul's CoE junior schools are dangerous because of heavy traffic and drivers being in a rush. Also, Oxford Road has narrow pavements which always causes concern for the children's safety, and some very local children walk to school on their own. I have witnessed few near misses around the school area so I would feel very concerned if the crossing patrol was not there anymore.

Envoyé de mon iPhone

Think it's a silly idea the school patroller does an excellent job in making sure it's safe to and from school I think if removed would cause more problems for the worse

It is absolute madness to remove School Crossing Patrollers when our children's safety is paramount.

Without our patrol crossing officer we wouldn't feel safe crossing the road with our little ones

It would not be safe for us to cross on such a busy time ... our lollipop lady is essential in helping the children be safe

It won't be safe as there are many undisciplined motorists speeding, rushing and not giving way to children with parents waiting to cross.

The benefit to the school community and parents is immeasurable. It is a false economy to stop such services. The traffic in Murray Road is horrendous and adult supervision desperately required by retaining the school crossing patroller. Cars are often mindless towards the children which occupy the narrow pavements and start and end of school. I am disappointed in the council leadership to consider any reduction causing danger to children against the small cost.

Our schools need our crossing patroller. The roads around Walter and St. Paul's are so busy with traffic being near the town and station the patroller does an amazing job of directing the flow of traffic, and stopping inconsiderate drivers from parking and endangering the lives of children. She keeps not only the younger children safe but also secondary school girls from the holt or children from Emmbrook safe who also use the crossing especially when they are often distracted by mobile phones. Having pelican crossing would be a nightmare on Murray road during the peak times as will constantly being pressed whereas the patroller is able to recognise when traffic needs to flow. She is essential to our children's safety.

This would put hundreds of children's lives at risk. Our lollipop lady ensures our children's safety and even helps with the congestion. She always makes people move from parking on double yellow lines and disabled parking. I have children with special needs and one of my children went to run into the round and the lollipop lady stopped him from doing so. We cannot get our children to school safely without her. Sometimes even with her it's extremely dangerous. Without her I'm positive there would be casualties.

The crossing patroller needs to stay, she does an amazing job & makes crossing 1000 times safer for the children

<p>Replacing lollipop lady with traffic light will cause even more chaos in the already congested Murray road. That means more reckless drivers and I fear for the kids' safety.</p>
<p>Removing the patroller at this school would be a great loss to the children, school and community. Not only does she ensure children and adults can cross safely she also diverts drivers trying to park on double yellow lines and ensures Murray Road doesn't come to a halt. The zebra crossing that is already in place is often driven across with people waiting to cross therefore would be unsuitable for children to use safely without the patroller there.</p>
<p>I am fine with removing school crossing patrollers if permanent crossings are put in place. Parents can easily handle this.</p>
<p>We really need to keep the patroller - it is a very stressed road system and she is very valuable</p>
<p>Without the lollypop lady the road would be a lot unsafe as the traffic would get worse as it will back up on roads leading to and from school</p>
<p>I believe it's outrageous the Council try to save money which children safety. Installing bumps/traffic lights like in Emmbrook is going to cost money as well</p>
<p>It feels unsafe without the crossing patroller as there are drivers not following the rules.</p>
<p>Have you have seen Murray Road in the morning? It's mostly a one lane road due to parked cars and Mrs Burch acts as a Traffic controller. Without her it would be chaos and also highly dangerous for the children. People are always in a hurry and stressed by the traffic bottleneck there. You do not need a 24 hour crossing, you need a crossing patroller at the beginning and end of the school day. The children learn when it is safe to cross and drivers have to stop. It would be highly irresponsible for you to remove this valuable service.</p>
<p>Our crossing patroller is essential to help families navigate Murray Road at peak times. She is highly regarded by the school community and neighbouring houses. Her efforts to prevent reckless driving and potential accidents undoubtedly keeps our pupils safe. I have personally witnessed many occasions where her intervention has been essential. The proximity of our schools to the train station means that local roads are extremely busy, particularly at the start and the end of the school day, and parking issues further exacerbate this problem. Our controller directs traffic daily, preventing the roads around our schools from grinding to a halt. A pelican or zebra crossing would be utterly ineffective at these times and in these situations.</p>
<p>These crossing people are of immense importance</p>
<p>Road is too busy to not have a lollipop lady</p>
<p>Terrible and dangerous idea to remove the crossing patrol person on Murray Rd! Not only does she keep everyone safe, but also monitors the traffic and moves people on. For parking/waiting dangerously!!!</p>
<p>Please do not take away our crossing patrol, we need it due to an extremely busy road, keep us safe</p>
<p>I cannot say how much the patrol lady is important in saving the lives of the school children especially Walter infant school. Please reconsider the decision</p>

We need the crossing patrol as the Murray road is too narrow and in her absence the traffic becomes chaotic, which is extremely dangerous for kids and also drivers.

We want the looly lady.

The school crossing controller should NOT be removed - it is a very dangerous road!! Drivers speed down the road and if the controller is removed it is a major accident just waiting to happen!!

I do not see the benefit of a permanent crossing in Murray Road (opposite Walter Infant / St. Pauls Junior School / Meadow Nursery. The road is very narrow and a fixed crossing will only slow and complicate through traffic outside school drop-off and pick-up times. A school crossing controller who is helping children to cross safely and can use common sense to respond to fluctuating traffic situations is a far better solution.

I think the lack of a school will make the walk to school unsafe. Currently the school patroller ensures the safe flow of traffic which is difficult due to the high volume of traffic trying to go down Murray road. The area is very difficult both before and after school and needs ADDITIONAL safety measures.

Our crossing patroller, Mrs Burch, does a marvellous job of helping our families navigate Murray Road at peak times. She is highly regarded by the school community and her efforts to prevent reckless driving and potential accidents undoubtedly keeps our pupils safe. The proximity of our schools to the train station means that local roads are extremely busy, particularly at the start and the end of the school day, and parking issues further exacerbate this problem. Mrs Burch regularly directs traffic, preventing the roads around our schools from grinding to a halt. We would be lost without her!

I don't believe that safe crossings will be sufficient for this crossing due to the risks associated with the very narrow path outside St Paul's. This crossing patroller does an excellent job of keeping people moving and more importantly the children safe. I also observe her making sure that people do not park in a dangerous way next to the crossing.

Obviously an emotive subject as the crossing patroller is a welcome friendly face every day and teaches children road safety, but regardless of that, having a human in charge of the crossing point means both traffic and pedestrians benefit; whilst a zebra or pelican crossing would provide a safe point for children and parents to cross, it would undoubtedly cause additional hold ups for car users. The current crossing patroller is excellent at insuring traffic also gets time to move, holding people who cross back to allow the bottle neck of traffic to clear. Traffic lights will cause long delays; a red light is a red light regardless of how many people cross and traffic could be waiting for nothing. A zebra crossing on a road where one side is almost entirely made up of parked cars during school drop off and collection times is not safe; the view of people trying to cross will be blocked for those crossing from the school to the other side. The crossing patroller waits on the opposite side to ensure she has a clear view of the traffic before stepping out.

Our patroller is essential for keeping our children safe - the road is used as a through road for people not wishing to wait at the level crossing and Mrs Burch is

the only reason why traffic keeps moving and accidents are very rare. There is no parking at the school so parents do try and park on pavements, double yellow lines, at the nursing home and residents houses - Mrs Burch currently polices this and makes sure it doesn't happen.

I would urge you to keep the school crossing patrol on Murray Road, as it is an essential aid in keeping our children safe at peak access times. Without a crossing patrol, I hate to think how dangerous crossing will become with the sheer weight of traffic and number of children using Murray Road at Peak times.

This is stupid idea a child will be killed if this goes ahead Murray Road gets so so busy at drop off and pick up time

I am disgusted that Wokingham council would even contemplate this action which will endanger young children.

Removal of this lady would such a loss. She is incredibly proactive at these extremely busy times of day. An unmanned crossing would not deal with the parked cars right in front of the school gates. It would put our children at risk.

Please do not remove the lollipop lady on Murray Road, outside Walter Infant and St Paul's. Her presence keeps our children safe. The footpath outside the schools are very narrow and with hundreds of children and parents walking along this road and on this footpath, it would be incredibly easy for children to spill onto the road, without a lollipop lady. There is no way a zebra crossing will keep the children as safe as the presence of a lollipop lady/traffic controller. Please do not remove this valuable and important service for the sake of all the children at Walter Infant and St Paul's.

The school crossing patrol makes sure that our kids cross the road safe especially where this school is. The road is so dangerous and a lot of people drive unresponsive. They do not care about the kids. If it not was for the school crossing patrol a lot of kids would have been at risk getting injured or even worse! The council should not touch or change anything about the school crossing patrol. Doing so the council is putting our kids in danger!

Our crossing patrolled does an amazing job ensuring the safety of over 300 children aged three to eleven years as well as younger siblings. She regularly advises drivers who are parking inconsiderately to relieve congestion on an increasingly busy road and has also discussed road safety with both of my children in their nursery and infant years.

The value of Mrs Burch on Murray Road is huge. Not only does she keep every family safe, help to manage frequent gridlock situations and give everybody a warm greeting to start their days, she also encourages safety awareness to the children before they cross, such as walking scooters across the road and stopping a good distance from the roadside to wait to cross. She is an excellent role model to the children teaching them safety and friendliness to others. At All Saints where my eldest is in year 6, the issue is more the speed at which cars often travel down Norreys Avenue, which would be dealt with by a permanent crossing. However, the concern would be that much needed parking spaces would be lost in the construction of a crossing, with parking already in short supply, causing regular

<p>gridlock (meaning people mounting the curbs causing danger to pedestrians) and people's driveways being blocked etc.</p>
<p>Ridiculous. So dangerous. There are some drivers that drive way too fast down Murray Road, and someone may get hurt one day from this silly decision to get rid of Mrs Birch</p>
<p>School Patrol is required during school hours else it will cause terrible traffic. Students and parents are continuously crossing the roads which will lead to more traffic as nobody is there to control them. It may also lead to accidents.</p>
<p>I consider the proposals to remove patrollers to be dangerous and unnecessary. The Walter/St Pauls patroller is extremely valuable and does far more than any static crossing could.</p>
<p>The school crossing patroller is vital in this area when controlling the traffic and protecting the children whilst crossing. A pelican/zebra crossing would NOT provide the same level of protection for the children during peak times. I suspect we would see more frequent traffic gridlock also.</p>
<p>I think this would be a mistake. The crossing patroller on Murray Road does so much more than just help the families cross safely and wish them a good day. It would of course be sad not to have her cheery smile in the morning however Mrs Burch does so much more than this and this is the reason for my objection to this proposal. Mrs Burch ensures the traffic continues to flow safely along the road and to lose this key figure would be a complete mistake. Any days without her there in the mornings are utter traffic chaos and grid lock! Having lived on the road for the last 14 years, the road is mostly very quiet of cars and pedestrians and only busy at these peak times so a zebra crossing just wouldn't be necessary. Traffic lights would stop the traffic when it's already potentially grid locked and just make the situation worse. There are entrances to 2 school and a nursery opposite the entrance to a nursing home, Mrs Burch ensures the traffic can move freely down the road at busy times and so prevents these entrances being obstructed, something vital for the nursing home should an ambulance be required. We need Mrs Burch on Murray Road to continue to do the fantastic job she has been doing for so many years. It would be a complete mistake to have her removed from post.</p>
<p>It is important that as children get older they learn to get to school on their own. The school crossing patroller is vital and reassuring for both children crossing the road safely and for parents in letting their children get to school on their own. There is so much traffic around the school and cars driving and parking in a dangerous manner that it is not safe to let primary aged children cross on their own. A child's life is worth far more than the financial saving that this measure would bring about.</p>
<p>The school crossing patroller provides a much greater benefit to the local community than just enabling the safe crossing of Murray Road. That benefit far outweighs the limited savings that would be made through the abolition of this service. The patroller balances the needs of both pedestrians and drivers by collecting pedestrians and crossing them in groups. With a zebra crossing pedestrians at the busy times would simply stream across the road offering no break to vehicles. This could lead to a great deal of additional frustration on the</p>

part of drivers and yet more reckless and inconsiderate driving. A pelican crossing would not take account of the situation on the road and could create more problem than it solves. At busy times Murray Road is effectively just one lane and the school crossing patroller often takes it upon herself to direct the traffic so that everyone can keep moving. She is also very active at calling out people who have parked illegally or inconsiderately. As a local resident who regularly walks up and down Murray Road at different times of the day and week, I would suggest that there would be no real benefit in having a permanent crossing. The road is simply not busy enough at other times that it is unsafe to cross. Apart from school drop-off and pick-up times there is very light traffic and there is good visibility along the road. For anyone travelling to and from town or the station there is already a pelican crossing outside the schools on Oxford Road which provides an alternative route. In conclusion, I am strongly against the removal of the school crossing patroller for the reason that the benefits to the local community of keeping her far outweigh the costs.

The school crossing patroller does a wonderful job and has a pivotal role in keeping our children safe on a very busy and often gridlocked road. The road by Walter Infant school is incredibly hectic at school start and end times and I dread to think just how much worse and potentially dangerous this situation would get if we lost our school crossing patroller.

I have one son who attends Walter Infants, hoping to continue at St Paul's Juniors and one son who attends Meadow Nursery and hoping to continue at Walter Infants. We walk every day and will be crossing Murray Road several times a day over the next few years. The crossing lady, Mrs Burch does an excellent job. She not only ensures that every child (and adult) is safely across the road, she also is very aware of cars parked on the double yellow lines or zigzag lines and politely explains why they should not be parked there. Mrs Burch also does a very good job of keeping the traffic moving at very busy times, whilst ensuring the pedestrians safety and this is all with smile and good morning. Murray Road, like most roads in Wokingham and because of its proximity to the station, becomes excessively busy at peak times, with many drivers often being more concerned on getting their own children to school or not being late. This can cause a lack of awareness for those around them and I do not feel a Pelican or Zebra crossing is a suitable or safe solution. I would very much like to know why school crossing patrollers need to be removed? Can it really save much more money for the council? I'm sure that the other safety crossing patrollers in Wokingham are similar to Mrs Burch and we would be losing important community assets if they are removed. If more houses and people are continually being built and brought into the area the council need to rethink their plans for roads and most importantly safety, instead of cutting costs more investment needs to be made in both.

I think the safety of my 3 children and all the children of the school will be compromised. It is a very busy through road, and the parking is shocking. The controller manages the traffic and the safe entrance of my children to school. If she wasn't there, my children would not be safe.

In the mornings the road is very busy with local work traffic and school traffic. The traffic concertinas up and the school crossing patroller does an amazing job in keeping it moving safely. With a static crossing this could not happen. There are lots of children crossing the roads and cars parked if they had to wait for an electronic patrol I fear there would definitely be more accidents and road rage which is already bad along that road. I don't believe it is necessary to have a crossing there 24 hours a day as there is little traffic or pedestrians at other time.

I strongly disagree with the proposals. Murray Road is an extremely busy and congested road, especially during school open/close. There are constant issues of road rage and backed up traffic where cars cannot move due to parked cars/congestion. This is very dangerous for children and the Patroller on many occasions helps free up backed up traffic and congestion by talking them through moving. There are two schools and a nursery that use this site so there are so many children who would be put at risk by getting rid of the patroller. The patroller also asks people to move who have parked on the school yellow lines also ensuring safety of children to enter the schools without obstruction. The current traffic light crossing is on Oxford Road, not Murray Road. For children that are on the other side of Oxford Road, coming towards the school from the station, this crossing is of no use so they would have to cross Murray Road anyway. This would be very dangerous with no patrol working as people would be tempted to cross anywhere. Having a patrol person ensures most people cross in safety at the same place. By installing a zebra crossing here, there would be a constant stream of people crossing and traffic would be more snarled up than it is now encouraging more risks by motorists. The patrol worker ensures that people wait for a short time, letting traffic pass and then once there is a small group, cross together. This enables the constant flow of vehicles yet keeps the pedestrians safe as they are not crossing all over Murray Road and it ensures traffic has stopped for them which it would not do necessarily if they were crossing wherever they felt like. If a traffic light system was installed here, There would be no space for people to wait on the pavement as they volume of pedestrians is so great with siblings and pushchairs too. The patrol worker ensures a perfect balance between numbers crossing and the free movement of traffic, which technology could not respond to in the same way.

Lots of morning congestion around Meadow/Walter/St Pauls and crossing patroller help keep children safe

I am concerned that the removal of the patrol officer will lead to an increase in dangerous driving and inconsiderate parking. The area where the crossing patroller works get really clogged and we will see more cars going up on pavements which is really dangerous with children walking on those pavements. Cars do this because they can get trapped on single lane and row of cars in both directions unable to move backwards or forwards. This happens when there is no-one managing who needs to go and who to stay put. The crossing patrol does a lot more than stopping cars to let children cross- she manages the traffic too and tells people to move when they are parking on the yellow zigzags or other equally stupid things that people do when they are in a rush. The school run brings out the

worse in some drivers unfortunately and it will be so much worse without someone there prioritising the safety of the children.

My child attends Meadow Nursery which is on the same site as 2 schools: Walter Infants and St Paul's. I have been made aware of this consultation and although I am aware of the need to cut public services, removing the remaining school crossing patrollers is frankly dangerous. The council should support parents and children who walk to school and Murray road gets particularly busy because of its proximity to the train station. Many cars bypass the railway crossing by diverting along Meadow Road and then Murray Road. There is also no space for parents to park who have to drive to pick-ups so the result is a lot of cars along a 1 single road given that parking is on the road as well. You will already know this. Your letter tries to pretend that replacing a patroller with a '24 hour solution' is better for the local community but as you well know, this service is only needed for the times that school and nursery children are pouring out of 3 schools. How much of a saving would this change represent and would it really be worth it? The fact this patroller helps children from all 3 sites means you are already saving money. This is a terrible idea and I hope the council works hard to keep front line staff where they are needed.

Murray road is extremely busy and dangerous at drop off and pick up time. In the mornings with the proximity to the station, the mix of commuters and school traffic the congestion can be extreme and the school crossing patroller not only gets the children across the road safely, but helps to keep the flow of the traffic moving. There have been incidences of people being hit by cars already and I dread to think what may happens the traffic and children are left to their own devices on the road.

I feel much more assured that cars will stop at a busy time of day when the School Crossing lady is there. She is friendly and competent. I think she provides a great service.

I would have concerns if the school crossing patroller was not there for the safety of children and parents crossing Murray Road. I fear an accident would be inevitable and a matter of time. The lady in this job is very fair to both drivers and pedestrians and does a fabulous job. She brightens up your day and loves her job clearly. It is part of community spirit and retaining some of that is important. However my comments are mainly regarding concern for safety of the children should they no longer be there.

The crossing patroller, Mrs Burch, does a fantastic job. It is a very busy road and she is integral to safe road crossing for the children and parents of the schools.

The crossing guard on Murray Road to Walter School does an excellent job. Replacing her with traffic lights would be disruptive and cause traffic delays at non-school times and it is too busy a road at school times for the Zebra Crossing to be a safe option.

Murray Road does not have an automated crossing and is very busy. Removing the School Crossing Patrol for Walter\St Pauls is not safe for children and parents trying to get to the school

<p>The crossing patroller Diane 'Mrs Lollipop' to the children is much loved and so integral to our safety. As a teacher and Mum, I appreciate how important it is for children to have a safe place to cross the road.</p>
<p>It's an absolute disgrace to remove the crossing patrollers - these are cut backs that should never be made!</p>
<p>People drive down the road with no consideration for the children trying to cross. When there is no school patrol they don't stop to allow us to cross so she is vital for safety.</p>
<p>We think the school crossing patroller does a great job and is very much needed by the school. The road is very very busy and the standard of driving is very poor at peak times. We have seen many issues resolved by the patroller. She is needed. A crossing without her would not be sufficient.</p>
<p>That road is a nightmare at School pick up and drop off times. The lolly pop lady actually helps the situation. If we take her away and put lights there this would cause so much more road rage from the drivers and unnecessary problems. Removing our lady would be a critical mistake.</p>
<p>It will cause more traffic and potential accident to young children on such a busy road</p>
<p>School crossing controller is critical to safety in a very busy area where two schools are based and there is a lot of traffic not always related to the schools</p>
<p>I feel that you are endangering our children by removing the crossing patrollers</p>
<p>Despite zebra crossings patrol people are important in ensuring the children's absolute safety down that busy road. People can drive quite fast despite it clearly being school run time</p>
<p>Lights will make the traffic worse. Our lolly pop lady does a great job of directing traffic when it gets stuck.</p>
<p>Removal of the lollipop lady would be extremely detrimental to the safety of both children and adults. Plus the already dreadful traffic around the school which is worsened by people trying to avoid the level crossing which seems to be down more than up at this time in the morning will be made much worse. The lollipop lady has the ability to step in and out of the road quicker than that of lights on a pedestrian crossing. She is also wonderful at moving on people that have parked just outside the school illegally. This would be a very inappropriate decision if she was gotten rid of.</p>
<p>I have a child and Walter infants and another at Meadow Nursery School, both on Murray Road. I have significant concerns about the potential removal of the school crossing patroller and the impact on safety for my children. The traffic along Murray Road is very heavy with drivers using the route as a cut through from Reading Road to Barkham Road, in addition to the school traffic for 3 different settings located on Murray Road. I do not believe that a pelican or zebra crossing will provide a safe alternative because they will not allow the traffic to flow readily . Nor can it prevent parents parking in dangerous places or deal with incidents of road rage. There is no doubt in my mind that this is a dangerous decision. Maybe if the councillors decided not to take their annual pay rise the money could be spent on the salary of our school crossing patroller instead.</p>

Mrs Burch does an amazing job keeping everyone safe her presence is vital to the safety of our children & you can't put a price on that!

The lollipop control is essential for both safety and for traffic to flow. The volume of traffic is simply too high to flow if there is no one controlling the flow around the Murray / Oxford Road junction. Without control and safe crossing, it is only a matter of time before there are accidents with the number of small children, and smaller siblings needing to cross the road. The cost must be small, the risk great. Look at alternative funding options, community donations? Rather than removing service.

I have been walking my children to school for over 5 years, and I can say confidently that the crossing controller outside Walter/St. Paul's saves injury on a regular basis, in a way that an automated crossing just couldn't do. Let me explain. Murray Road is a complex beast at school rush hour. There are 3 schools (including Meadow Nursery), narrow roads, hundreds of parents, children and dogs on pavements that are too narrow for the number of people and push chairs, cars zipping in and out of parking spaces, other cars using it as a cut through and residents using their driveways. Sometimes it's the people that get jammed, such that you can't get onto the pavement over the crossing because of the number of bodies in the way. The crossing controller manages this by preventing anyone else from crossing the road and keeping the cars waiting until it is safe. Sometimes it's the cars that get jammed (usually due to inconsiderate, impatient drivers), such that the road gridlocks. The crossing controller manages this by holding the pedestrians at bay and directing the car drivers individually in order to clear the situation. I have seen many incidents that, without human anticipation and reaction, would have endangered children's safety. I feel very strongly that a machine could not do this job, and that a human crossing controller is critical to keeping parents and children safe and managing traffic flow at busy school times. A pelican crossing wouldn't work because the cars would never be able to move. Traffic lights wouldn't work because the pavements are too narrow to hold the weight of people. Please, please, please do not remove the human element from this crossing and introduce extra risk to the children's journeys to school.

Traffic lights will not be safe for such a busy road where cars are always reversing and bumping up curbs and double parking outside the school. how can a year three child walk across that road safely

We NEED to keep our crossing patroller. For the safety of our children. Mrs Burch does an excellent job of controlling the traffic.

Having human intervention is key to reduce distinct risk to children risking a crossing with bi directional traffic.

Please retain the school crossing patroller. She does a wonderful job of keeping the kids safe during busy mornings. Thanks

It is already a crazy traffic scene there. Automated signal will make it worse. Strongly recommend to continue with the school patrol

Disappointed that the consultation doesn't encourage as many people as possible to submit comments. There is an assumption that only parents of existing children at these schools will be the ones you would expect to be interested. Would future

parents have a view? What about parents whose children have recently or not so recently been at the school? What about drivers who use this road and welcome the guidance they are given. Having a person in the middle of the road is a powerful deterrent. Will the police be asked for their views? School crossing patrollers provide an external link between the school, the children and who takes them to school. I would like to know exactly what is being suggested, when the report will be produced and how a decision will be made.

I strongly oppose getting rid of the school crossing patrols. Your weasely vague use of 'where appropriate' with respect to alternate crossing facilities makes it apparent that you have few solid intentions to do anything other than stopping funding the patrols. How can you consult in such vague terms - with absolutely no guarantees on what you will do to protect the safety of children in the borough? If you're going to replace the crossing patrols with something else specific - tell us what that is. Surely you've done some sort of road safety audit or risk assessment on this proposal that has led you to a view of options beyond a "maybe we will, maybe we won't, we're not sure, we might tell you what we'll do later". This is no way to statutorily consult on public sector proposals. On a narrower point - I was under the impression that good practice in government consultations suggested 12 weeks as an acceptable consultation period; a minimum even. Is this how long your consultation is running? I'm proud to pay my tax and this proposal is shaming. I have seen a great deal of poor traffic light (and other road traffic) compliance around St Paul's/Walter Schools - people running red lights and nearly mowing down children. The fact that my daughter has been hit by a vehicle at a nearby crossing, by a driver running a red light, causes me to feel intense unease at your proposal. I will actively and vehemently oppose it. I understand the budget pressures you're under but I simply do not believe you are at this point - other things must be cut first: sell your car park(s) for development, do fortnightly bin collections, your parking charges are ludicrously low - raise them, introduce a workplace parking levy.

I feel the children would be unsafe with our lolly pop lady there have been so many occasions she has stepped in when children have tried to cross without an adult

I am not happy about changes. We definitely need Crossing Patrol. Deals with many incidents, stops accidents

The road outside the school is busy and dangerous. I will only let my kids cross with the patrol. There is non-school commuter traffic which zips down there plus there are often angry and frustrated drivers. It's no road for children to cross. It's dangerous. Please don't let an accident or fatality be the reason for bringing it back if you axe this vital service for our children.

The volume of traffic going past Walter/St Paul's is very high and without the school crossing lady it would be very dangerous crossing the road. I

Terrible idea! These roads are very unsafe and we need the assistance of the school crossing.

I think the idea of getting rid of the crossing patrollers is a bad one, as the traffic down Murray Road is crazy during school pick up and drop off. I have seen impatient drivers speeding off with a complete disregard for the swarms of children

<p>and families getting to and from school. Mrs Burch does a fantastic job laying down the law and controlling the traffic, she teaches the kids to wait until its safe and does so with a lovely smile on her face. To get rid of her would be a huge mistake.</p>
<p>Murray Road is incredibly busy in the mornings and our crossings lady does a fantastic job to keep the children safe. There needs to be some safe way for crossing children/parents at the school.</p>
<p>Murray road is very busy and cross patroller doing great job if she will be removed kids will be in danger</p>
<p>The crossing lady does not just make sure children cross the road safely, she provides a whole traffic management system, keeping the flow of traffic moving in a really tricky spot. When she is not there, children, led by their parents, cross wherever they like. Also cars force their way onto the traffic coming up from Meadow Road.</p>
<p>Yes I think that removing the school patroller will endanger children's lives, as the traffic problems on Murray Road are awful at school drop off. Without the patroller controlling not only the crossing but traffic as well I feel this will only get worse.</p>
<p>We would be lost without our crossing patrol, cars regularly pull up on zig zags or up on the kerb, and our patroller keeps them all under control, it would be so dangerous if this was to removed and replaced with permanent lights</p>
<p>The Crossing controlled by human has more advantages than traffic lights. I can't imagine traffic/ how many car accidents will occur without our Lollipop Lady</p>
<p>Rely on our lollipop lady. Her assistance is invaluable</p>
<p>I am not happy about removal of the crossing Patrol near St. Pauls Junior and Walter Infant</p>
<p>It would be very difficult to lose our crossing patroller as she is a great help outside school</p>
<p>We need to keep our school crossing patrol lady. She in invaluable and plays a huge part in keeping our children safe.</p>
<p>We really can't do without the patrol crossing, it keeps everyone safe as cars speed through this cut through. Also lots of unsafe parking that is kept under control.</p>
<p>We really need the school patroller. She does an amazing job of keeping our children safe on such a busy road.</p>
<p>If you implement zebra crossing, then people will be crossing continuously there will be even more traffic jam than now.</p>
<p>Please do not get rid of the school lollipop lady at Walter / St. Paul's schools. The road is very dangerous and she does amazing job of ensuring the children don't get run over.</p>
<p>It's very hard for me to hold 3 children's hands at once to cross. I just hope and pray no child gets hurt if we lose the lollypop lady</p>

Comments relating to pupils from Willow Bank Infant School

Crossing patrols were created to keep children safe whilst crossing roads to come to school. Will they be reinstated after a child dies? How is removing safety features for pedestrians helping to encourage walking to school? Having a pelican crossing outside the school will delay traffic, cause queues and pollution from waiting vehicles, whereas the crossing patrol only works when necessary!

The lollipop lady is part of the school community. Her contribution is more than just helping to cross the road. Children going to Waingels also cross with her. Please do not assume that only primary school children affected. They also help with traffic flow management at busy periods.

The school lollipop lady is an integral part of our community, she is bright and friendly, knows all the children and families who use the crossing, helps the children learn how to cross the road safely - neither of my children have run into the road or accessed a road dangerously since they have started school and watching the crossing on a daily basis has contributed to that. The lollipop lady is an integral member of the school community, keeps our children safe and is able to make observations about car use to feed back to the school

Although this is a permanent 20mph zone, it seems that vehicles rarely adhere to the speed limit. Also parking in the road, especially around the start and end of school, can make visibility of oncoming vehicles very difficult. The current crossing patrol officer does an excellent job of keeping the children and parents safe because she is positioned on the opposite side of the road to the parked cars and therefore has better visibility. A zebra crossing will not increase safety because the vehicles will not always have visibility of people wanting to cross the road and vice versa.

I would strongly be in favour of retaining the current crossing controller and not to change town unmanned system.

I think that removing a key safety personnel in any area is a disappointing proposal for any school but particularly those with very dangerous roads. My own school absolutely benefits from having a crossing patrol due to the busy nature of the road

The patroller is an important safety feature for our school they cannot be removed

I feel that it would be create an unsafe environment for the children trying to get to school as many cars driving through the area drive above the speed limit of 20mph. There is also no other safe way of crossing as cars continuously park on the double yellows every morning and afternoon blocking the drop down kerbs and restrict views on the road to see oncoming traffic. I do not believe a zebra crossing would help as many cars are going too fast to notice or stop for the crossing. I would strongly reconsider the removal of the lollipop lady outside Willow Bank

Infant and Junior school. Also, you have a duty of care to ensure the safety of all the children in your borough arriving and leaving school each day. This is not the area to be making cuts!

Duffield Road is a very busy road, particularly at the morning drop off. I live in Charvil so I drive to Ashenbury Park and walk my boys up to the Willow Bank Schools. My concern would be that without the visible presence of Mrs Rawlings people would ignore the zebra crossing. I take the point that the zebra crossing would then be there all day rather than just at drop off and pickup but I think it's fair to say that at other times of day 1) the road is much quieter and 2) there are not so many people needing to cross the road.

I strongly disagree with this decision, Mrs Rawlings safe crossing duty is extremely important especially with the parking issues and the speed of some drivers. Without her is an accident waiting to happen.

I would like the patroller to stay as I think it is safer.

The school is in a 20 zone. This constantly gets ignored with drivers zooming along here, even at busy times. Double yellow lines are also constantly ignored. Having a controller acts as a deterrent at the times it is most needed as well as ensuring children get to safely cross the road. It is an accident waiting to happen, so please keep the controller. I also believe that given the elderly a purpose and gets them out of the house. Surely it is better to pay them a little for a valued community service rather than the alternative of loneliness, depression and other health issues.

I am deeply concerned by the proposed removal of the crossing patrollers. While I only have one child who attends the school, I travel to and from each morning and afternoon with my three children, two of whom travel on bikes / scooters, one of whom is in a pushchair. The crossing patroller has been invaluable to help me keep my children safe. There are numerous parked cars next to the school entrance on the right, which can make crossing hard to judge when leaving the school grounds. Although I obviously keep my children as close to me as I can and continue to teach them how to behave safely around roads, there have been moments when they have not listened for whatever reason (it does not help that my boy has ASD and is a runner, regularly creating safety concerns) and it has been a tremendous relief to have the crossing patroller there to ensure they stop where they need to stop. I believe she has saved my son from serious harm on at least two occasions since he started school in September. I am sure I cannot be the only parent with these kinds of concerns. An unmanned crossing will not resolve the issue with the parked cars or exuberant children on what is a busy road.

Comments relating to pupils from Woodley C of E Primary School

The lollipop lady is part of the school community. Her contribution is more than just helping to cross the road. Children going to Waingels also cross with her. Please do not assume that only primary school children affected. They also help with traffic flow management at busy periods.

I understand why a zebra crossing would seem sensible, however the lady standing at the crossing allows my daughter to cross safely and gain more confidence. Drivers DO not slow down on that road when the lady is not there and maybe pay attention to Zebra crossing. The road is always very busy cars are parked either illegal or very close together so the safety lady helps everyone cross safely.

Whilst the crossing outside Woodley C of E is not have the same traffic volume as others in the area it is quite complicated. In addition to the through traffic on Hurricane Way, there are cars entering and exiting the school and community centre car park on one side and traffic turning right from Comet Way. All of this traffic is constrained into a single lane due to the traffic calming measures in place outside the school. It is often difficult as an adult to predict what the drivers are going to do when faced with this situation let alone a 10 year old child who may be walking to school unaccompanied. The school crossing patrol brings more certainty to this chaos. Another aspect that should be considered is the additional security offered by having a school crossing patrol stationed outside the school. These people know the children and their families and what is normal outside the school gate providing an additional set of eyes and ears. Whilst I wouldn't expect this person to take direct action in the event of an incident occurring they may provide an early warning or in the event of an incident occurring, critical information..

Please keep the Lollipop ladies! Crossing the road is so much safer and easier when there is a physical human being in the middle of the road to help everyone cross it. I have found on days there is nobody there, many drivers are not courteous to people trying to cross and the road gets blocked up with cars as the traffic doesn't flow as well either. Our children are much safer with a person out there helping them cross than a zebra crossing type situation.

I think the crossing patrollers do a wonderful job keeping our children safe, and cannot be replaced by a formal crossing. The crossing patrol ladies do a wonderful job in not only teaching our children about road safety, but also act as a visible deterrent to speeding cars, or those who might not otherwise stop for traffic lights or a zebra crossing. Removing them will cost lives.

Comments from respondents not linked to any school

The safety of the children is being put at risk especially in the early hours/afternoon when the traffic is at its peak. Why is Loddon Primary School not considered in this consultation? My children attend this school

This is a badly written survey! My children are older than junior school age but I still have an opinion. The crossing at Keephatch is a busy road and cars do not take notice always of Crispins but if someone is standing in the road they stop. A school in Bracknell recently someone was injured on a crossing without a lollipop lady. Cut back on other things but not children's safety.

Installing permanent crossings such as a zebra or pelican type will benefit all residents crossing the roads at these points at all times, because it will be there 24/7 and will help slow the traffic at all times in these areas. This is a good idea.

Stop the cuts. Provide a properly funded service rather than spending £600 a day on interim heads of service

Our daughter is currently attending Meadow nursery (Murray Rd) and will attend Walter Infant from September 2018 onwards. We are always walking to nursery/school, and the presence of the lady helping us cross the road makes all the difference. The traffic around the schools is often very chaotic, so it seems crucial to rely on help to ensure children's safety. Please keep the system in place!

My children have recently left Keephatch Primary and the crossing patroller is essential on this busy road where cars regularly drive over the speed limit.

My child attends meadow nursery and we use the safe crossing at St Paul's school. We find that the safe crossing is necessary as Murray road gets very busy at drop off and pick up times.



DO NOT SCALE

KEY

 Proposed location of controlled pedestrian (zebra) crossing

A	NOT ISS	DAH	ISSUED	@	@
REV	DATE	BY	DESCRIPTION	CHK	APD

DRAWING STATUS: FOR INFORMATION ONLY



**WOKINGHAM
BOROUGH COUNCIL**

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: Wokingham Borough Council

ARCHITECT:

PROJECT: Duffield Road,
Woodley

TITLE: Proposed controlled crossing
(Zebra)
Location plan

SCALE @ SIZE: 1:1000	CHECKED: SLA	APPROVED:
CAD FILE: 18-05 LOCATION PLAN NOI	DESIGN/DRAWN: SLA	DATE: May 2018
PROJECT No: 18/19 captial	DRAWING No: 18-05 NOI Location plan	REV: A

Willow Bank
Primary Schools

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

This page is intentionally left blank



DO NOT SCALE

KEY

Proposed controlled crossing (zebra) location

A	NOT ISS	DAH	ISSUED	@	@				
REV	DATE	BY	DESCRIPTION	CHK	APD				

DRAWING STATUS: **FOR INFORMATION ONLY**

**WOKINGHAM
BOROUGH COUNCIL**

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: Wokingham Borough Council

ARCHITECT:

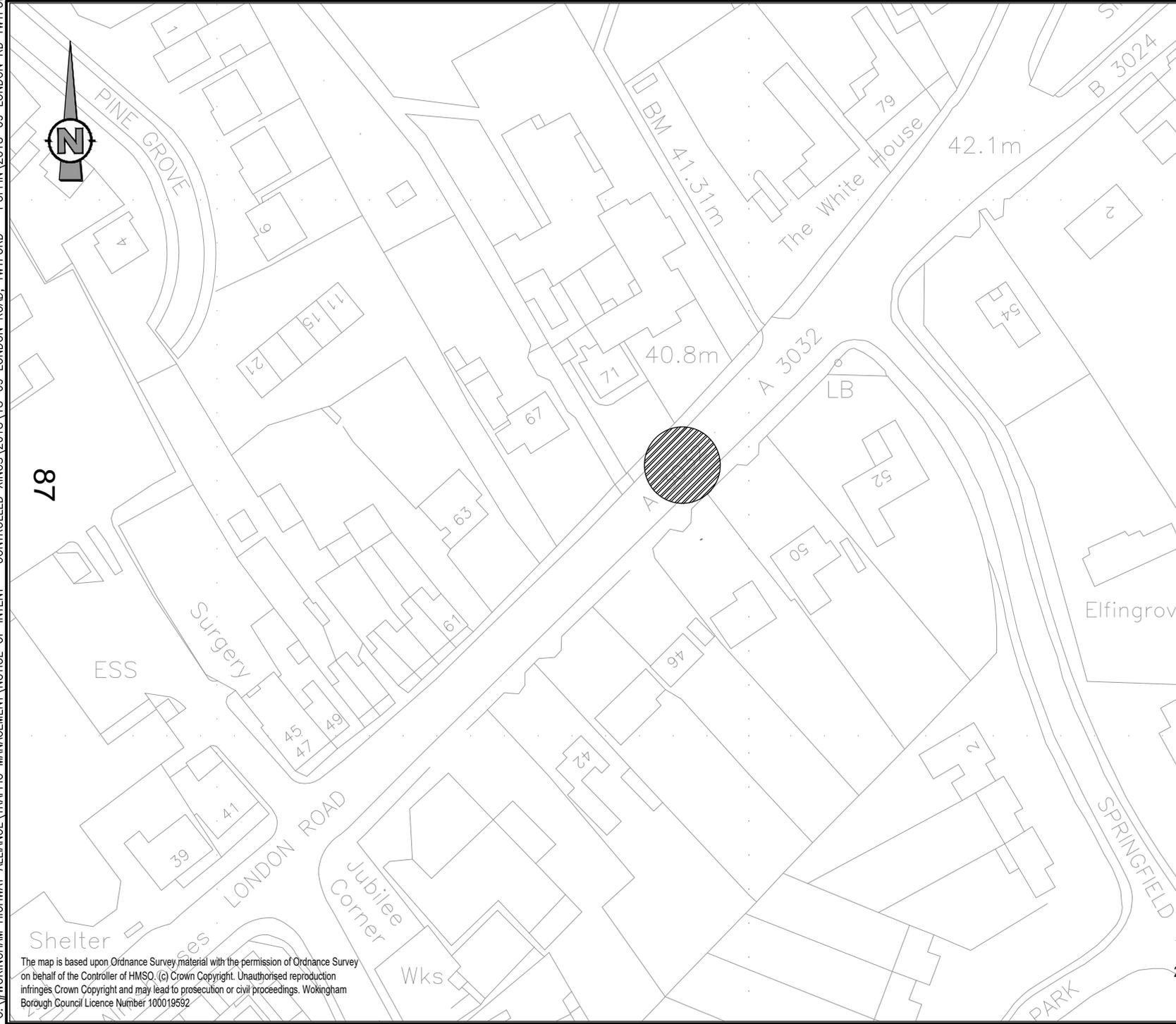
PROJECT: Hurricane Way
Woodley

TITLE: Proposed location of controlled crossing (zebra)

SCALE @ SIZE: 1:1000	CHECKED: SLA	APPROVED:
CAD FILE: 18-06 HW NOI LOCATION PLAN	DESIGN/DRAWN: SLA	DATE: May 2018
PROJECT No: 18/19 Capital	DRAWING No: 18-06 HW NOI LOCATION	REV: A

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

This page is intentionally left blank



DO NOT SCALE

KEY

 PROPOSED LOCATION OF CONTROLLED (PUFFIN) CROSSING

A	NOT ISS	DAH	ISSUED		
REV	DATE	BY	DESCRIPTION	CHK	APD

DRAWING STATUS: FOR INFORMATION ONLY



**WOKINGHAM
BOROUGH COUNCIL**

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL

ARCHITECT:

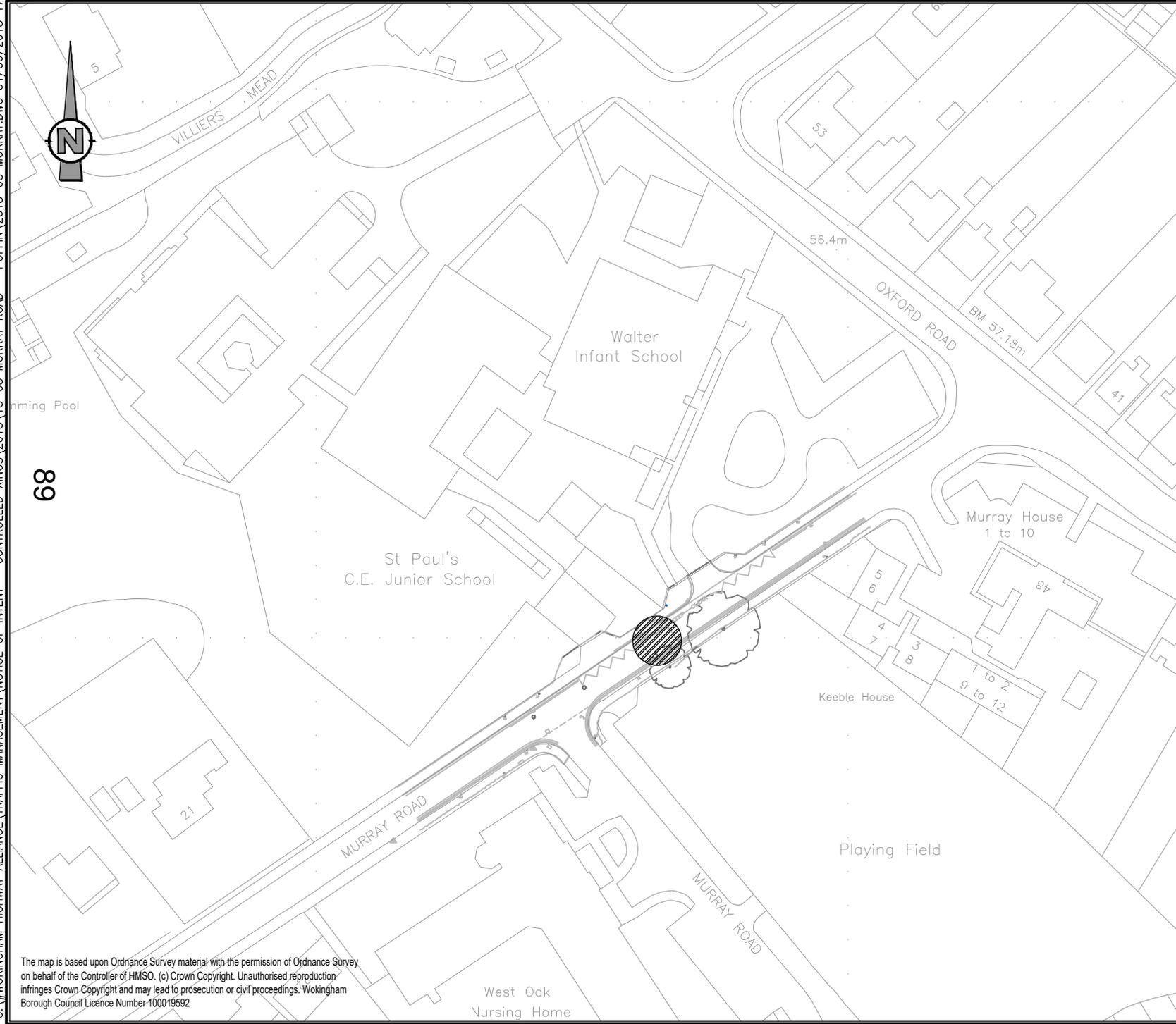
PROJECT: London Road
Twyford

TITLE: Proposed location of
Controlled Crossing
(Puffin)

SCALE @ SIZE: 1:1000	CHECKED:	APPROVED:
CAD FILE: 2018-09 LONDON RD TWYFORD	DESIGN/DRAWN: SA	DATE: June 2018
PROJECT No: 2018-09 London Rd Twy	DRAWING No: 2018-09 London Rd	REV: A

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

This page is intentionally left blank



68

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

DO NOT SCALE

KEY

 Proposed location of controlled (zebra) crossing

A	NOT ISS	DAH	ISSUED	@	@
REV	DATE	BY	DESCRIPTION	CHK	APD

DRAWING STATUS: FOR INFORMATION ONLY



**WOKINGHAM
BOROUGH COUNCIL**

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL

ARCHITECT:

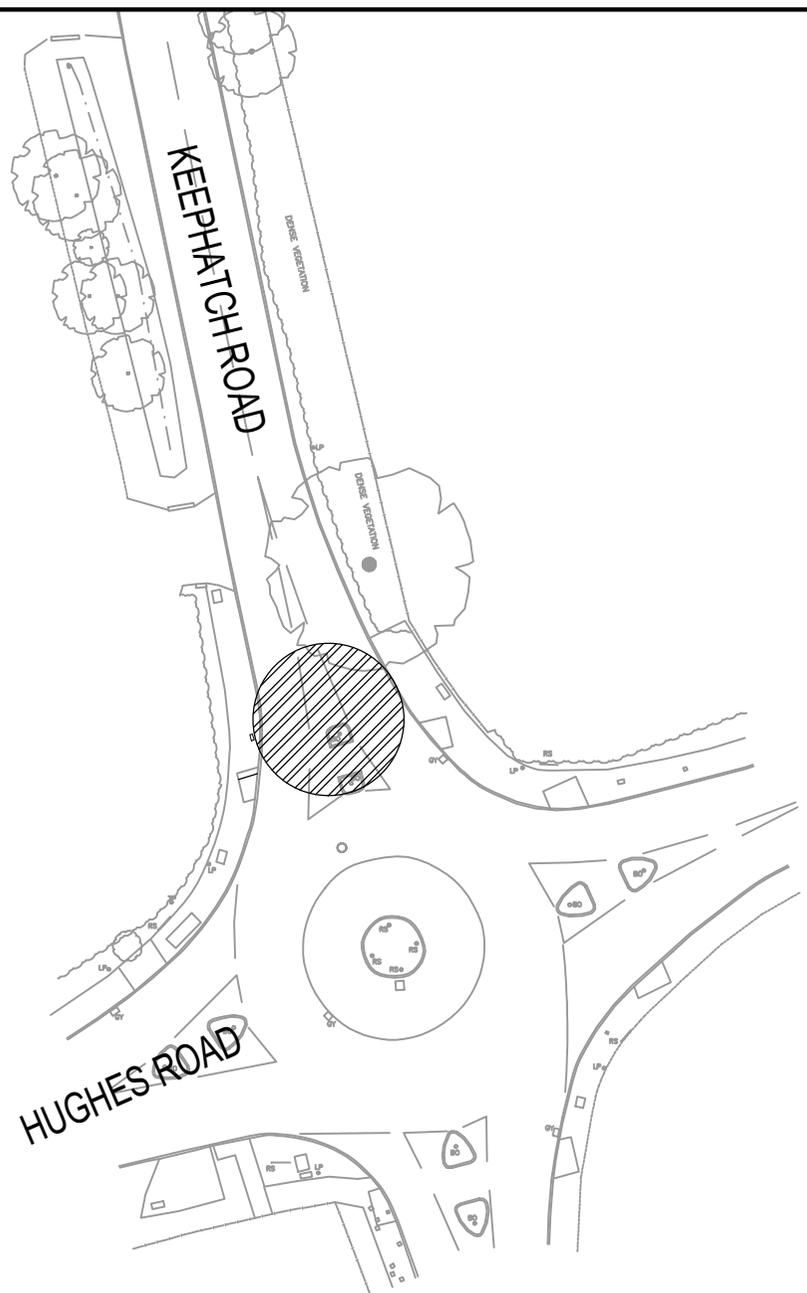
PROJECT: Murray Road
Wokingham

TITLE: Proposed location of
Controlled Crossing
(Zebra)

SCALE @ SIZE: 1:1000	CHECKED:	APPROVED:
CAD FILE: 2018-08 MURRAY	DESIGN/DRAWN: SA	DATE: June 2018
PROJECT No: 2018-08 Murray	DRAWING No: 2018 - 08 Murray	REV: A

This page is intentionally left blank

91



The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

DO NOT SCALE

KEY

 PROPOSED LOCATION OF CONTROLLED (ZEBRA) CROSSING

A	NOT ISS	DAH	ISSUED		
REV	DATE	BY	DESCRIPTION	CHK	APD

DRAWING STATUS: FOR INFORMATION ONLY



**WOKINGHAM
BOROUGH COUNCIL**

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL

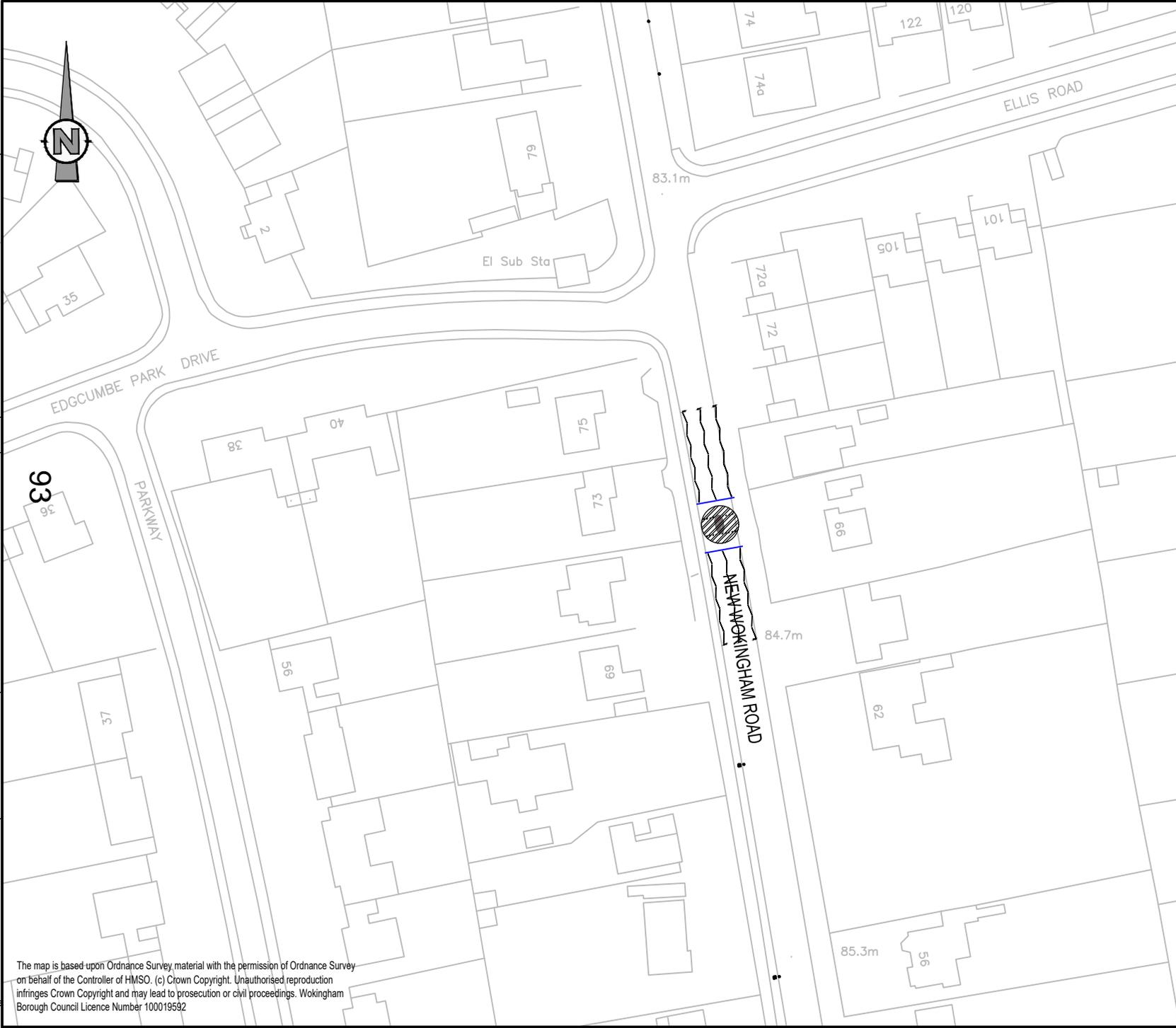
ARCHITECT:

PROJECT: Keepatch Road,
Wokingham

TITLE: Proposed location of
Controlled Crossing
(ZEBRA)

SCALE @ SIZE: 1:500	CHECKED:	APPROVED:
CAD FILE: 18-07 Keepatch Road	DESIGN/DRAWN: SA	DATE: June 2018
PROJECT No: 2018-07	DRAWING No: 2018-07 Keep	REV: A

This page is intentionally left blank



The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

DO NOT SCALE

KEY

 PROPOSED LOCATION OF CONTROLLED (PUFFIN) CROSSING

A	NOT ISS	DAH	ISSUED		
REV	DATE	BY	DESCRIPTION	CHK	APD

DRAWING STATUS: FOR INFORMATION ONLY



**WOKINGHAM
BOROUGH COUNCIL**

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL

ARCHITECT:

PROJECT: New Wokingham Road
Crowthorne

TITLE: Proposed location of
Controlled Crossing
(Puffin)

SCALE @ SIZE: 1:1000	CHECKED:	APPROVED:
CAD FILE: NOI 2018-10 NWR	DESIGN/DRAWN: SA	DATE: June 2018
PROJECT No: 2018-10	DRAWING No: 2018-10 NWR	REV: A

This page is intentionally left blank



The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

DO NOT SCALE

KEY

 PROPOSED LOCATION OF CONTROLLED (PUFFIN) CROSSING

A	NOT ISS	DAH	ISSUED		
REV	DATE	BY	DESCRIPTION	CHK	APD

DRAWING STATUS: FOR INFORMATION ONLY



WOKINGHAM BOROUGH COUNCIL

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL

ARCHITECT:

PROJECT: Silverdale Road
Earley

TITLE: Proposed location of
Controlled Crossing
(Puffin)

SCALE @ SIZE: 1:1000	CHECKED:	APPROVED:
CAD FILE: 2018-11 SILVERDALE PUFFIN	DESIGN/DRAWN: SA	DATE: June 2018
PROJECT No: 2018-11	DRAWING No: 2018-11 Silverdale Rd	REV: A

This page is intentionally left blank

Wokingham Borough Council
Notice of Intent
Proposed Installation of Controlled Crossings
Duffield Road & Hurricane Way - Woodley, Keephatch Road & Murray Road – Wokingham, London Road – Twyford, New Wokingham Road – Crowthorne, Silverdale Road - Earley

Notice is hereby given that, Wokingham Borough Council ("the Council"), pursuant to its powers under Section 23 of the Road Traffic Regulation Act 1984 (as amended), intends to install four (4) controlled (Zebra) crossings on Duffield Road & Hurricane Way (Woodley), Keephatch Road (Wokingham) and Murray Road (Wokingham) and three (3) controlled (Puffin) Crossings on London Road, (Twyford), New Wokingham Road (Crowthorne) and Silverdale Road (Earley).

The locations of the proposed Zebra crossings are:

- i) Duffield Road, approximately fifty five (55) metres north west of its centreline junction with Uppingham Drive.
- ii) Keephatch Road, approximately seventeen (17) metres north of the centre of the roundabout at Hughes Road.
- iii) Murray Road, approximately seventy seven (77) metres south west of its junction with Oxford Road
- iv) Hurricane Way, approximately thirty (30) metres north east of its centreline junction of Comet Way.

The location of the proposed Puffin crossings are:

- i) London Road, approximately forty four (44) metres south west of its centre line junction with Springfield Park.
- ii) New Wokingham Road, approximately forty four (44) metres south of its centre line junction with Edgumbe Park Drive
- iii) Silverdale Road, approximately thirty eight (38) metres north west of its junction with Gipsy Lane.

Any person requiring more information in connection with this proposal should contact the Council's Traffic Management Team, during normal office hours on telephone number 0118 974 6856.

A copy of this Notice and a plan detailing the location of the said crossings may be inspected at Wokingham Borough Council, Shute End, Wokingham (Environment) for a period of 21 days from the date of this Notice. These documents are also available online at www.wokingham.gov.uk/tro

Any comments on the proposals should be made in writing and state the grounds on which they are made and should be sent to Sara-Louise Allman, Traffic Management Team, WBC, Shute End, Wokingham, RG40 1WL or via email to TM.consultations@wokingham.gov.uk no later than 5th July 2018. Please note that all representations received in response to this Notice are public documents and may be viewed by anyone.

Josie Wragg
Interim Director, Environment
Wokingham Borough Council
Shute End
Wokingham
RG40 1WL
14th June 2018

This page is intentionally left blank

Dear headteacher,

As part of its ongoing review of the value of its non-statutory services, Wokingham Borough Council is holding a public consultation on its remaining School Crossing Patrollers, one of whom is currently operating near your school.

There are currently seven school crossing patrollers in the Wokingham Borough and the Council is asking residents and stakeholders views on a proposal to remove these services and replace them with safe permanent crossings (pelican/zebra crossings) where appropriate. These crossing would provide safe crossing points at all times (24hour) rather than just in the mornings and afternoons.

It is proposed the school crossing patrol service at these locations should cease at the end of this academic year (July 2018). The affected sites are: All Saints Primary School, Norreys Avenue, Wokingham; St Paul's Junior & Walter Infant Schools, Murray Road, Wokingham; Keephatch Primary School, Keephatch Road, Wokingham; Willow Bank Infant & Junior School, Duffield Road Woodley; South Lake Primary School, Nightingale Road, Woodley; Woodley CE Primary School, Hurricane Way, Woodley; Polehampton Infant & Junior Schools, London Road Twyford.

At each of these sites the Council will carry out an assessment in accordance to the processes described in the document produced by Road Safety GB entitled 'School Crossing Patrol Service Guidelines'. Where the assessment indicates a patroller would be justified, the Council will design and deliver where applicable a formal pedestrian crossings before the start of the new academic school in September 2018.

Consultation on these proposed changes will run until to Friday March 2. Schools, parents and guardians affected by the proposals can take part online. Follow the link: <http://www.wokingham.gov.uk/news-and-consultation/consultation-and-having-your-say/current-consultations/> or visit www.wokingham.gov.uk and search for Consultations.

This page is intentionally left blank

Dear clerk,

As part of its ongoing review of the value of its non-statutory services, Wokingham Borough Council is holding a public consultation on its remaining School Crossing Patrollers, at least one of whom is operating in your town / parish.

There are currently seven school crossing patrollers in the Wokingham Borough and the Council is asking residents and stakeholders views on a proposal to remove these services and replace them with safe permanent crossings (pelican/zebra crossings) where appropriate. These crossing would provide safe crossing points at all times (24hour) rather than just in the mornings and afternoons.

It is proposed the school crossing patrol service at these locations should cease at the end of this academic year (July 2018). The affected sites are: All Saints Primary School, Norreys Avenue, Wokingham; St Paul's Junior & Walter Infant Schools, Murray Road, Wokingham; Keephatch Primary School, Keephatch Road, Wokingham; Willow Bank Infant & Junior School, Duffield Road Woodley; South Lake Primary School, Nightingale Road, Woodley; Woodley CE Primary School, Hurricane Way, Woodley; Polehampton Infant & Junior Schools, London Road Twyford.

At each of these sites the Council will carry out an assessment in accordance to the processes described in the document produced by Road Safety GB entitled 'School Crossing Patrol Service Guidelines'. Where the assessment indicates a patroller would be justified, the Council will design and deliver where applicable a formal pedestrian crossings before the start of the new academic school in September 2018.

Consultation on these proposed changes will run until to Friday March 2. Schools, parents and guardians affected by the proposals can take part online. Follow the link: <http://www.wokingham.gov.uk/news-and-consultation/consultation-and-having-your-say/current-consultations/> or visit www.wokingham.gov.uk and search for Consultations.

This page is intentionally left blank

NEWS RELEASE



**WOKINGHAM
BOROUGH COUNCIL**

Date: January 22 2018
Issued by: David Allen (0118) 974 6066

Issue No: 4d
Version: 1.0

CONSULTATION ON FUTURE OF SAFE SCHOOL CROSSINGS

As part of its ongoing review of the value of its non-statutory services, Wokingham Borough launching a public consultation on its remaining School Crossing Patrollers.

The council is proposing to build permanent safe crossing at the seven locations where school crossing patrollers currently operate. Pedestrian crossings (zebra or pelican crossings) offer safe crossings for all users at all times. The affected sites are: All Saints Primary School, Norreys Avenue, Wokingham; St Paul's Junior & Walter Infant Schools, Murray Road, Wokingham; Keephatch Primary School, Keephatch Road, Wokingham; Willow Bank Infant & Junior School, Duffield Road Woodley; South Lake Primary School, Nightingale Road, Woodley; Woodley CE Primary School, Hurricane Way, Woodley; Polehampton Infant & Junior School, London Road Twyford.

The proposal is to assess each site in accordance to Road Safety GB's 'School Crossing Patrol Service Guidelines'. Where the assessment shows the need for a safe crossing, the Council will build a pedestrian crossing before the start of the new academic school in September 2018. Once a pedestrian crossing has been installed and fully operational, school crossing patrollers are no longer required. Last year some school crossing patrollers operating on existing pedestrian crossings were removed with no safety impact recorded to date.

Cllr Keith Baker, executive member for highways and transport said: "Our preferred option is pedestrian crossings that offer a 24/7 safe crossing which has additional benefits to all residents crossing the road at any time and not just school children. A pedestrian crossing has a well-known legal basis, which all motorists should fully understand and obey. Earlier this year we discontinued the school crossing patrols service at locations where there was already a safe pedestrian crossing and have proved to be successful in maintaining the safety of the children involved. It follows that the provision of a school crossing patroller on the new pedestrian crossings is likely to be discontinued in the same way as the earlier actions. However it does need to be stressed that no decision will be made until after the consultation."

**-MORE-
103**

Consultation on these proposed changes runs until Friday March 2. Schools, parents and guardians affected by the proposals can take part online. Visit www.wokingham.gov.uk and search for Consultations.

- ENDS -

Note to editors

1. More from Cllr Keith Baker on 07917 673 370

NEWS RELEASE



**WOKINGHAM
BOROUGH COUNCIL**

Date: Friday June 15 2018
Issued by: David Allen (0118) 974 6066

Issue No: 64d/18
Version: 0.1

PERMANENT SAFE CROSSING TO BE BUILT AT EIGHT SCHOOLS

Wokingham Borough Council is to build eight new permanent pedestrian crossings at school sites across the borough.

The new crossing will all be in place before the school term starts in September 2018 and will provide safe 24-hour a day crossings for all residents. The decision follows a public consultation carried out earlier this year into school sites currently service by a School Crossing Patroller. The consultation, which had about 360 responses, did not highlight any specific safety issues relating to the removal of School Crossing Patrollers, many comments were focused on wider issues outside the remit of School Crossing Patrollers.

Therefore, we have decided to extend safe crossings by replacing the current crossing patrollers with safe permanent pedestrian crossings. We will also provide permanent crossings at two sites that have not had a crossing patroller for some time. This will mean that people of all ages are helped to safely cross the road at all times rather than the current limited periods and locations where crossing patrollers are currently in place.

The affected sites where a zebra crossing will be put in place are:

- St Paul's Junior & Walter Infant Schools in Wokingham
- Woodley CofE Primary School in Woodley
- Willow Bank Infant & Junior Schools in Woodley
- Keep Hatch Primary School in Wokingham
- South Lake Primary School in Woodley

The affected site where a signal-controlled crossing will be put in place is:

- Polehampton CofE Infant & Junior Schools in Twyford

**MORE-
105**

The existing crossing location in Norreys Avenue (All Saints CE (Aided) Primary School) will be enhanced in addition to the existing system of traffic calming and 20mph speed limit zone.

In addition, new signal-controlled crossings will be installed at two sites that have not had a patroller operating on the site for some time these are at Oaklands Primary School and Loddon Primary School.

Executive member for transport and highways, Cllr Keith Baker said: “We do not take a decision such as this lightly. We looked carefully at the consultation responses and it is clear that some parents like the reassurance of a school crossing patroller and that the patrollers are popular.

“But it is also clear that permanent crossings are a safe alternative. Last year, we removed patrollers from locations that had permanent crossings and there have been no issues around the pedestrian crossing providing safe passage for school children. In fact, the crossings have provided a safe way for all pedestrians, including pupils and parents, crossing the road. Furthermore, it has increased the safety for crossing these roads outside of school times.”

With the installation of the permanent crossings, the council’s remaining school crossing patrol roles have been put at risk of redundancy subject to a formal HR process.

- ENDS -

Further information:

More from Cllr Keith Baker, executive member for transport and highways, (0791) 767 3370 or email: keith.baker@wokingham.gov.uk

Tel: (0118) 974 6000
E-mail: communityengagement@wokingham.gov.uk
Date: 14 June 2018
Ref: SCP 2018



**WOKINGHAM
BOROUGH COUNCIL**

To: Schools

Environment

P.O. Box 153

Shute End, Wokingham

Berkshire RG40 1WL

Tel: (0118) 974 6000

Minicom No: (0118) 974 6991

BY EMAIL ONLY

Dear Headteacher,

SCHOOL CROSSING PATROLS AND PEDESTRIAN CROSSINGS

Wokingham Borough Council continually reviews the value of its non-statutory services and has been examining the effectiveness of pedestrian crossings as a proven safe way of crossing a road.

There are currently seven school crossing patrollers in the Wokingham Borough. Following a public consultation this year, it has been decided to cease this service at the end of this academic year (July 2018) and to install permanent controlled pedestrian crossings such as a zebra or signal-controlled crossings or improve an existing formal crossing.

The affected sites where a zebra crossing will be put in place are:

- St Paul's Junior & Walter Infant Schools in Wokingham
- Woodley CofE Primary School in Woodley
- Willow Bank Infant & Junior Schools in Woodley
- Keep Hatch Primary School in Wokingham
- South Lake Primary School in Woodley

The affected site where a signal-controlled crossing will be put in place is:

- Polehampton CofE Infant & Junior Schools in Twyford

The existing crossing location in Norreys Avenue (All Saints CE (Aided) Primary School) will be enhanced in addition to the existing system of traffic calming and 20mph speed limit zone.

In addition, new signal-controlled crossings will be installed at two sites that have not had a patroller operating on the site for some time, these are at Oaklands Junior School and Loddon Primary School.

Should you have any queries please contact the Executive Member for Highways and Transport, Cllr Keith Baker, at keith.baker@wokingham.gov.uk. Alternatively contact Matt Gould, Highways Service Manager, at Matthew.Gould@wokingham.gov.uk or call (0118) 974 6460.

Yours sincerely,

Josie Wragg

Interim Director of Environment

Tel: (0118) 974 6000
E-mail: communityengagement@wokingham.gov.uk
Date: 14 June 2018
Ref: SCP 2018



**WOKINGHAM
BOROUGH COUNCIL**

To: Schools

Environment

P.O. Box 153

Shute End, Wokingham

Berkshire RG40 1WL

Tel: (0118) 974 6000

Minicom No: (0118) 974 6991

BY EMAIL ONLY

Dear Councillor,

SCHOOL CROSSING PATROLS AND PEDESTRIAN CROSSINGS

Wokingham Borough Council continually reviews the value of its non-statutory services and has been examining the effectiveness of pedestrian crossings as a proven safe way of crossing a road.

There are currently seven school crossing patrollers in the Wokingham Borough. Following a public consultation this year, it has been decided to cease this service at the end of this academic year (July 2018) and to install permanent controlled pedestrian crossings such as a zebra or signal-controlled crossings or improve an existing formal crossing.

The affected sites where a zebra crossing will be put in place are:

- St Paul's Junior & Walter Infant Schools in Wokingham
- Woodley CofE Primary School in Woodley
- Willow Bank Infant & Junior Schools in Woodley
- Keep Hatch Primary School in Wokingham
- South Lake Primary School in Woodley

The affected site where a signal-controlled crossing will be put in place is:

- Polehampton CofE Infant & Junior Schools in Twyford

The existing crossing location in Norreys Avenue (All Saints CE (Aided) Primary School) will be enhanced in addition to the existing system of traffic calming and 20mph speed limit zone.

In addition, new signal-controlled crossings will be installed at two sites that have not had a patroller operating on the site for some time, these are at Oaklands Junior School and Loddon Primary School.

Should you have any queries please contact the Executive Member for Highways and Transport, Cllr Keith Baker, at keith.baker@wokingham.gov.uk. Alternatively contact Matt Gould, Highways Service Manager, at Matthew.Gould@wokingham.gov.uk or call (0118) 974 6460.

Yours sincerely,

Josie Wragg

Interim Director of Environment

Title: Wokingham Borough Council: School Crossing Patrol Service – Consultation Report - April 2017

Report to Malcolm Richards, Executive Member for Highways and Transport

Prepared by Alex Deans, Matthew Gould & Dave Allen, Environment

Purpose

The report provides a summary of comments received from the 102 individual responses received to the public consultation and the two petitions (409 signatures) also received on the subject. All comments have been considered and a summary of the key issues raised along with WBC's response is contained at Appendix A. The majority of the comments related to perceived safety issues at specific locations

Background

School Crossing Patrollers are currently employed by the Council, primarily to help children cross the road safely on their way to and from school. The service is discretionary and currently employs 15 patrollers. Of these 15 patrollers currently employed, five operate on a pelican crossing and three on a zebra crossing. The remaining seven patrollers operate at locations where there are no formal controlled crossing facilities

Wokingham Borough Council continually reviews the value of its non-statutory services, and in this context has been considering the effectiveness of pedestrian crossings as a proven, efficient and safe way of crossing a road. Eight of the school crossing patrollers are currently operating where there is already a safer crossing facility in place.

It was therefore proposed the school crossing patrol service at these eight locations should cease at the end of this academic year (July 2017).

The eight locations that were part of this proposal were:

- Emmbrook Infant & Junior School, Emmbrook Road, Wokingham.
- Highwood Primary School, Fairwater Drive, Woodley
- The Coombes Primary School, School Road, Arborfield.
- Earley St Peters Primary Church of England School, Church Road, Earley.
- Whiteknights Primary School, Elm Road, Earley
- Beechwood Primary, Headley Road
- Hawthorns Primary School, Northway, Woosehill
- Sonning Church of England School Primary School, Pound Lane, Sonning

The remaining seven sites (not listed above) with school crossing patrollers but no formal crossings are exempt from these proposed changes.

Consultation

A public consultation was held to seek comments on this proposal from March 1 to April 19. The table below provides details of the number of responses received by the Council in response to this consultation.

Location	Number of responses received
Sonning CofE Primary School	34 comments
Emmbrook Schools	27 comments
Earley St Peters School	16 comments
The Coombes School	8 comments
Whiteknights Primary School	2 comments
Beechwood Primary School	2 comments
General comments (including those that refer to more than one school)	13 comments
Highwood School	No individual comments (was referred to by one general comment)
Hawthorn Primary School	No comments
EPetition: We the undersigned petition Wokingham Borough Council to abandon its proposal to cease school crossing patrol service at eight locations at the end of this academic year (July 2017).	161 signatures
Hardcopy Petition: to Keep our Road Crossing Patrollers and Keep our Children Safe.	248 signatures

Appendix A below provides a summary of the key issues raised by consultees along with responses from WBC

The consultation has generated a number of responses from concerned individuals and it is clear that the School Crossing Patrol Service is a well-respected and valued service amongst those responding with the SCP providing not only assistance for those requiring it to cross the road but also additional safeguarding of young road users in general.

A significant proportion of the comments received relate to general road safety issues i.e. poor driving, speeding vehicles, inconsiderate parking. Issues such as these are not unique to locations near school crossing patrol sites and ultimately it is drivers' responsibility to drive safely and legally. As the only agency currently with the power to enforce these issues any concerns should be reported to the police on the non-emergency number 101. When Wokingham Borough Council takes on Civil Parking Enforcement (CPE) from the police later this year it will be able to target problem areas more effectively.

Another common theme is the general safety for children on their journey to school. It is the responsibility of the parent or carer to ensure their child is safe on the highway. Parents and carers play a vital part in teaching children general road safety and also how to cross the road safely, however the Council through its Road Safety Education

team also provide road safety training and activities to school and this service will continue.

Both zebra crossings and pelican (light-controlled) crossings are safer places for pedestrians to cross the road and where they are given priority and as such pedestrians do not require additional assistance when using them. In many cases, the provision of an SCP predated the crossing and was kept when the formal crossing was installed.

All but one of the pelican crossings is a Puffin crossing (Pedestrian user friendly intelligent crossing). Puffin crossings look very similar to Pelicans and are an updated version of a Pelican crossing. One of the main differences is that the red and green man signals are just above the WAIT box and not on the other side of the road. Puffin crossings have special sensors built in which can detect a pedestrian waiting and makes sure that traffic remains stopped until all the pedestrians have crossed.

When SCPs work on light-controlled crossings, they must step off the kerb and take their position in the road while the red light is showing for traffic, so they do not confuse drivers. SCPs must use the lights to stop traffic, essentially replicating the function of the traffic lights.

Conclusion

Consideration of all the responses to the consultation show that they have not raised any issues that should prevent WBC continuing with its proposed removal of the SCP at eight locations that are already served by formal pedestrian crossings facilities from the end of the academic year being July 2017.

The consultation has however highlighted some site specific issues that should be investigated before the start of the new school year in September 2017. These are:

- that all pedestrian crossing at the affected sites be inspected to ensure all lining, signing and other site aspects are all in good condition;
- that consideration be given to the conversion of one pelican crossing to a Puffin;
- that all light controlled crossings are inspected to ensure RMS is operational and that the signal equipment is in good order.

In addition it is recommended that all schools affected are reminded that they have access to the Council's Road Safety and MyJourney Teams who can facilitate further road safety training for pupils if requested.

Further details of our Road Safety activities can be found on the Council's website at <http://www.wokingham.gov.uk/parking-road-works-and-transport/road-safety/> or by contacting the team via email road.safety@wokingham.gov.uk.

Details relating to MyJourney Wokingham can be found at the following link <http://www.myjourneywokingham.com/discover-wokingham/schools/> The web page also provides a link to a contact page should you require any additional information on or wish to access any the initiatives.

Appendix A: Summary of consultation Responses and WBC response

Responses relating to Sonning C of E Primary School	
Comments were received from 34 respondents and letters were received from the SCP at the school and Sonning Parish Council	
Key Themes	WBC response
Poor driving – speeding on Pound Lane	<p>Speed limit enforcement is a matter for Thames Valley Police and any concerns should be reported to them on the non-emergency number 101.</p> <p>The Council will ensure all lining, signing and other site aspects are present and in good condition before the start of the new school year (Sept 2017)</p>
Poor parking – dangerous parking around school and there is a lack of safe parking opportunities	<p>Poor parking is a problem at many schools however it is the responsibility of driver to ensure they do not park inconsiderately.</p> <p>When WBC takes over Civil Parking Enforcement (CPE) from the police later this year it will be able to target problem areas more effectively, and the council has already agreed with the Parish Council to a future review of parking here in the light of CPE.</p>
Poor driving – drivers failing to stop at pedestrian crossing	It is drivers' responsibility to drive safely and legally and any concerns should be reported to the police on the non-emergency number 101.
Pound Lane is a very busy road with volume of traffic that means SCP is required	Pedestrian Crossings provide safe crossing points and are appropriate for busy roads. The crossing point is also co-located with a road hump to help reduce vehicle speeds.
Higher proportion of parents have to drive as they live in surrounding villages	The volume of traffic does not prevent a Pedestrian Crossing from being a safer crossing facility
Pedestrian crossing is near a dangerous bend which blocks motorists' view	All SCP locations are reviewed on a regular basis. The last assessment carried out on the site (July 2016) did not raise any issues or concerns with the site or specifically the bend. It should be noted that the approach from the bend falls within a 20mph and visibility sightlines are adequate. Additionally, there is warning signage and school flashers on both approaches.
The SCP is a popular figure who teaches children road safety	It is acknowledged that SCP's are popular however, the discretionary service they have been employed to provide is to ensure people can cross safely and this duplicates the function of the Pedestrian Crossing. WBC can facilitate additional road safety training if schools request it.
There have been attempted abductions in neighbouring counties and SCP increases	School Crossing Patrollers are employed by the Council to help children cross the road safely on their way to and from school. The presence of people at school drop-off

security	and pick provides mitigation.
The savings are not large and could be made in other ways	The level of savings is not relevant to the decision being made. The issue is that SCP and Pedestrian Crossings perform the same function and, therefore, both are not necessary.
The pedestrian crossing is a zebra crossing not a traffic-light controlled crossing, which would be safer	Zebra Crossings are considered safe crossing facility. However where appropriate zebra crossing could be converted at a later date to a pelican crossing should funding be available and the site location permits the provision of such a facility
Sonning Parish Council has offered a financial contribution to keep the SCP but would not want to employ one directly	Options for alternative funding are not part of this consultation. The borough council's position remains that the SCP are not necessary due to the presence of the Pedestrian Crossing

Responses relating to Earley St Peters CofE Primary School	
Comments were received from 16 respondents	
Key Themes	WBC response
Church Road is particularly busy / dangerous road with heavy congestion in the rush hours	Pedestrian Crossings provide safe crossing points and are appropriate for busy roads
Frequent incidence of poor driving and cycling – failure to stop at red light and dangerous parking	<p>It is drivers' and cyclists' responsibility to drive safely. The Council will ensure all lining, signing and other site aspects are present and in good condition before the start of the new school year (Sept 2017)</p> <p>Poor parking is a problem at many schools however it is the responsibility of driver to ensure they do not park inconsiderately.</p> <p>When WBC takes over Civil Parking Enforcement (CPE) from the police later this year it will be able to target problem areas more effectively</p>
Pavements too narrow for the volume of pupils	It is parents' and carers' responsibility to ensure the safety of their children on journeys to and from school. SCP's role is to ensure people can cross roads safely not to ensure safety in other areas.
Earley St Peter's is a large school and therefore there are more pupils attempting to cross road than other schools and SCP help 'crowd control'	It is parents' and carers responsibility to ensure the safety of their children on journeys to and from school. SCP's role is to ensure people can cross roads safely not to ensure safety in other areas.
There is no audible signal that it is safe to cross at the pedestrian crossing	The audible signal is to assist users that are visually impaired and is not used as a primary indication that it is safe to cross as not all crossing will have this enabled. This is often disabled when crossings are located near houses. As an additional aid to users that are visually impaired there is a rotating cone located at the base of

	the button box which users are taught to use
The phasing of the lights at the pedestrian crossing does not allow enough people to cross during busy period and without a SCP this could encourage people to cross dangerously	<p>The length of time people have to cross the road should not be affected by the presence of the SCP because SCP have to operate with the traffic light phasing.</p> <p>This crossing at this location is a Puffin crossing. Puffin crossings have sensors to detect if there are pedestrians waiting at the side of the road to cross. This allows the Puffin crossing to cancel the crossing demand, as it won't be needed if the pedestrians are no longer there.</p> <p>Another sensor is used to extend the crossing time if necessary, by checking/monitoring for pedestrians still in the crossing area.</p>

Responses relating to The Coombes Primary School	
Comments were received from eight respondents and a letter from Arborfield and Newland Parish Council	
Key Themes	WBC Response
Frequent incidence of poor driving – failure to stop at red light and dangerous parking at school drop-off	<p>It is drivers' and cyclists' responsibility to use the highway in a safe manner and follow the laws of the road. The Council will ensure all lining, signing and other site aspects are present and in good condition before the start of the new school year (Sept 2017)</p> <p>Poor parking is a problem at many schools however it is the responsibility of parents to ensure they do not park inconsiderately.</p> <p>When WBC takes over Civil Parking Enforcement (CPE) from the police later this year it will be able to target problem areas more effectively</p>
Frequent incidence of children running from parents and need for SCP to help 'crowd control'	It is parents' and carers responsibility to ensure the safety of their children on journeys to and from school. SCP's role is to ensure people can cross roads safely not to ensure safety in other areas.
School has only one entrance that is close to the main road and is over-crowded at drop-off and pick-up times	SCP's role is to ensure people can cross roads safely not to ensure safety in other areas. Schools are responsible for their own access and can seek to make changes if necessary.
The volume of traffic outside school is increasing and will increase due to the Arborfield Garrison Strategic Development Location	Pedestrian Crossings provide safer crossing points and are appropriate for busy roads
Parents have been encouraged to park at the Royal British Legion and walk to school and a walking bus has been	Parents walking their children from the British Legion and the walking bus can use the Pedestrian Crossing. There is no reason for parents to stop walking their children to school as a result of these changes. It is

established. This route takes them across the pedestrian crossing. Fear has been expressed that removal of SCP would lead to more parents driving to school	parents' and carers responsibility to ensure the safety of their children on journeys to and from school.
Lorries and other traffic use School Lane rather than Langley Common Road (due to its designation as a B road) and this leads them passed the school despite Langley Common Road being more suitable for large volumes of traffic	Pedestrian Crossings provide safer crossing points and are appropriate for busy roads.

Responses relating to Emmbrook Schools	
Comments were received from 27 respondents and two letters were received from the SCP at the school	
Key themes	WBC response
The SCP is a reassuring and popular figure for parents and children	It is acknowledged that SCP are popular however, the service they have been employed to provide is to ensure children can cross safely and this duplicates the function of the Pedestrian Crossing
North Wokingham Strategic Development Location will lead to increase in traffic on local roads	Pedestrian Crossing provide safer crossing points and are appropriate for busy roads
Poor driving. Speeding is a problem outside school – appeal for increased traffic calming and measure to prevent parents parking illegally outside school	<p>It is drivers' responsibility to drive safety and legally and any concerns should be reported to the police on the non-emergency number 101.</p> <p>Poor parking is a problem at many schools however it is the responsibility of the driver of the vehicle to ensure they do not park inconsiderately.</p> <p>When WBC takes over Civil Parking Enforcement (CPE) from the police later this year it well be able to target problem areas more effectively.</p> <p>There is a series of traffic calming in place around Emmbrook School and an enforceable 20mph speed limit. Issues regarding noncompliance should be reported to the police on the non-emergency number 101.</p>
The site has three schools and two pre-schools in close proximity and therefore additional safety issues than	There is a series of traffic calming in place around Emmbrook School and an enforceable 20mph speed limit. These measures are considered appropriate for the area.

other areas	
Audible alert on crossing has been turned off and should be reactivated. In addition, the phasing of crossing would need to be changed if there is not SCP	The audible signal is to assist users that are visually impaired and is not used as a primary indication that it is safe to cross as not all crossings will have this enabled. This is often disabled when crossings are located near houses. As an additional aid to users that are visually impaired there is a rotating cone located at the base of the button box which users are taught to use
Pavements around school are narrow and SCP helps reduce the danger this causes. In addition children climb on the yellow safety barriers – adding to danger	It is parents' or carer's responsibility to ensure the safety of their children. School Crossing Patrollers are employed by the Council to help children cross the road safely on their way to and from school.
Safe walking routes to school need to be developed	Safe walking routes are in place for the school and WBC can facilitate further road safety training in school if the school requests it.
School has SEN children, including a hearing impaired unit, the children from which will not hear cars or sirens well.	Pedestrian Crossings provide safer crossing points for users with additional needs, including those that are sight and hearing impaired. It is the responsibility of parents and carers to ensure their children are helped to walk to school when necessary.

Responses relating to Beechwood School	
Comments were received from two respondents (including the school headteacher)	
Poor driving – cars failing to stop at pedestrian crossing	It is drivers' responsibility to drive safely and legally and any concerns should be reported to the police on the non-emergency number 101.
Poor driving – cars speeding outside school (request for traffic calming)	It is drivers' responsibility to drive safety. Additional speed reducing measures could be considered at individual sites, however enforcement is the responsibility of Thames Valley Police and any measure could only be introduced in collaboration with them. Any concerns should be reported to them on the non-emergency number 101.
Request for traffic light operated crossing to replace zebra crossing	Zebra Crossings are considered safe crossing facilities. However where appropriate zebra crossing could be converted at a later date to a pelican crossing should funding be available and the site location permits the provision of such a facility
Request for more road safety education	WBC does offer road safety training and will provide further training at the school should the school request it.

Responses relating to Whiteknights School
--

Comments were received from two respondents	
Volume of traffic makes road dangerous	Pedestrian Crossing provide safe crossing points and are appropriate for busy roads
SCP ensures children do not stand too close to edge of road	It is parents' and carers responsibility to ensure the safety of their children on journeys to and from school. SCP's role is to assist people in crossing the road and not to ensure their safety in other areas.
SCP is a popular and valued figure	It is acknowledged that SCP are popular however, the service they have been employed to provide is to ensure children can cross safely and this duplicates the function of the Pedestrian Crossing

General responses (and those relating to more than one school)	
Comments were received from 13 respondents	
Poor driving – in particular drivers failing to stop, speeding and being distracted by phones etc	It is drivers' responsibility to drive safety. Additional speed reducing measures could be considered at individual sites if problems persist however enforcement is the responsibility of Thames Valley Police and any measure could only be introduced in collaboration with them. Any concerns should be reported to them on the non-emergency number 101.
Drivers and pedestrians do not understand Pedestrian Crossing and as a result act dangerously on the,	Pedestrian Crossings are covered in the Highway Code and it is the responsibility of all drivers to understand them. WBC's My Journey and Road Safety Teams can support schools to increase parents and children's understanding of them.
Without SCP, who will report issues such as Pedestrian Crossing lights that fail, drivers who do not stop and road surface problems	All these issues can be reported by parents or school staff directly to the police (in the case of dangerous driving) or to WBC. Most traffic light facilities within the borough have a remote monitoring system (RMS) which will report any faults automatically. All locations will be checked to ensure that the RMS are working.

This page is intentionally left blank

Wokingham Borough Council: School Crossing Patrol Service – Consultation Report

Purpose

The report provides a summary of comments received from the 393 individual responses received to the public consultation including one objection from Twyford Parish Council in relation to the crossing patrol serving Polehampton Schools . In addition there was an E-Petition set up during the consultation entitled ‘Save our School Crossing Patrollers’ which ended in June 2018 which had 222 names attached. All comments have been considered and a summary of the key issues raised along with WBC’s response is contained at Appendix A

Background

School Crossing Patrollers (SCP) are currently employed by the Council, primarily to help children cross the road safely on their way to and from school. The service is discretionary and currently employs 7 patrollers operating where there are no formal controlled crossing facilities and one part time manager.

The council removed funding for the SCP service from the Medium Term Financial Plan agreed at Full Council in 2015. However, implementation of this decision was delayed and a special funding item funded the temporary continuation of the service.

Following a consultation last year, the decision to withdraw the service was partially implemented with the removal of 8 patrollers who had been operating at locations that already had a safe formal crossing facility i.e. Zebra or Signal Controlled crossing. This change has been successfully implemented with no recorded increase in safety issues.

A second public consultation sought views on the remaining sites where a patroller current operates before the council commit to fully implementing the 2015 decision by replacing the SCP with permanent safe crossings, such as zebra-crossings, where drivers must stop if anybody is crossing the road.

The affect sites are:

- All Saints Primary School, Norreys Avenue, Wokingham;
- St Paul’s Junior & Walter Infant Schools, Murray Road, Wokingham;
- Keep Hatch Primary School, Keephatch Road, Wokingham;
- Willow Bank Infant & Junior School, Duffield Road Woodley;
- South Lake Primary School, Nightingale Road, Woodley;
- Woodley CE Primary School, Hurricane Way, Woodley; &
- Polehampton Infant & Junior School, London Road Twyford.

In addition two sites which are currently unmanned have been assessed for the provision of a formal crossing, namely;

- Oaklands Infant & Junior School, New Wokingham Road &
- Loddon Primary, Silverdale Road

It is proposed that permanent safe crossings will be provided where possible at the above sites. It is envisaged that these will be completed by the start of the new academic year in September 2018. It is proposed that the remaining SCP service will cease at the end of this academic year (July 2018).

Consultation

The Council’s constitution requires that the council consult on the removal of the SCP service. A public consultation was therefore undertaken to seek comments on this proposal and ran from 22 January to 2 March 2018. The table below provides details of the number of responses received by the Council in response to this consultation.

School	Number of responses relating to a pupil at school (please note parents could tick more than one school)
All Saints Primary School	5 comments
St Pauls Junior School	157 comments
Walter Infant School	132 comments
Keep Hatch Primary School	103 comments
Willow Bank Junior School	13 comments
Willow Bank Infant	13 comments
South Lake Primary School	No comments
Woodley CofE Primary School	6 comments
Polehampton CofE Infant School	12 comments
Polehampton CofE Junior School	5 comments
No school	7 comments

How children of the respondents get to school				
	4 or more	3	2	1
Walk, ride, scoot with an adult	14	30	119	117
Walk, ride, scoot on their own	11	4	10	51
Are driven to school	2	5	19	17

What school year respondents’ children are in	
Foundation	98
Year one	80
Year two	63
Year three	76
Year four	73
Year five	66
Year six	70

In addition, a response from Tywford Parish Council has been received stating that “Tywford Parish Council object to the withdrawal of the school crossing patroller role”. No further comments were provided.

Appendix A below provides the key issues raised by consultees along with responses from WBC.

Summary

This consultation has generated a number of responses from concerned individuals and it is very clear that School Crossing Patroller at each of the locations are well-respected and valued members of the school community by those responding, with many commenting that the SCP provides not only assistance for those requiring to cross the road but also additional safeguarding of young road users in general.

Of the 393 individual responses received, 9 were in support of the proposals. Comments included “sounds fine as I can cross the road with my child”; “I am fine with removing SCPs if permanent crossings are in place. Parents can easily handle this”; “[I would] rather the money was spent elsewhere, parents can cross the road” & “a 24 hour road crossing would be safer”.

A large proportion of the other comments related to general road safety issues such as dangerous driving, speeding vehicles and poor parking. Issues such as these are not unique to locations near school crossing patrol sites and ultimately it is drivers’ responsibility to drive safely and legally and any concerns relating to poor driving should be reported to the police on the non-emergency number 101. Parking issues can now be dealt with directly by Wokingham Borough Council following the transfer of enforcement powers from the police last October (2017) through Civil Parking Enforcement (CPE). Parking adjacent to schools is a known concern and since the introduction of CPE, the council has prioritised visits by the enforcement officers at schools. The team is relatively small in number and it is therefore not possible to be at every school every day therefore any specific problems relating to parking can be reported to the council’s parking enforcement team. The team will then work with the schools to address these issues and target areas of concern in their patrols.

The general safety for children on their journey to school was also raised as a significant concern. However, it is ultimately the responsibility of the parent or carer to ensure their child is safe on the highway. Parents and carers play a vital part in teaching children general road safety and also how to cross the road safely. The council through its Road Safety Education Team also work with schools to provide road safety training and activities and this service will continue.

The zebra crossings and pelican (light-controlled) crossings proposed to serve the schools to replace the SCP are safer places for pedestrians to cross the road. Fundamentally, these facilities will benefit all users 24 hours a day, 7 days a week and not just at school times.

Conclusion

The council removed funding for the SCP service from the Medium Term Financial Plan agreed at Full Council in 2016. Partial withdrawal the service has been implemented with the removal of 8 patrollers last year and it is now proposed to remove the remaining 7 patrollers and to replace these with safe crossing facilities.

As the removal of the remaining SCP results in a reduction on service, the council has consulted on this proposal. The responses to the consultation have not raised any specific issues that should prevent WBC continuing with its proposal to provide safe permanent crossing at the seven locations and, following their installation, remove the

SCP from the end of the academic year, in July 2018. The SCP service is a discretionary service and whilst it is clear that the SCP is a valued member of the school community, they are only there to assist those needing assistance to cross the road. All other aspects of the journey to school is the responsibility of the parent or carer. The provision of permanent formal crossing facility will provide assistance to those crossing the road at all times including the start and end of the school day as well as outside of these hours and will therefore benefit a wider proportion of the council's residents during the full day.

As an additional note, it is recommended that all schools affected are reminded that they have access to the Council's Road Safety and MyJourney teams who can facilitate further road safety training for pupils if requested. They can also contact the CPE service to request that patrols visit the site at school opening and closing times to address unsafe parking. (include link)

Further details of our Road Safety activities can be found on the council's website at <http://www.wokingham.gov.uk/parking-road-works-and-transport/road-safety/> or by contacting the team via email road.safety@wokingham.gov.uk.

Details relating to MyJourney Wokingham can be found at the following link <http://www.myjourneywokingham.com/discover-wokingham/schools/> The web page also provides a link to a contact page should you require any additional information on or wish to access any the initiatives.

Appendix A: Summary of consultation responses and WBC response

Responses relating to All Saints Primary School	
Key Themes	WBC response
Poor driving – speeding in the area	Speed limit enforcement is a matter for Thames Valley Police and any concerns should be reported to them on the non-emergency number 101.

Responses relating to St Paul’s Junior School	
Key Themes	WBC response
The SCP is a reassuring and popular figure for parents and children and is part of school community (including comments by two teachers)	It is acknowledged that SCP are popular however, the service they have been employed to provide is to ensure children can cross safely and this would duplicate the function of the Pedestrian Crossing once installed.
The area is heavily used by traffic during school drop-off and pick-up times	Pedestrian Crossings provide safe crossing points and are appropriate for busy roads
Frequent incidence of poor driving– including cars mounting the kerbs	It is drivers’ responsibility to drive safely. The Council will ensure all lining, signing and other site aspects are present and in good condition before the start of the new school year (Sept 2018). Matters of poor driving should be directed to the police via the non-emergency number 101.
Pavements too narrow for the volume of pupils	It is parents’ and carers’ responsibility to ensure the safety of their children on journeys to and from school. SCP’s role is to ensure people can cross roads safely not to ensure safety in other areas.
There are significant incidents of poor parking in the area which increases the risk to children	Poor parking is a problem at many schools however it is the responsibility of car driver to ensure they do not park inconsiderately and it is not the responsibility of the SCP to prevent poor/illegal parking. WBC will pass on comments to its Civil Parking Enforcement contractor to investigate and see if additional patrol are required

Responses relating to Walter Infant School	
Key Themes	WBC Response
Frequent incidence of poor driving	It is drivers’ responsibility to use the highway in a safe manner and follow the laws of the road. The Council will ensure all lining, signage and other site aspects are present and in good condition before the start of the new school year (Sept 2018) When WBC takes over Civil Parking Enforcement (CPE) from the police later this year it will be able to target problem areas more effectively

There are significant incidents of poor parking in the area which increases the risk to children	Poor parking is a problem at many schools however it is the responsibility of car driver to ensure they do not park inconsiderately and it is not the responsibility of the SCP to prevent poor/illegal parking. WBC will pass on comments to its Civil Parking Enforcement contractor to investigate and see if additional patrol are required
Incidence of children running from parents and need for SCP to help 'crowd control'	It is parents' and carers responsibility to ensure the safety of their children on journeys to and from school. SCP's role is to ensure people can cross roads safely not to ensure safety in other areas.
The area is heavily used by traffic during school drop-off and pick-up times	Pedestrian Crossings provide safe crossing points and are appropriate for busy roads

Responses relating to Keephatch School	
Key themes	WBC response
The SCP is a reassuring and popular figure for parents and children and is part of the school community	It is acknowledged that SCP are popular however, the service they have been employed to provide is to ensure children can cross safely and this duplicates the function of the Pedestrian Crossing
Housing development in the area is leading to increase in traffic on local roads	Pedestrian Crossing provide safer crossing points and are appropriate for busy roads
Poor driving. Speeding is a problem in the area and police have taken action in the past	It is drivers' responsibility to drive safety and legally and any concerns should be reported to the police on the non-emergency number 101. There is a series of traffic calming in place around the school and an enforceable speed limit. Issues regarding noncompliance should be reported to the police on the non-emergency number 101.
SCP are important in encouraging children to walk to school and this may decrease if SCP are remove	There is no reason that the replacement of SCP with a permanent crossing should deter children from walking and WBC provides support to school to encourage their children to walk through Road Safety Education.

Responses relating to Willow Bank Infant and Junior School	
Poor driving – speeding	It is drivers' responsibility to drive safely and legally and any concerns should be reported to the police on the non-emergency number 101. The school is in a 20mph zone and can be enforced by the police
SCP is popular part of the school community	It is acknowledged that SCP are popular however, the service they have been employed to provide is to ensure children can cross safely and this duplicates the function of the Pedestrian Crossing

SCP are important in encouraging children to walk to school and this may decrease if SCP are remove	There is no reason that the replacement of SCP with a permanent crossing should deter children from crossing and WBC provides support to school to encourage their children to walk through Road Safety Education.
Poor parking is common and obscures views	Poor parking is a problem at many schools however it is the responsibility of car drivers to ensure they do not park inconsiderately and it is not the responsibility of the SCP to prevent poor/illegal parking. WBC will pass on comments to its Civil Parking Enforcement contractor to investigate and see if additional patrol are required

Responses relating to Woodley CofE Primary	
Key themes	WBC response
The SCP is a reassuring and popular figure for parents and children	It is acknowledged that SCP are popular however, the service they have been employed to provide is to ensure children can cross safely and would duplicate the function of the Pedestrian Crossing once installed
The area is heavily used by traffic during school drop-off and pick-up times	Pedestrian Crossings provide safe crossing points and are appropriate for busy roads
The junction where the SCP operates in complicated and it is hard to tell where traffic is going to come from	It is parents' and carers responsibility to ensure the safety of their children on journeys to and from school. The provision of a formal crossing facility will assist all users in crossing the road in the same way a SCP would. As with a school crossing patroller, there is a legal requirement for vehicles to stop ether at a red traffic signal or when a person is using a Zebra Crossing.
SCP are important in encouraging children to walk to school and this may decrease if SCP are remove	There is no reason that the replacement of SCP with a permanent crossing should deter children from walking and WBC provides support to school to encourage their children to walk

Responses relating to Polehampton CofE Infant and Junior Schools	
Key themes	WBC response
The area is heavily used by traffic during school drop-off and pick-up times	Pedestrian Crossings provide safe crossing points and are appropriate for busy roads
Some comments supported the prosed change as a safety improvement	Both SCP and permanent crossing provide safe crossing points, however permanent crossing have the advantage of being available 43/7
SCP are important in encouraging children to walk to school and this may decrease if SCP are remove	There is no reason that the replacement of SCP with a permanent crossing should deter children from walking and WBC provides support to school to encourage their children to walk

General responses (and those not relating to a school)

Poor driving – in particular drivers failing to stop and speeding

It is drivers' responsibility to drive safely. Additional speed reducing measures could be considered at individual sites if problems persist however enforcement is the responsibility of Thames Valley Police and any measure could only be introduced in collaboration with them. Any concerns should be reported to them on the non-emergency number 101.

Safe School Crossing Survey

Wokingham Borough Council is considering the future of some of its Safe School Crossing, please complete the survey below to give us your views

Q1 Which of the schools affected by the proposals do your children attend? If you do not have children at any of the schools, please go to question 4?

- All Saints Primary School
- St Paul's Junior School.....
- Walter Infant School
- Keephatch Primary School
- Willow Bank Infant School.....
- Willow Bank Junior School.....
- South Lake Primary School.....
- Woodley CofE Primary School
- Polehampton CofE Infant School
- Polehampton CofE Junior School.....

Q2 Please let us know how many of your children:

	Four or more	Three	Two	One
Walk/ride/scoot to school with an adult	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Walk/ride/scoot to school on their own	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Are driven to school	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Q3 What school years do you have children (please tick all appropriate)

- Foundation.....
- Year one.....
- Year two
- Year three
- Year four
- Year five
- Year six

Q4 Do you have any comments on the proposed changes to Safe School Crossings

Q5 Please provide an email address if you would like to be updated on the findings of this consultation

This page is intentionally left blank

WOKINGHAM BOROUGH COUNCIL JOB DESCRIPTION



Job Title: School Crossing Patrol Organiser	Job Ref: 4458											
Service: Technical Services	Section: Traffic and Road Safety											
Reports To: Road Safety Officer	Location: Shute End											
Grade: 6	Salary: £22,001 - £25,220 pro-rata for part time (£13,082 – £14,996)											
Employment Status: Permanent												
Hours of Work: Part Time – 22 hours a week. Potential flexibility during the school holidays												
<p>Service/Section Purpose: To carry out the Council's policy with regard to the management and operation of the school crossing patrol service.</p>												
<p>Job Purpose: Responsible for the management and operation of 25 school crossing patrol sites, including all the administration of the service, appointment, training and supervision of all 25 permanent part-time patrollers. The post holder could be requested to undertake additional duties that are in keeping with the nature of this post to assist the delivery of Road Safety ETP.</p>												
<p>Organisation Chart:</p> <div style="text-align: center; border: 1px solid black; padding: 10px;"> <table style="margin: auto; border-collapse: collapse;"> <tr> <td style="border: 1px solid black; padding: 5px;">GROUP MANAGER – Place and Neighbourhood Services</td> </tr> <tr> <td style="border: 1px solid black; padding: 5px; text-align: center;"> </td> </tr> <tr> <td style="border: 1px solid black; padding: 5px; text-align: center;">Head of Technical Services</td> </tr> <tr> <td style="border: 1px solid black; padding: 5px; text-align: center;"> </td> </tr> <tr> <td style="border: 1px solid black; padding: 5px; text-align: center;">Team Leader – Traffic and Road Safety</td> </tr> <tr> <td style="border: 1px solid black; padding: 5px; text-align: center;"> </td> </tr> <tr> <td style="border: 1px solid black; padding: 5px; text-align: center;">Road Safety Officer</td> </tr> <tr> <td style="border: 1px solid black; padding: 5px; text-align: center;"> </td> </tr> <tr> <td style="border: 1px solid black; padding: 5px; text-align: center;">SCHOOL CROSSING PATROL ORGANISER</td> </tr> <tr> <td style="border: 1px solid black; padding: 5px; text-align: center;"> </td> </tr> <tr> <td style="border: 1px solid black; padding: 5px; text-align: center;">25 School Crossing Patrollers (P/T)</td> </tr> </table> </div>		GROUP MANAGER – Place and Neighbourhood Services		Head of Technical Services		Team Leader – Traffic and Road Safety		Road Safety Officer		SCHOOL CROSSING PATROL ORGANISER		25 School Crossing Patrollers (P/T)
GROUP MANAGER – Place and Neighbourhood Services												
Head of Technical Services												
Team Leader – Traffic and Road Safety												
Road Safety Officer												
SCHOOL CROSSING PATROL ORGANISER												
25 School Crossing Patrollers (P/T)												

Scope

Financial Accountabilities

Budgets directly controlled (please state if this has been delegated to the post-holder) Yes, SCPS Budget

Budgets monitored on day-to-day basis: Weekly

Staff Responsibilities

Number of employees managed/supervised: 25 permanent P/T school crossing patrollers

Number of FTE (Full Time Equivalents) employees managed/supervised: See above
158.6 hrs, per week for permanent staff = 4.29 FTE

Health and Safety responsibility

In accordance with the provisions of the Health and Safety at Work Act 1974 take reasonable care for the health and safety of yourself and of other persons who may be affected by your acts or omissions at work; and co-operate with the Council to enable the Council to perform or comply with its duties under statutory health and safety provisions

Management of Physical Assets

Nature of physical assets directly controlled, (eg. children's home): None

Details of service contracts managed: Monitor budget of Wokingham Highways Alliance to conduct site visits and risk assessments.

Summary of Main Contacts.

Staff within Place & Neighbourhood Services, Human Resources, Education, Public Relations, WSP Consultants/Wokingham Highways Alliance (WHA), Schools, Exchequer Services, Members, Reprographics, Legal Services.

Head teachers at schools, (Community Centres, shops, libraries, Village Halls, Church Halls etc. in local community for advertising purposes), Members of the public, Town & Parish Councils, press, local radio and other road safety staff in Local Authorities and Thames Valley Police.

Main Tasks/Accountabilities	Approx% of working time spent
List up to ten key tasks or main accountabilities. Begin each task with an action verb. This is not intended to be an exhaustive or definitive list, You may be required to carry out other duties as required.	
Managing all out-posted patrollers (25 sites). Interviewing of problem areas. All 25 sites are visited each term.	30
Arranging advertising and publicity for recruitment, majority in local communities near vacant sites, others in local newspapers. Producing termly newsletters and talks to nursery schools.	10
Responding and rapidly resolving matters reported by patrollers, e.g. light failures, accidents, parking violations, roadworks, verbal and physical abuse – in term time. During the school holidays you will be providing additional support to the Road Safety Team	20
Liaising with members of public, Councillors, Head Teachers and other contacts regarding management of service. Correspondence and filing maintenance	15
Interviewing (in local communities), appointment, training and accompanying administration of all new patrollers and equipment provision. Ordering uniforms and stock control.	5
Recording site visits and any necessary action.	10
Organising payroll each month and dealing with payroll queries. Monitoring expenditure and liaising with Finance regarding formulation of budget.	5
Monitoring review of all sites which is being undertaken by WHA. Periodic review of sites regarding justification and times of operation (in conjunction with WHA) and site risk assessments. Responding to public requests for new sites and liaison with WHA on this	5
This is not intended to be an exhaustive or definitive list, you may be required to carry out other duties as required.	100%

PERSON SPECIFICATION

Please ensure that you read the person specification carefully as this will be used to assess candidates as part of the shortlist and interview process.

(Please indicate if the criteria are essential or desirable.)

E = Essential to carry out role to minimum required standard

D = Desirable but not essential to carry out the role

<p>Knowledge/Qualifications: (including professional body qualifications, NVQs etc and Training)</p> <ul style="list-style-type: none"> • NVQ 3 level or equivalent in a relevant area • Must be able to demonstrate team building and possess a variety of marketing skills. • Knowledge of requirements of Health & Safety at Work and Road Traffic legislation. 	<p>D E D</p>
<p>Skills/Abilities: Problem solving, creative thinking, team working, quality focus, customer service, report writing, IT skills, people management</p> <ul style="list-style-type: none"> • Effective administration and office management skills, including processing time sheets, adjustments and queries, monitoring budgets. • Effective communication and successful interviewing skills. • Ability to be caring, empathetic and motivational. • To manage a varied workforce who are all out-posted, and support and build a committed team of staff. • Proactive and innovative thinking required to develop varied forms of advertising and public and media relations. • Initiative and problem solving skills required to make immediate decisions. • Confidence and assertiveness with wide variety of people. • Customer care skills including ability to deal with aggressive behaviour on site and on the phone. • Letter and report writing skills. • Set and achieve objectives to create high standards. • Planning and initiating new policies and procedures. • Determination and persistence to make changes to improve service. 	<p>E E E E D E E E D D D D</p>
<p>Experience: type and extent. e.g. "some experience of arranging meetings, extensive experience of statistical analysis". Be specific and do not specify length of experience.</p> <ul style="list-style-type: none"> • Experience of administration and office management. • Experience of business accounts, personnel and person management skills, demonstrate successful interview and presentation skills. • Good customer care skills. • Good inter-personal communication skills and confidence with media useful. 	<p>E D E E</p>

<p>Personal Qualities: or character relevant to the job such as ability to work as part of a team, a caring attitude, a good listener, a sense of responsibility, a positive attitude etc.</p> <ul style="list-style-type: none"> • Able to work alone, in possibly dangerous situations in the Borough, and make immediate decisions. • Responsible, positive and friendly attitude. • Good communicator who can combine being a manager with building a committed team of out-posted staff. • Supportive and caring to school crossing patrol service personnel and its customers. • Energy and commitment to providing a safe crossing to and from schools and to fill vacancies for patrollers. • Proactive and innovative attitude to advertising in the Borough. 	<p>E</p> <p>E</p> <p>E</p> <p>E</p> <p>D</p>
<p>Special Factors: e.g. hold driving licence, work special hours, prepared to travel, attend evening meetings, hazardous conditions etc</p> <ul style="list-style-type: none"> • Unaccompanied early and late visits all 25 sites once a term plus others when required. • Dealing with unusual problems and situations on site including prosecutions for dangerous driving, personnel issues, training issues and coping with verbal and physical abuse to patrollers and organiser. • Willing to be on call and out-of-hours working to deal with matters at various sites. • Hold driving licence. 	<p>E</p> <p>E</p> <p>D</p> <p>E</p>

BEHAVIOUR PROFILE

	Degree Required	Behaviour Statement
Core Behaviours		
✓ Personal Effectiveness	Influences	Has the ability to influence, & shows sustained drive & determination, plans work effectively for self & others if appropriate
✓ Impact	Influences	Effectively manages their own emotions & stress, takes others views into account; actively listens to other's views & concerns & responds appropriately
✓ Resilience	Facilitates	Optimistic about change; facilitates the progress of change; analyses the impact of change on their own job & work area; remains positive & future orientated
✓ Joined Up Thinking	Facilitates	Develops appropriate networks to facilitate effective working; takes account of WBC's climate & culture; able to think beyond immediate role; appreciates the political framework WBC operates within
✓ Analysis & Judgment	Facilitates	Gathers information & facilitates sound, logical conclusions from that information; able to get the information needed without direction of manager; can articulate rationale for decisions
✓ Interpersonal	Influences	Recognises the needs of others; takes account of another's point of view; encourages loyalty from others; provides a reassuring presence
✓ Customer & Community Focus	Facilitates	Ensures Customer Service standards are consistently met
✓ Respect	Facilitates	Promotes & facilitates equality, diversity, rights & responsibilities of individuals; deals with difficult situations sensitively

Management Behaviours		
✓ Providing Direction	Acts	Sets team & individual objectives in line with WBC Vision, Values & Priorities; thinks ahead to identify problems
✓ Coaching	Acts	Adapts management style according to the individual need; encourages individual to identify solutions for

		themselves; adopts a non directive approach (where appropriate)
✓ Managing Excellence	Facilitates	Creates a quality mindset - getting things right first time; improves quality & all the tools & techniques that go with quality in service area
✓ Managing & Development Teams	Facilitates	Proactively seeks to develop their management skills; develops staff through coaching in order to drive up team performance; Recognises individual/team achievements & celebrates success

Leadership Behaviours	N/A	
✓ Positive Role Model		
✓ Leading Change		
✓ Strategic Focus		

This page is intentionally left blank

Information regarding School Crossing Patroller salaries and other expenditures e.g. uniform

139

Salaries - Total Cost			
Sum of Amount	Column Labels		
Row Labels	2017-2018	2018-2019	Grand Total
101010	3150.77		3150.77
101289	5084.86		5084.86
101361	3297.36	1137.09	4434.45
101645	2573.62	676.12	3249.74
101687	2579.68		2579.68
102114	4080.36	1403.3	5483.66
102235	8108.53		8108.53
103355	715.04		715.04
104598	1201.09		1201.09
105867	2123.05		2123.05
106385	3675.12	1258.42	4933.54
106737	4043.21	1405.18	5448.39
106738	2947.09	980.75	3927.84
111318	928.74		928.74
111530	1303.05		1303.05
111953	2122.05	1338.58	3460.63
Grand Total	47933.62	8199.44	56133.06

Salaries - Net Pay		
Employee	2017-2018	2018-2018
101010	3014.09	
101289	3515	
101361	3297.36	1,137.09
101645	2573.62	676.12
101687	2254.6	
102114	4080.36	1,403.30
102235	2786.05	
103355	715.04	
104598	1180.21	
105867	2102.17	
106385	3671.92	1,258.02
106737	3251.91	1120.77
106738	2947.09	980.75
111318	729.91	
111530	1044.67	
111953	1558.66	986.25
Grand Total	38722.66	7,562.30

Other Expenditure	
Row Labels	Sum of Amount
Catering - Refreshments	430.2
Clothing	40.9
Grand Total	471.1

This page is intentionally left blank

Equality Impact Assessment

This form is intended to assist in considering how a decision, policy, process or other change may impact on equality, therefore providing evidence of compliance with the Public Sector Equality Duty. This form is not required should the questions have been addressed through another route. Further information on this Duty and Assessment can be found on the intranet.

Name	Matthew Gould
Job Title	Service Manager, Transport and Road Safety
Service	Highways and Transport

What is the decision, policy or change?

To remove school crossing patrols (a discretionary service provision) from seven locations within the borough that operate at the start and end of the school day and in their place install permanent 24/7 formal pedestrian crossings facilities e.g. Zebra or Pelican Crossing

What outcome are you looking to achieve?

Maintain appropriately safe walking routes to local schools, whilst making best use of local authority resources and to reduce the ongoing annual revenue spend on discretionary services.

How does it hope to achieve this?

Presently there are 7 school crossing patrol officers operating within the borough to assist school children crossing the road in their journeys to school. The vast majority of primary school age children are already accompanied to school by either an adult or an older child. Where the guidance identifies that a School Crossing Patrol could be justified, the council will over provide at this location and install a permanent formal crossing

Who benefits and how (and so who does not and how)?

Both the public and the council. The council will make a budgetary saving from the revenue budgets used to provide the annual service by removing the provision of school crossing patrollers. The council will make a one off capital expenditure to provide formal crossings at each of the locations, this will provide formal crossing facilities for all members of the public to benefit from when crossing the roads at these locations. These facilities will be available 24/7 to assist all users to cross the road. During the start and end of the school day there will be minimal disruption to the school children who make use of these crossings because they are also accompanied by an adult.

What analysis has been undertaken and what evidence has been considered?

In terms of Equalities considerations, the only protected characteristics that apply are age and disability. Age is applicable as this change relates directly to children of school age, and disability as there may be young children with additional needs making use of the school crossing patrol. In both cases, these changes are unlikely to impact significantly on people with these characteristics.

Have you consulted any individuals or groups and what did this determine? If not, why has this decision been made?

The ability for people to submit comments to the Council has been provided via the Council's Consultation and Engagement Service. Within this we have asked respondents to state if they are parents of primary school age children.

What is the impact of the evidence and consultation on the proposed decision, policy or change? (e.g. stopped, amended, justified and continued).

Whilst it is expected that most parents would like to maintain all services that have a perceived role in keeping their children safe, it is also clear that in relation to traveling to and from school, primary school age children are already actively supported in doing this by adults or older children. In addition the proposed changes will only be made after there is an alternative form of crossing assistance provided i.e. Zebra or Pelican Crossing

Any other information, evidence, consideration or comments?

None



**WOKINGHAM
BOROUGH COUNCIL**

**ACORNS
PROJECTS
LIMITED**



**Duffield Road, Woodley, Wokingham
Proposed Zebra Pedestrian Crossing Facility
Combined Stage 1 and 2 Road Safety Audit**

For Wokingham Borough Council

Prepared by Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

JANUARY 2018

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants
Redwood House
3 Eaton Park
Eaton Bray
Bedfordshire
LU6 2SP

Telephone: 01525-222359
Mobile: 07860-629328
E-mail: abc@acornsprojects.com

Version No: 1.0

Document Location

Ensure that this document is current. Printed documents and locally copied files may become obsolete due to changes to the master document.

This is a controlled document. The source of the document can be found on the file server at location:

C:\Acorns\Clients\WokinghamBoroughCouncil\DuffieldRoadWoodleyWokingham\Reports\DuffieldRoadWoodleyWokinghamCombinedStage1&2RSAV1.0.doc

Revision History

This document has the following history:

Version No.	Version Date	Summary of Changes	Changes marked
1.0	31/01/2018	N/A	N/A

Approvals

This document requires the following approvals:

Name	Title
Adriano B. Cappella	Audit Team Leader
James Bedingfeld	Audit Team Member

Distribution

This document has also been distributed to:

Name	Title & Organisation
Patrick McElroy	Graduate Highways Engineer - WSP Wokingham Highway Alliance
Tim Allen	Associate - WSP Wokingham Highway Alliance

1.0 INTRODUCTION

1.1 This report results from a Combined Stage 1 and 2 Road Safety Audit carried out on the Duffield Road, Woodley, Wokingham, Proposed Zebra Pedestrian Crossing Facility, at the request of the Project Sponsor/Client Project Manager for Wokingham Borough Council, Neighbourhood Services, PO Box 153, Shute End, Wokingham, Berkshire, RG40 1WL. The Design Organisation is the WSP Wokingham Highway Alliance, who are based within the Neighbourhood Services offices in Wokingham.

1.2 The scheme proposals comprise the removal of an existing uncontrolled pedestrian crossing facility and directly adjacent, a proposed Zebra pedestrian crossing facility will be installed. The proposed Zebra pedestrian crossing facility will be sited in Duffield Road, Woodley, Wokingham, outside the existing Willow Bank Infant and Junior School.

1.3 The Audit Team membership was as follows:

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)
(Audit Team Leader) Director, Acorns Projects Limited

James Bedingfeld MCIHT
(Audit Team Member) Associate Consultant, Acorns Projects Limited

1.4 The Audit took place at the Eaton Bray office of Acorns Projects Limited during January 2018. The Audit was undertaken in accordance with the Audit Brief contained in the WSP Wokingham Highway Alliance E-Mail to Acorns Projects Limited dated the 10th January 2018. The Audit comprised an examination of the drawings provided by the Design Organisation and, are listed in Annex A.

1.5 The drawings consisted of a copy of the general arrangement, road markings, signage and surfacing arrangement and, standard details (7 No. sheets). Copies of the scheme drawings at both A3 and A4 size were provided for the Audit Team's use. Road traffic collision data, vehicular traffic flow data, and, pedestrian and pedal cycle flow data has not been provided for the purposes of this Combined Stage 1 and 2 Road Safety Audit.

- 1.6** A visit to the site was undertaken during the morning of the 30th January 2018 by both Audit Team Members together. During the morning site visit, the weather was reasonably mild, with sunny spells and, the existing carriageway surface was dry. Vehicular traffic conditions at the time of the morning site visit were observed to be light. A few pedestrians but no pedal cyclists were observed during the morning site visit.
- 1.7** The terms of reference of the Audit are as described in HD 19/15. The Audit Team has examined and reported only on the road safety implications of the scheme as presented and, has not examined or verified the compliance of the designs to any other criteria. However, to clearly explain a safety problem or the recommendation made to resolve the identified problem, the Audit Team may, on occasion, have referred to a Design Standard without touching on technical audit.
- 1.8** No Departures from Design Standards have been reported by the Design Organisation.
- 1.9** All Problems and Recommendations are referenced to the design drawings and the locations have been indicated on the A4 plan supplied for use by the Audit Team in Annex B.
- 1.10** Issues identified and observations made during this Combined Stage 1 and 2 Road Safety Audit and site inspection which the Audit Team wishes to draw to the specific attention of the Audit Project Sponsor, i.e. the Local Highway Authority, Wokingham Borough Council, will be set out in a separate letter. These issues could include maintenance items and operational issues. The Audit Team has not identified any issues during this Combined Stage 1 and 2 Road Safety Audit and site inspection that are considered to be outside the Terms of Reference.

2.0 ITEMS RAISED AT THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

2.1 GENERAL

2.1.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.2 LOCAL ALIGNMENT

2.2.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.3 JUNCTIONS

2.3.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.4 NON MOTORISED USER PROVISION

2.4.1 PROBLEM

Location 1 - The existing verge area to the immediate north west of the proposed Zebra pedestrian crossing facility in Duffield Road (Drawing No. DUFF/GA/001 Rev A).

Summary - Accommodating an existing pedestrian desire line.

The site visit has established that directly adjacent to the location of the proposed Zebra pedestrian crossing facility, a trodden path is present within the existing verge area, demonstrating that there is an existing pedestrian desire line. The trodden path leads to existing dropped kerbs, which tie into dropped kerbs on the opposite side of the vehicular access/egress for the Willow Bank Infant and Junior School.

At this Combined Stage 1 and 2 Road Safety Audit, the scheme drawings do not appear to include any proposals to accommodate the identified existing pedestrian desire line, which will undoubtedly continue when the proposed Zebra pedestrian crossing facility has been installed. Thus, full connectivity in an around the area/location of the proposed Zebra pedestrian crossing facility will not be available for pedestrians.

Concern arises that omitting or not accommodating the identified existing pedestrian desire line could result in a detriment to the operational safety for pedestrians, as any crossing movements of the vehicular access/egress for the Willow Bank Infant and Junior School will continue to be via the existing verge area and then to the location of the dropped kerbs, which are also devoid of tactile paving.

RECOMMENDATION

It is Recommended that a short length of new footway of a minimum of 1.8 metres, but preferably 2.0 metres in width, should be provided between the location of the proposed Zebra pedestrian crossing facility and the uncontrolled pedestrian crossing facility across the vehicular access/egress for the Willow Bank Infant and Junior School.

In addition, it is Recommended that the existing dropped kerbs should be checked for compliance in respect of kerb upstands (0 to 6 mm) and, that tactile paving should be installed on both sides of the vehicular access/egress for the Willow Bank Infant and Junior School, thus providing full pedestrian connectivity leading to and from the location of the proposed Zebra pedestrian crossing facility.

It may also be advisable to take up and relocate the existing yellow pedestrian guardrailing adjacent to the existing mature tree, such that it is parallel to the Duffield Road channel line. This will help ensure that having crossed the vehicular access/egress for the Willow Bank Infant and Junior School, that pedestrians will remain on the Recommended new length of footway and not stray straight into the adjacent vehicular carriageway.

2.5 ROAD SIGNS, CARRIAGEWAY MARKINGS & STREET LIGHTING

2.5.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

END OF PROBLEMS IDENTIFIED AND RECOMMENDATIONS OFFERED IN THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

3.0 AUDIT TEAM STATEMENT

We certify that this Audit has been carried out in accordance with HD 19/15.

AUDIT TEAM LEADER

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)

Director

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 1st February 2018

AUDIT TEAM MEMBER

James Bedingfield MCIHT

Associate Consultant

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 1st February 2018

ANNEX A

ANNEX A

DUFFIELD ROAD, WOODLEY, WOKINGHAM

PROPOSED ZEBRA PEDESTRIAN CROSSING FACILITY

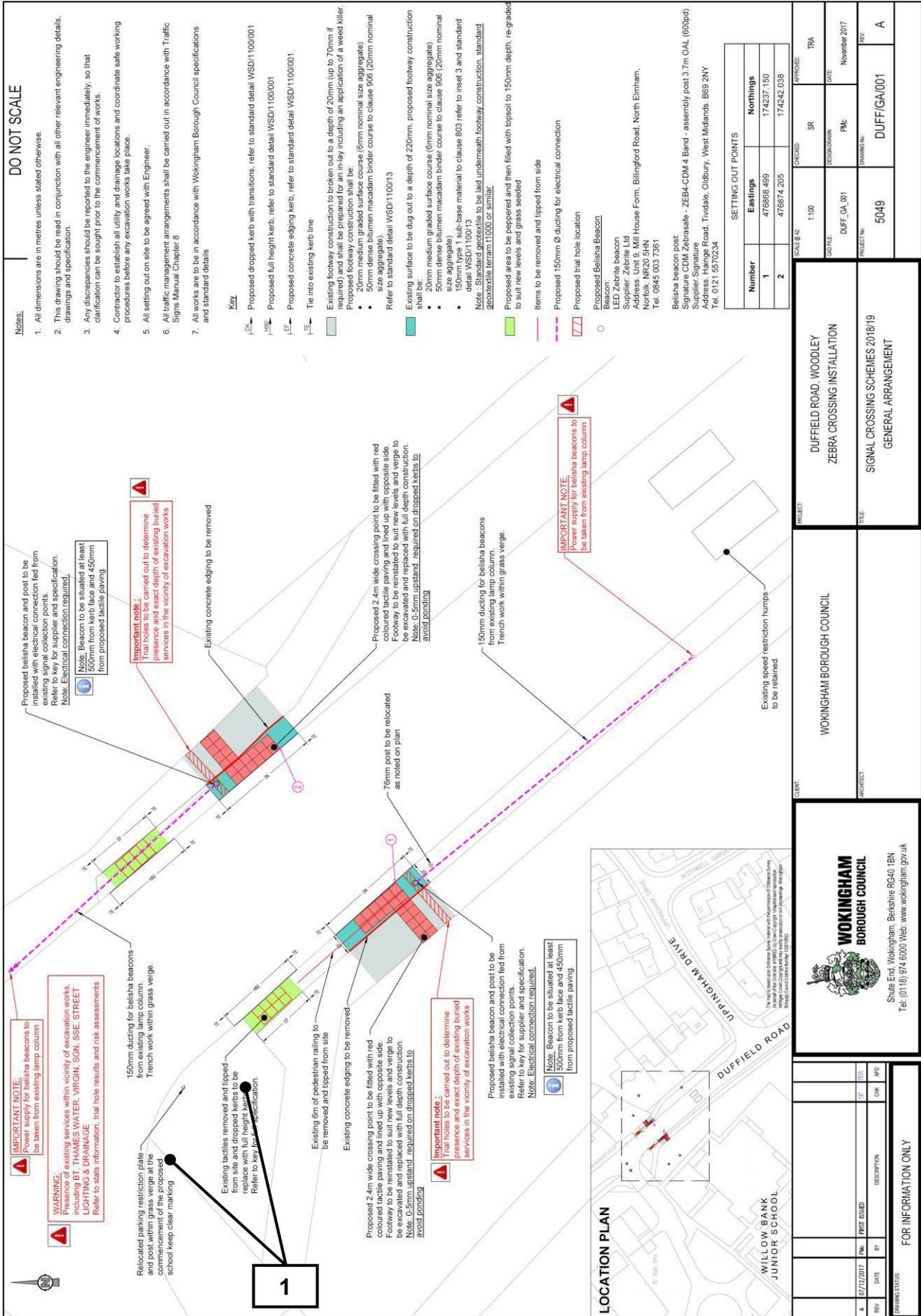
COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

LIST OF WOKINGHAM BOROUGH COUNCIL DRAWINGS SUBMITTED FOR AUDITING

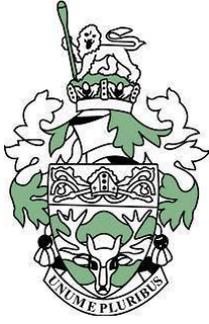
DRAWING NO.	TITLE
DUFF/GA/001 Rev A	General Arrangement
DUFF/GA/002 Rev A	Road Markings, Signage & Surfacing Arrangement
WSD/500/3 Rev B	Service Ducts
WSD/900/2 Rev B	Pavement Construction Thickness Major Access Roads & Intermediate Roads (Less than 250 commercial vehicles per day)
WSD/1100/1 Rev A	Kerbs & Channels
WSD/1100/2 Rev A	Vehicular & Pedestrian Crossovers
WSD/1100/4 Rev B	Concrete Paviers & Flags & Signal Controlled Crossing Points
WSD/1100/13 Rev B	Footways, Cycleways & Verges
WSD/1200/1 Rev B	Traffic Signs

ANNEX B

ANNEX B - PROBLEM LOCATION PLAN



This page is intentionally left blank



**WOKINGHAM
BOROUGH COUNCIL**

**ACORNS
PROJECTS
LIMITED**



**Hurricane Way, Woodley, Wokingham
Proposed Zebra Pedestrian Crossing Facility
Combined Stage 1 and 2 Road Safety Audit**

For Wokingham Borough Council

Prepared by Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

MARCH 2018

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants
Redwood House
3 Eaton Park
Eaton Bray
Bedfordshire
LU6 2SP

Telephone: 01525-222359
Mobile: 07860-629328
E-mail: abc@acornsprojects.com

Version No: 1.0

Document Location

Ensure that this document is current. Printed documents and locally copied files may become obsolete due to changes to the master document.

This is a controlled document. The source of the document can be found on the file server at location:

C:\Acorns\Clients\WokinghamBoroughCouncil\HurricaneWayWoodleyWokingham\Reports\HurricaneWayWoodleyCombinedStage1&2RSAV1.0.doc

Revision History

This document has the following history:

Version No.	Version Date	Summary of Changes	Changes marked
1.0	15/03/2018	N/A	N/A

Approvals

This document requires the following approvals:

Name	Title
Adriano B. Cappella	Audit Team Leader
David A. Bowie	Audit Team Member

Distribution

This document has also been distributed to:

Name	Title & Organisation
Tim Allen	Associate - WSP Wokingham Highway Alliance

1.0 INTRODUCTION

- 1.1 This report results from a Combined Stage 1 and 2 Road Safety Audit carried out on the Hurricane Way, Woodley, Wokingham, Proposed Zebra Pedestrian Crossing Facility Project, at the request of the Project Sponsor/Client Project Manager for Wokingham Borough Council, Neighbourhood Services, PO Box 153, Shute End, Wokingham, Berkshire, RG40 1WL. The Design Organisation is the WSP Wokingham Highway Alliance, who are based within the Neighbourhood Services offices in Wokingham.
- 1.2 The scheme proposals comprise the removal of the existing kerb build out speed reduction features at the location of an existing uncontrolled pedestrian crossing facility, which will be replaced with the installation of a new Zebra pedestrian crossing facility. The proposed Zebra pedestrian crossing facility will be sited in Hurricane Way, Woodley, Wokingham, adjacent to the existing Woodley Church of England Primary School. It is also proposed to remove the existing west bound priority give way arrangement to the immediate east of the proposed Zebra pedestrian crossing facility.
- 1.3 The Audit Team membership was as follows:
- Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)
(Audit Team Leader) Director, Acorns Projects Limited
- David A. Bowie BSc (Hons), MCIHT, (HA Certificate of Competency - Feb 2014)
(Audit Team Member) Associate Consultant, Acorns Projects Limited
- 1.4 The Audit took place at the Eaton Bray office of Acorns Projects Limited during March 2018. The Audit was undertaken in accordance with the Audit Brief contained in the WSP Wokingham Highway Alliance E-Mail to Acorns Projects Limited dated the 26th February 2018. The Audit comprised an examination of the drawings provided by the Design Organisation and, are listed in Annex A.
- 1.5 The drawings consisted of a copy of the general arrangements (2 No. sheets) and, standard details (6 No. sheets). Copies of the scheme drawings at both A3 and A4 size were provided for the Audit Team's use. Road traffic collision data, vehicular traffic flow data, and, pedestrian and pedal cycle flow data has not been provided for the purposes of this Combined Stage 1 and 2 Road Safety Audit.

- 1.6** A visit to the site was undertaken during the afternoon of the 13th March 2018 by both Audit Team Members together. During the afternoon site visit, the weather was mild, cloudy and overcast with some sunny spells and, the existing carriageway surface was dry. Vehicular traffic conditions at the time of the afternoon site visit were observed to be light to moderate. A few pedestrians and one pedal cyclist were observed during the afternoon site visit.
- 1.7** The terms of reference of the Audit are as described in HD 19/15. The Audit Team has examined and reported only on the road safety implications of the scheme as presented and, has not examined or verified the compliance of the designs to any other criteria. However, to clearly explain a safety problem or the recommendation made to resolve the identified problem, the Audit Team may, on occasion, have referred to a Design Standard without touching on technical audit.
- 1.8** No Departures from Design Standards have been reported by the Design Organisation.
- 1.9** All Problems and Recommendations are referenced to the design drawings and the locations have been indicated on the A4 plan supplied for use by the Audit Team in Annex B.
- 1.10** Issues identified or observations made during this Combined Stage 1 and 2 Road Safety Audit and site inspection which the Terms of Reference exclude from this report, but which the Audit Team wishes to draw to the specific attention of the Audit Project Sponsor, i.e. the Local Highway Authority, Wokingham Borough Council, will be set out in a separate letter. These issues could include maintenance items and operational issues. The Audit Team has not identified any issues during this Combined Stage 1 and 2 Road Safety Audit and site inspection that are considered to be outside the Terms of Reference.

2.0 ITEMS RAISED AT THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

2.1 GENERAL

2.1.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.2 LOCAL ALIGNMENT

2.2.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.3 JUNCTIONS

2.3.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.4 NON MOTORISED USER PROVISION

2.4.1 PROBLEM

Locations 1 & 2 - The northern and southern sides of Hurricane Way (Drawing No. HURR/GA/002 Rev A).

Summary - The removal of the kerb build outs will reduce the available footway areas for the anticipated platoons and numbers of parents and young school children, particularly at the end of the school day and, should parents and children be inadvertently forced into the vehicular carriageway, there could be an increased risk of vehicular/pedestrian conflicts and subsequent collisions occurring, whereby pedestrians could potentially sustain high levels of personal injury.

The scheme proposals indicate that the existing kerb build outs on both the northern and southern sides of Hurricane Way will be removed and, will be replaced with a new Zebra pedestrian crossing facility. This will result in much reduced widths of footway areas available for the anticipated platoons and numbers of parents and young school children, particularly at the end of the school day and, especially on the school side of Hurricane Way. The site visit has established that vehicular approach speeds, particularly from the west, appeared to be higher than desirable until a point where the existing kerb build outs were influencing speeds, as they were intended to do.

Concern arises that should parents and young school children be inadvertently forced into the vehicular carriageway due to the numbers of parents and school children congregating on the footway areas, there could be an increased risk of vehicular/pedestrian conflicts and subsequent vehicular/pedestrian collisions occurring, whereby parents and school children could potentially sustain high levels of personal injury. This issue is likely to be exacerbated by the potential increase in vehicle speeds resulting from the removal of the existing traffic calming features.

RECOMMENDATION

It is Recommended that the existing kerb build out on the northern side of Hurricane Way, i.e., outside the school, should be retained in order to accommodate the anticipated platoons and numbers of parents and young school children congregating on the footway, prior to attempting to cross the Hurricane Way carriageway.

It is also Recommended that an increased area of footway/landing pad on the southern side of Hurricane Way should be constructed by utilising the existing verge area identified during the site visit, together with the retention of the existing give way arrangement for west bound vehicular traffic. The retention of both the kerb build out on the northern side of Hurricane Way and the give way arrangement for west bound vehicular traffic will continue to influence vehicular speeds where most needed at the location of the proposed Zebra pedestrian crossing facility.

Alternatively, should it be ascertained that wider footways or landing pads cannot be retained or provided as part of a proposed Zebra pedestrian crossing layout, it is Recommended that the existing layout should be retained, particularly as the crossing width is much narrower and, that the existing vehicular speed reduction measures comprising the kerb build outs and the west bound give way arrangement will continue to significantly influence vehicular speeds past the crossing point.

2.5 ROAD SIGNS, CARRIAGEWAY MARKINGS & STREET LIGHTING

2.5.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

END OF PROBLEMS IDENTIFIED AND RECOMMENDATIONS OFFERED IN THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

3.0 AUDIT TEAM STATEMENT

We certify that this Audit has been carried out in accordance with HD 19/15.

AUDIT TEAM LEADER

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)

Director

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 28th March 2018

AUDIT TEAM MEMBER

David A. Bowie BSc (Hons), MCIHT, (HA Certificate of Competency - Feb 2014)

Associate Consultant

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 28th March 2018

ANNEX A

ANNEX A

HURRICANE WAY, WOODLEY, WOKINGHAM

PROPOSED ZEBRA PEDESTRIAN CROSSING FACILITY

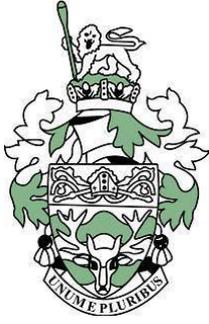
COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

LIST OF WOKINGHAM BOROUGH COUNCIL DRAWINGS SUBMITTED FOR AUDITING

HURR/GA/001 Rev A	General Arrangement Sheet 1 of 2
HURR/GA/002 Rev A	General Arrangement Sheet 2 of 2
WSD/500/3 Rev B	Service Ducts
WSD/500/13 Rev B	Jointing Chambers For Traffic Signal & Street Lighting Cables
WSD/1100/2 Rev A	Vehicular & Pedestrian Crossovers
WSD/1100/4 Rev B	Concrete Pavements & Flags & Signal Controlled Crossing Points
WSD/1100/13 Rev B	Footways, Cycleways & Verges
WSD/1200/1 Rev B	Traffic Signs

ANNEX B

This page is intentionally left blank



**WOKINGHAM
BOROUGH COUNCIL**

**ACORNS
PROJECTS
LIMITED**



**Keephatch Road, Wokingham
Proposed Zebra Pedestrian Crossing Facility
Combined Stage 1 and 2 Road Safety Audit**

**For Wokingham Borough Council
Prepared by Acorns Projects Limited
Safety Traffic Project Management & Highway Engineering Consultants**

MARCH 2018

Acorns Projects Limited
Safety Traffic Project Management & Highway Engineering Consultants
Redwood House
3 Eaton Park
Eaton Bray
Bedfordshire
LU6 2SP

Telephone: 01525-222359
Mobile: 07860-629328
E-mail: abc@acornsprojects.com

Version No: 1.0

Document Location

Ensure that this document is current. Printed documents and locally copied files may become obsolete due to changes to the master document.

This is a controlled document. The source of the document can be found on the file server at location:

C:\Acorns\Clients\WokinghamBoroughCouncil\KeephatchRoadWokingham\Reports\KeephatchRoadWokinghamCombinedStage1&2RSAV1.0.doc

Revision History

This document has the following history:

Version No.	Version Date	Summary of Changes	Changes marked
1.0	01/03/2018	N/A	N/A

Approvals

This document requires the following approvals:

Name	Title
Adriano B. Cappella	Audit Team Leader
James Bedingfeld	Audit Team Member

Distribution

This document has also been distributed to:

Name	Title & Organisation
Tim Allen	Associate - WSP Wokingham Highway Alliance

1.0 INTRODUCTION

1.1 This report results from a Combined Stage 1 and 2 Road Safety Audit carried out on the Keephatch Road, Wokingham, Proposed Zebra Pedestrian Crossing Facility Project, at the request of the Project Sponsor/Client Project Manager for Wokingham Borough Council, Neighbourhood Services, PO Box 153, Shute End, Wokingham, Berkshire, RG40 1WL. The Design Organisation is the WSP Wokingham Highway Alliance, who are based within the Neighbourhood Services offices in Wokingham.

1.2 The scheme proposals comprise the removal of an existing uncontrolled pedestrian crossing facility in the form of dropped kerbs, tactile paving and a pedestrian refuge island, and immediately to the north, a proposed Zebra pedestrian crossing facility will be installed. The proposed Zebra pedestrian crossing facility will be sited in Keephatch Road, Wokingham, to the immediate north of the Champion Way/Hughes Road/Keephatch Road roundabout.

1.3 The Audit Team membership was as follows:

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)
(Audit Team Leader) Director, Acorns Projects Limited

James Bedingfeld MCIHT
(Audit Team Member) Associate Consultant, Acorns Projects Limited

1.4 The Audit took place at the Eaton Bray office of Acorns Projects Limited during January and March 2018. The Audit was undertaken in accordance with the Audit Brief contained in the WSP Wokingham Highway Alliance E-Mail to Acorns Projects Limited dated the 18th January 2018. The Audit comprised an examination of the drawings provided by the Design Organisation and, are listed in Annex A.

1.5 The drawings consisted of a copy of the general arrangement, road markings and traffic signs arrangement, setting out arrangement, tracking arrangement and, standard details (6 No. sheets). Copies of the scheme drawings at both A3 and A4 size were provided for the Audit Team's use. Road traffic collision data, vehicular traffic flow data, and, pedestrian and pedal cycle flow data has not been provided for the purposes of this Combined Stage 1 and 2 Road Safety Audit.

- 1.6** A visit to the site was undertaken during the afternoon of the 30th January 2018 by both Audit Team Members together. During the afternoon site visit, the weather was reasonably mild, with sunny spells and, the existing carriageway surface was dry. Vehicular traffic conditions at the time of the afternoon site visit were observed to be light. Two pedestrians but no pedal cyclists were observed during the afternoon site visit.
- 1.7** The terms of reference of the Audit are as described in HD 19/15. The Audit Team has examined and reported only on the road safety implications of the scheme as presented and, has not examined or verified the compliance of the designs to any other criteria. However, to clearly explain a safety problem or the recommendation made to resolve the identified problem, the Audit Team may, on occasion, have referred to a Design Standard without touching on technical audit.
- 1.8** No Departures from Design Standards have been reported by the Design Organisation.
- 1.9** All Problems and Recommendations are referenced to the design drawings and the locations have been indicated on the A4 plan supplied for use by the Audit Team in Annex B.
- 1.10** Issues identified and observations made during this Combined Stage 1 and 2 Road Safety Audit and site inspection which the Audit Team wishes to draw to the specific attention of the Audit Project Sponsor, i.e. the Local Highway Authority, Wokingham Borough Council, will be set out in a separate letter. These issues could include maintenance items and operational issues. In this regard, the Audit Team have made reference to one issue identified and observation made as referred to in a Covering Letter to the Design Organisation, WSP Wokingham Highway Alliance, dated the 14th March 2018. This Covering Letter should be supplied to the Audit Project Sponsor, i.e. the Local Highway Authority, Wokingham Borough Council and, be considered in conjunction with this Combined Stage 1 and 2 Road Safety Audit Report.

2.0 ITEMS RAISED AT THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

2.1 GENERAL

2.1.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.2 LOCAL ALIGNMENT

2.2.1 PROBLEM

Location 1 - The eastern side of Keephatch Road (Drawing No. KEEP/GA/001 Rev A).

Summary - The existing dense vegetation on the eastern side of Keephatch Road could, in the future scenario, impact upon the intervisibility between south bound vehicles and pedestrians seeking to cross from the eastern to the western side of Keephatch Road.

The scheme proposals indicate that in order to improve forward visibility to the proposed Zebra crossing facility for Keephatch Road south bound drivers, the existing dense vegetation on the eastern side of the carriageway will be cut back. Whilst this may be effective in the short term, it is reasonable to suggest that with the passing of time and following numerous annual growth seasons, the vegetation may grow back to the levels identified during the site visit.

Concern arises that in the future scenario, the vegetation could impact upon intervisibility between south bound drivers and pedestrians seeking to cross from the eastern to the western side of Keephatch Road. This situation could result in an increased risk of vehicular/pedestrian collisions occurring, whereby pedestrians could potentially sustain a high level of personal injury, particularly those pedestrians who may be blind or visually impaired.

RECOMMENDATION

It is Recommended that the identified existing dense vegetation should be completely removed, to a point where a suitable and appropriate permanent forward visibility envelope for a 30 mph speed limit/design speed will be provided, thus ensuring that intervisibility between vehicles and pedestrians will not be impacted upon in the future scenario. In adopting this Recommendation, it should be remembered that cutting back the dense vegetation on a cyclic basis will also no longer be an on-going maintenance liability for the Highway Authority.

2.2.2 PROBLEM

Location 2 - The Keephatch Road north bound approach to the proposed Zebra pedestrian crossing facility (Drawing Nos. KEEP/GA/001 Rev A & KEEP/GA/002 Rev A).

Summary - The close proximity of the proposed Zebra pedestrian crossing facility to the north bound exit from the Campion Way/Hughes Road/Keephatch Road roundabout could result in an increased risk of nose to tail shunt type collisions occurring between a leading and any following vehicles within the circulatory area of the roundabout.

The scheme proposals indicate that a new Zebra pedestrian crossing facility will be installed on the Keephatch Road northern arm of the Campion Way/Hughes Road/Keephatch Road roundabout. The proposed Zebra pedestrian crossing facility will be sited in very close proximity to the inscribed circle diameter of the roundabout. The site visit has established that drivers entering the circulatory area of the roundabout from Hughes Road, intending to travel in the north bound Keephatch Road direction, were paying little or no attention to that which may be occurring on the Keephatch Road northern arm of the roundabout. A drivers focus was seen to be dominated by seeking an opportunity to enter the roundabout circulatory area of the roundabout, by almost exclusively looking only to the right.

Concern arises that this situation could result in drivers accelerating away from Hughes Road into the Keephatch Road northern arm of the roundabout, and then being suddenly faced with the presence of a stationary vehicle as drivers allow pedestrians to cross from the western side to the eastern side of the carriageway. This situation could result in an increased risk of nose to tail shunt type collisions occurring, between a leading and any following vehicles.

RECOMMENDATION

It is Recommended that ideally and if possible, the proposed Zebra pedestrian crossing facility should be sited further to the north. As this will result in the crossing being slightly off the ideal pedestrian desire line, visi-rail guard railing panels may be required around the kerblines from Campion Way and Hughes Road into the Keephatch Road northern arm of the roundabout, in order to physically guide and encourage pedestrians to utilise the designated Zebra crossing point.

In addition, it is Recommended that red high friction surfacing should be applied between the inscribed circle diameter of the roundabout and the Zebra crossing stop line as opposed to the charcoal coloured high friction surfacing currently indicated, as this will visually enhance the presence of the proposed Zebra pedestrian crossing facility.

In further mitigation of the potential need for drivers to have to brake suddenly or heavily, the charcoal coloured high friction surfacing should be extended and also be applied within the inscribed circle diameter of the roundabout, for a suitable and appropriate distance and area.

2.3 JUNCTIONS

2.3.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.4 NON MOTORISED USER PROVISION

2.4.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.5 ROAD SIGNS, CARRIAGEWAY MARKINGS & STREET LIGHTING

2.5.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

END OF PROBLEMS IDENTIFIED AND RECOMMENDATIONS OFFERED IN THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

3.0 AUDIT TEAM STATEMENT

We certify that this Audit has been carried out in accordance with HD 19/15.

AUDIT TEAM LEADER

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)

Director

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 14th March 2018

AUDIT TEAM MEMBER

James Bedingfeld MCIHT

Associate Consultant

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 14th March 2018

ANNEX A

ANNEX A

KEEPHATCH ROAD, WOKINGHAM

PROPOSED ZEBRA PEDESTRIAN CROSSING FACILITY

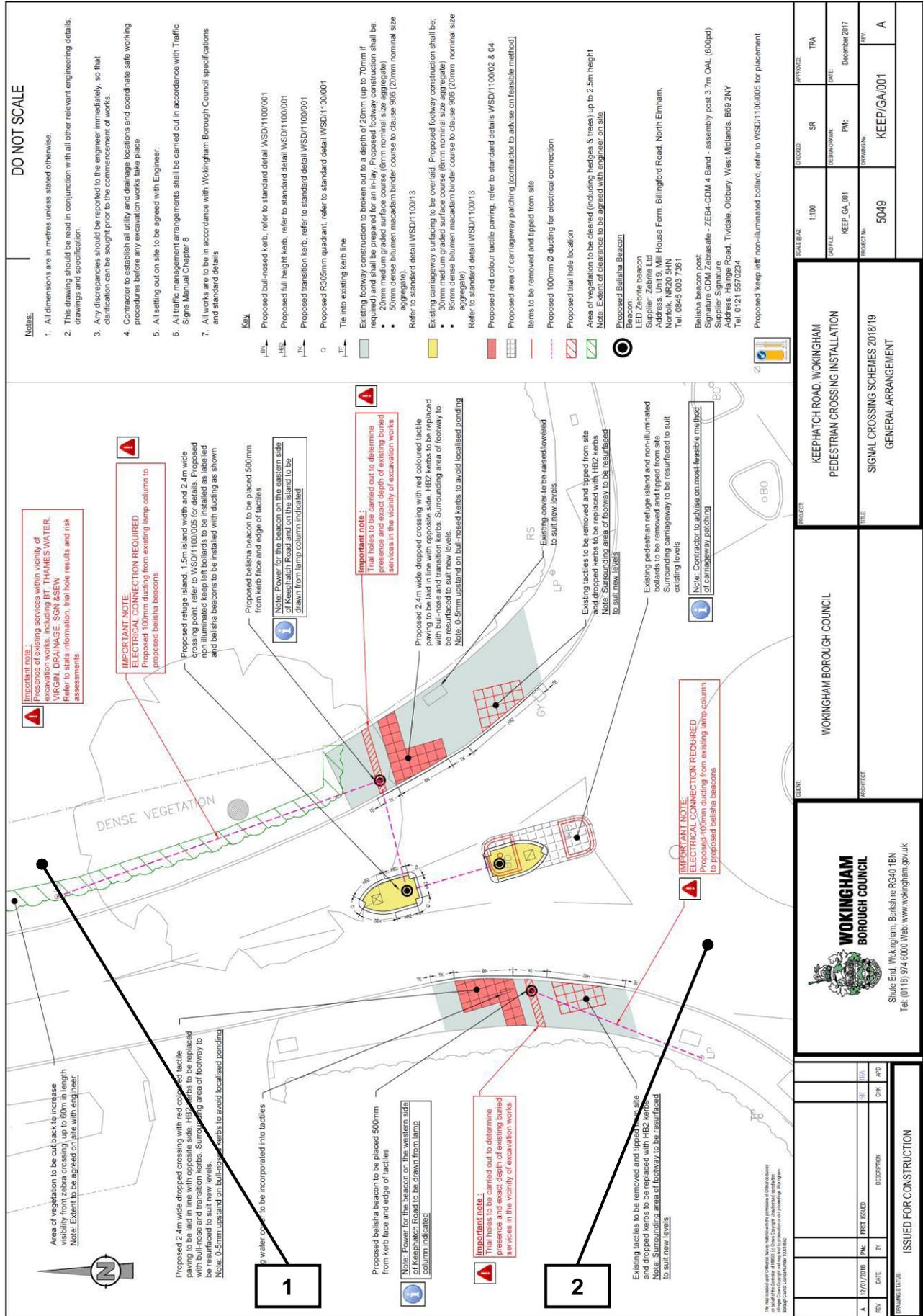
COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

LIST OF WOKINGHAM BOROUGH COUNCIL DRAWINGS SUBMITTED FOR AUDITING

DRAWING NO.	TITLE
KEEP/GA/001 Rev A	General Arrangement
KEEP/GA/002 Rev A	Road Markings & Traffic Signs Arrangement
KEEP/SO/001 Rev A	Setting Out Arrangement
KEEP/TR/001 Rev A	Tracking Arrangement
WSD/500/3 Rev B	Service Ducts
WSD/1100/1 Rev A	Kerbs & Channels
WSD/1100/2 Rev A	Vehicular & Pedestrian Crossovers
WSD/1100/4 Rev B	Concrete Paviers & Flags & Signal Controlled Crossing Points
WSD/1100/13 Rev B	Footways, Cycleways & Verges
WSD/1200/1 Rev B	Traffic Signs

ANNEX B

ANNEX B - PROBLEM LOCATION PLAN





**WOKINGHAM
BOROUGH COUNCIL**

**ACORNS
PROJECTS
LIMITED**



**London Road, Twyford, Wokingham
Proposed Puffin Pedestrian Crossing Facility
Combined Stage 1 and 2 Road Safety Audit**

**For Wokingham Borough Council
Prepared by Acorns Projects Limited
Safety Traffic Project Management & Highway Engineering Consultants**

FEBRUARY 2018

Acorns Projects Limited
Safety Traffic Project Management & Highway Engineering Consultants
Redwood House
3 Eaton Park
Eaton Bray
Bedfordshire
LU6 2SP

Telephone: 01525-222359
Mobile: 07860-629328
E-mail: abc@acornsprojects.com

Version No: 1.0

Document Location

Ensure that this document is current. Printed documents and locally copied files may become obsolete due to changes to the master document.

This is a controlled document. The source of the document can be found on the file server at location:

C:\Acorns\Clients\WokinghamBoroughCouncil\LondonRoadTwyfordWokingham\Reports\LondonRoadTwyfordWokinghamCombinedStage1&2RSAV1.0.doc

Revision History

This document has the following history:

Version No.	Version Date	Summary of Changes	Changes marked
1.0	05/02/2018	N/A	N/A

Approvals

This document requires the following approvals:

Name	Title
Adriano B. Cappella	Audit Team Leader
James Bedingfeld	Audit Team Member

Distribution

This document has also been distributed to:

Name	Title & Organisation
Patrick McElroy	Graduate Highways Engineer - WSP Wokingham Highway Alliance
Tim Allen	Associate - WSP Wokingham Highway Alliance

1.0 INTRODUCTION

1.1 This report results from a Combined Stage 1 and 2 Road Safety Audit carried out on the London Road, Twyford, Wokingham, Proposed Puffin Pedestrian Crossing Facility, at the request of the Project Sponsor/Client Project Manager for Wokingham Borough Council, Neighbourhood Services, PO Box 153, Shute End, Wokingham, Berkshire, RG40 1WL. The Design Organisation is the WSP Wokingham Highway Alliance, who are based within the Neighbourhood Services offices in Wokingham.

1.2 The scheme proposals comprise the installation of a new Puffin pedestrian crossing facility, which will replace an existing pedestrian refuge only arrangement. The proposed Puffin pedestrian crossing facility will be sited in London Road, Twyford, Wokingham, in immediate proximity to the existing Polehampton Infant School.

1.3 The Audit Team membership was as follows:

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)
(Audit Team Leader) Director, Acorns Projects Limited

James Bedingfeld MCIHT
(Audit Team Member) Associate Consultant, Acorns Projects Limited

1.4 The Audit took place at the Eaton Bray office of Acorns Projects Limited during January and February 2018. The Audit was undertaken in accordance with the Audit Brief contained in the WSP Wokingham Highway Alliance E-Mail to Acorns Projects Limited dated the 25th January 2018. The Audit comprised an examination of the drawings provided by the Design Organisation and, are listed in Annex A.

1.5 The drawings consisted of a copy of the general arrangement, road markings and traffic signs arrangement, traffic signals specification and, standard details (8 No. sheets). Copies of the scheme drawings at both A3 and A4 size were provided for the Audit Team's use. Road traffic collision data, vehicular traffic flow data, and, pedestrian and pedal cycle flow data has not been provided for the purposes of this Combined Stage 1 and 2 Road Safety Audit.

- 1.6** A visit to the site was undertaken during the early afternoon of the 30th January 2018 by both Audit Team Members together. During the early afternoon site visit, the weather was reasonably mild, with sunny spells and, the existing carriageway surface was dry. Vehicular traffic conditions at the time of the early afternoon site visit were observed to be light to moderate. A reasonable number of pedestrians travelling to and from the nearby Ruscombe Road were observed during the early afternoon site visit. No pedal cyclists were observed during the early afternoon site visit.
- 1.7** The terms of reference of the Audit are as described in HD 19/15. The Audit Team has examined and reported only on the road safety implications of the scheme as presented and, has not examined or verified the compliance of the designs to any other criteria. However, to clearly explain a safety problem or the recommendation made to resolve the identified problem, the Audit Team may, on occasion, have referred to a Design Standard without touching on technical audit.
- 1.8** No Departures from Design Standards have been reported by the Design Organisation.
- 1.9** All Problems and Recommendations are referenced to the design drawings and the locations have been indicated on the A4 plan supplied for use by the Audit Team in Annex B.
- 1.10** Issues identified and observations made during this Combined Stage 1 and 2 Road Safety Audit and site inspection which the Audit Team wishes to draw to the specific attention of the Audit Project Sponsor, i.e. the Local Highway Authority, Wokingham Borough Council, will be set out in a separate letter. These issues could include maintenance items and operational issues. The Audit Team has not identified any issues during this Combined Stage 1 and 2 Road Safety Audit and site inspection that are considered to be outside the Terms of Reference.

2.0 ITEMS RAISED AT THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

2.1 GENERAL

2.1.1 PROBLEM

Location 1 - The proposed Puffin pedestrian crossing facility (Drawing Nos. LON/GA/001 Rev A & LON/TS/001 Rev A).

Summary - No provision for a traffic signal maintenance engineer to stop in immediate proximity to the proposed Puffin pedestrian crossing facility.

The scheme proposals include the installation of a new Puffin pedestrian crossing facility, which will replace the existing pedestrian refuge island. At this Combined Stage 1 and 2 Road Safety Audit, the scheme proposals do not appear to include the provision of a traffic signal maintenance engineer's layby or hardstanding area adjacent to the proposed Puffin pedestrian crossing facility. It will be important to ensure that an adequate and suitable facility is provided for the safety and benefit of site operatives, otherwise there could be a potential increased risk of vehicular/site operative collisions occurring, whereby site operatives are likely to sustain personal injury.

RECOMMENDATION

It is Recommended that a traffic signal maintenance engineer's layby or hardstanding area should be provided in proximity to the proposed Puffin pedestrian crossing facility. In determining a suitable location, the facility should be sited clear of the traffic lanes, traffic signal poles and signal heads and, any existing or proposed pedestrian and pedal cycle routes if possible.

2.1.2 PROBLEM

Locations 2 & 3 - The London Road north east bound and south west bound approaches to the proposed Puffin pedestrian crossing facility (Drawing Nos. LON/GA/001 Rev A & LON/TS/001 Rev A).

Summary - Reducing the potential for skidding and enhancing braking capacity on the north east bound and south west bound approaches to the proposed Puffin pedestrian crossing facility.

The scheme proposals indicate that new Puffin pedestrian crossing facility will be introduced within the London Road carriageway, which will replace the existing pedestrian refuge only arrangement. In operational terms, the introduction of the new Puffin pedestrian crossing facility and the respective stop lines will result in the creation of new and potentially heavy braking areas on the north east bound and south west bound approaches to the new Puffin pedestrian crossing facility.

As a result, concern arises that a high skid resistant surface course material (formerly referred to as the wearing course), may be required on the north east bound and south west bound approaches to the new Puffin pedestrian crossing facility, particularly as the site visit has established that the existing London Road carriageway on the north east bound and south west bound approaches to the new Puffin pedestrian crossing facility is in a very poor condition, with evidence of structural issues, crazing, potholes, poor trench reinstatements and patch repairs.

Whilst motorists should always be anticipating the unexpected, sudden or late braking may be required by motorists, particularly those who may be unfamiliar with the area, which could result in an increased risk of potential stop line overshoots and subsequent potential collisions occurring with pedestrians crossing at the new Puffin pedestrian crossing facility. This could result in pedestrians sustaining potentially high levels of personal injury, particularly those pedestrians who may be blind or visually impaired.

In addition, there could be a potential increased risk of nose to tail shunt type collisions occurring between a leading and any following vehicles on the immediate approaches to the new Puffin pedestrian crossing facility stop lines.

RECOMMENDATION

It is Recommended that at the construction stage of the project, a high skid resistant surface course material (formerly referred to as the wearing course), should be provided on the north east bound and south west bound approaches to the new Puffin pedestrian crossing facility, which should include the alignment where pedestrians will actually be crossing as surfacing defects are also evident in this area.

It is suggested that as a minimum, the length of high skid resistant surface course material (formerly referred to as the wearing course), should draw a relationship between the potential vehicular approach speeds and, the minimum stopping distances identified in the Highway Code, which confirms a shortest/overall stopping distance for a 30 mph limit as a linear distance of 75 feet or approximately 23 metres.

The shortest/overall stopping distances assume dry road conditions, a car with good brakes and an alert driver. It is acknowledged that generally, a longer linear distance is or can be provided in these circumstances.

Alternatively, it is Recommended that the existing carriageway surface is the subject of suitable and adequate repairs prior to the application of a traditional red high friction anti-skid surface material. However, it should be noted that the Audit Team do have reservations regarding this lifespan of this Recommendation, due to the significant levels of existing carriageway surface defects that would be present beneath a traditional red high friction anti-skid surface application.

2.2 LOCAL ALIGNMENT

2.2.1 PROBLEM

Locations 4, 5 & 6 - The London Road north east bound and south west bound approaches to the proposed Puffin pedestrian crossing facility (Drawing Nos. LON/GA/001 Rev A & LON/TS/001 Rev A).

Summary - The presence of the existing on-street parking bays (when occupied), the existing on-carriageway bus cage (when occupied), and, the existing overhanging vegetation are likely to result in a restriction or limited forward visibility to the nearside primary traffic signal heads. This could result in an increased risk of stop line overshoots and subsequent vehicular/pedestrian collisions occurring when traffic signal changes take place or, in addition, an increased risk of nose to tail collisions occurring between a leading and any following vehicles on the approach to the proposed Puffin pedestrian crossing facility stop lines.

The scheme proposals indicate that new Puffin pedestrian crossing facility will be introduced within the London Road carriageway, which will replace the existing pedestrian refuge only arrangement. The site visit has established the presence existing on-street parking bays, an existing on-carriageway bus cage and, the existing overhanging vegetation as indicated at Locations 4, 5 and 6 respectively.

When occupied, the existing on-street parking bays and the existing on-carriageway bus cage are likely to result in a restriction or limited forward visibility to the nearside primary traffic signal heads. In addition, the presence of the existing overhanging vegetation is also likely to result in a restriction or limited forward visibility to the nearside primary traffic signal head.

As a result, and whilst the Audit Team acknowledge the presence of central primary traffic signal heads which will be erected within the traffic separation islands in the centre of the carriageway, concern nevertheless arises that there could be a potential increased risk of stop line overshoots occurring when signal changes take place, with subsequent vehicular/pedestrian collisions occurring at the new Puffin crossing facility. This could result in pedestrians potentially sustaining high levels of personal injury, particularly those pedestrians who may be blind or visually impaired.

In addition, there could be a potential increased risk of nose to tail shunt type collisions occurring between a leading and any following vehicles on the immediate approaches to the new Puffin pedestrian crossing facilities stop lines when signal changes take place.

RECOMMENDATION

It is Recommended that at 1.05 metres above carriageway level, the maximum geometric stopping sight distances that can be achieved on the north east bound and south west bound approaches to the nearside primary traffic signal heads should be accurately established and confirmed accordingly.

Should the maximum geometric stopping sight distance achievable on the north east bound and south west bound approaches to the primary traffic signal heads be established as being sub-standard, it is Recommended that the linear length of the existing on-street parking bays should be truncated and, that the existing on-carriageway bus cage should be relocated, in order to improve a drivers forward visibility to the nearside primary traffic signal heads.

It is also Recommended that the identified overhanging vegetation at Location 6 should be cut right back or completely removed as necessary/appropriate, in order to improve a drivers forward visibility to the nearside primary traffic signal head.

2.3 JUNCTIONS

2.3.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.4 NON MOTORISED USER PROVISION

2.4.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.5 ROAD SIGNS, CARRIAGEWAY MARKINGS & STREET LIGHTING

2.5.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

END OF PROBLEMS IDENTIFIED AND RECOMMENDATIONS OFFERED IN THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

3.0 AUDIT TEAM STATEMENT

We certify that this Audit has been carried out in accordance with HD 19/15.

AUDIT TEAM LEADER

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)

Director

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 14th February 2018

AUDIT TEAM MEMBER

James Bedingfeld MCIHT

Associate Consultant

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 14th February 2018

ANNEX A

ANNEX A

LONDON ROAD, TWYFORD, WOKINGHAM

PROPOSED PUFFIN PEDESTRIAN CROSSING FACILITY

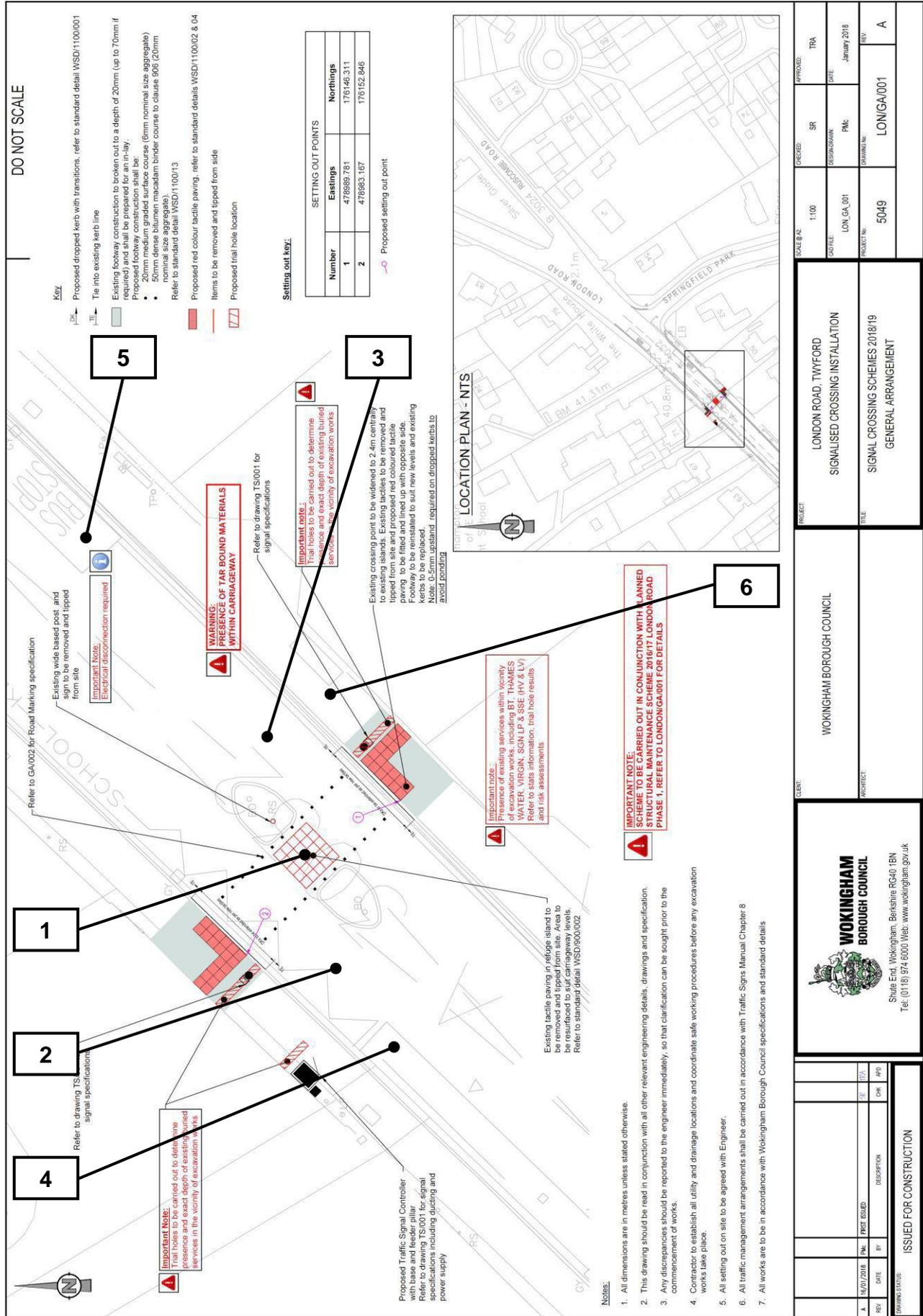
COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

LIST OF WOKINGHAM BOROUGH COUNCIL DRAWINGS SUBMITTED FOR AUDITING

DRAWING NO.	TITLE
LON/GA/001 Rev A	General Arrangement
LON/GA/002 Rev A	Road Markings & Traffic Signs Arrangement
LON/TS/001 Rev A	Traffic Signals Specification
WSD/500/3 Rev B	Service Ducts
WSD/500/13 Rev B	Jointing Chambers for Traffic Signal and Street Lighting Cables
WSD/900/2 Rev B	Pavement Construction Thickness Major Access Roads & Intermediate Roads (Less than 250 commercial vehicles per day)
WSD/1100/1 Rev A	Kerbs & Channels
WSD/1100/2 Rev A	Vehicular & Pedestrian Crossovers
WSD/1100/4 Rev B	Concrete Pavements & Flags & Signal Controlled Crossing Points
WSD/1100/13 Rev B	Footways, Cycleways & Verges
WSD/1200/1 Rev B	Traffic Signs

ANNEX B

ANNEX B - PROBLEM LOCATION PLAN





**WOKINGHAM
BOROUGH COUNCIL**

**ACORNS
PROJECTS
LIMITED**



**Murray Road, Wokingham
Proposed Zebra Pedestrian Crossing Facility
Combined Stage 1 and 2 Road Safety Audit**

**For Wokingham Borough Council
Prepared by Acorns Projects Limited
Safety Traffic Project Management & Highway Engineering Consultants**

FEBRUARY 2018

Acorns Projects Limited
Safety Traffic Project Management & Highway Engineering Consultants
Redwood House
3 Eaton Park
Eaton Bray
Bedfordshire
LU6 2SP

Telephone: 01525-222359
Mobile: 07860-629328
E-mail: abc@acornsprojects.com

Version No: 1.0

Document Location

Ensure that this document is current. Printed documents and locally copied files may become obsolete due to changes to the master document.

This is a controlled document. The source of the document can be found on the file server at location:

C:\Acorns\Clients\WokinghamBoroughCouncil\MurrayRoadWokingham\Reports\MurrayRoadWokinghamCombinedStage1&2RSAV1.0.doc

Revision History

This document has the following history:

Version No.	Version Date	Summary of Changes	Changes marked
1.0	16/02/2018	N/A	N/A

Approvals

This document requires the following approvals:

Name	Title
Adriano B. Cappella	Audit Team Leader
James Bedingfeld	Audit Team Member

Distribution

This document has also been distributed to:

Name	Title & Organisation
Patrick McElroy	Graduate Highways Engineer - WSP Wokingham Highway Alliance
Tim Allen	Associate - WSP Wokingham Highway Alliance

1.0 INTRODUCTION

1.1 This report results from a Combined Stage 1 and 2 Road Safety Audit carried out on the Murray Road, Wokingham, Proposed Zebra Pedestrian Crossing Facility Project, at the request of the Project Sponsor/Client Project Manager for Wokingham Borough Council, Neighbourhood Services, PO Box 153, Shute End, Wokingham, Berkshire, RG40 1WL. The Design Organisation is the WSP Wokingham Highway Alliance, who are based within the Neighbourhood Services offices in Wokingham.

1.2 The scheme proposals comprise the installation of a new Zebra pedestrian crossing facility, on an existing raised table speed reduction feature. The proposed Zebra pedestrian crossing facility will be sited in Murray Road, Wokingham, in immediate proximity to the existing Walter Infant School and, the existing St. Paul's Junior School.

1.3 The Audit Team membership was as follows:

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)
(Audit Team Leader) Director, Acorns Projects Limited

James Bedingfeld MCIHT
(Audit Team Member) Associate Consultant, Acorns Projects Limited

1.4 The Audit took place at the Eaton Bray office of Acorns Projects Limited during February 2018. The Audit was undertaken in accordance with the Audit Brief contained in the WSP Wokingham Highway Alliance E-Mail to Acorns Projects Limited dated the 8th February 2018. The Audit comprised an examination of the drawings provided by the Design Organisation and, are listed in Annex A.

1.5 The drawings consisted of a copy of the general arrangement, road markings and traffic signs arrangement and, standard details (6 No. sheets). Copies of the scheme drawings at both A3 and A4 size were provided for the Audit Team's use. Road traffic collision data, vehicular traffic flow data, and, pedestrian and pedal cycle flow data has not been provided for the purposes of this Combined Stage 1 and 2 Road Safety Audit.

1.6 A visit to the site was undertaken during the afternoon of the 13th February 2018 by both Audit Team Members together. During the afternoon site visit, the weather was chilly, grey and overcast with light rain and, the existing carriageway surface was wet.

Vehicular traffic conditions at the time of the afternoon site visit were observed to be light. Four pedestrians but no pedal cyclists were observed during the afternoon site visit.

- 1.7** The terms of reference of the Audit are as described in HD 19/15. The Audit Team has examined and reported only on the road safety implications of the scheme as presented and, has not examined or verified the compliance of the designs to any other criteria. However, to clearly explain a safety problem or the recommendation made to resolve the identified problem, the Audit Team may, on occasion, have referred to a Design Standard without touching on technical audit.
- 1.8** No Departures from Design Standards have been reported by the Design Organisation.
- 1.9** Issues identified and observations made during this Combined Stage 1 and 2 Road Safety Audit and site inspection which the Audit Team wishes to draw to the specific attention of the Audit Project Sponsor, i.e. the Local Highway Authority, Wokingham Borough Council, will be set out in a separate letter. These issues could include maintenance items and operational issues. In this regard, the Audit Team have made reference to five issues identified and observations made as referred to in a Covering Letter to the Design Organisation, WSP Wokingham Highway Alliance, dated the 21st February 2018. This Covering Letter should be supplied to the Audit Project Sponsor, i.e. the Local Highway Authority, Wokingham Borough Council and, be considered in conjunction with this Combined Stage 1 and 2 Road Safety Audit Report.

2.0 ITEMS RAISED AT THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

2.1 GENERAL

2.1.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.2 LOCAL ALIGNMENT

2.2.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.3 JUNCTIONS

2.3.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.4 NON MOTORISED USER PROVISION

2.4.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.5 ROAD SIGNS, CARRIAGEWAY MARKINGS & STREET LIGHTING

2.5.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

END OF PROBLEMS IDENTIFIED AND RECOMMENDATIONS OFFERED IN THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

3.0 AUDIT TEAM STATEMENT

We certify that this Audit has been carried out in accordance with HD 19/15.

AUDIT TEAM LEADER

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)

Director

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 21st February 2018

AUDIT TEAM MEMBER

James Bedingfeld MCIHT

Associate Consultant

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 21st February 2018

ANNEX A

ANNEX A

MURRAY ROAD, WOKINGHAM

PROPOSED ZEBRA PEDESTRIAN CROSSING FACILITY

COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

LIST OF WOKINGHAM BOROUGH COUNCIL DRAWINGS SUBMITTED FOR AUDITING

DRAWING NO.	TITLE
MURR/GA/001 Rev A	General Arrangement
MURR/GA/002 Rev A	Road Markings & Traffic Signs Arrangement
WSD/500/3 Rev B	Service Ducts
WSD/500/13 Rev B	Jointing Chambers For Traffic Signal & Street Lighting Cables
WSD/1100/2 Rev A	Vehicular & Pedestrian Crossovers
WSD/1100/4 Rev B	Concrete Paviers & Flags & Signal Controlled Crossing Points
WSD/1100/13 Rev B	Footways, Cycleways & Verges
WSD/1200/1 Rev B	Traffic Signs



**WOKINGHAM
BOROUGH COUNCIL**

**ACORNS
PROJECTS
LIMITED**



**New Wokingham Road, Crowthorne
Proposed Puffin Pedestrian Crossing Facility
Combined Stage 1 and 2 Road Safety Audit**

For Wokingham Borough Council

Prepared by Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

MARCH 2018

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants
Redwood House
3 Eaton Park
Eaton Bray
Bedfordshire
LU6 2SP

Telephone: 01525-222359
Mobile: 07860-629328
E-mail: abc@acornsprojects.com

Version No: 1.0

Document Location

Ensure that this document is current. Printed documents and locally copied files may become obsolete due to changes to the master document.

This is a controlled document. The source of the document can be found on the file server at location:

C:\Acorns\Clients\WokinghamBoroughCouncil\NewWokinghamRoadCrowthorne\Reports\NewWokinghamRoadCrowthorneCombinedStage1&2RSAV1.0.doc

Revision History

This document has the following history:

Version No.	Version Date	Summary of Changes	Changes marked
1.0	14/03/2018	N/A	N/A

Approvals

This document requires the following approvals:

Name	Title
Adriano B. Cappella	Audit Team Leader
David A. Bowie	Audit Team Member

Distribution

This document has also been distributed to:

Name	Title & Organisation
Tim Allen	Associate - WSP Wokingham Highway Alliance

1.0 INTRODUCTION

- 1.1 This report results from a Combined Stage 1 and 2 Road Safety Audit carried out on the New Wokingham Road, Crowthorne, Wokingham, Proposed Puffin Pedestrian Crossing Facility Project, at the request of the Project Sponsor/Client Project Manager for Wokingham Borough Council, Neighbourhood Services, PO Box 153, Shute End, Wokingham, Berkshire, RG40 1WL. The Design Organisation is the WSP Wokingham Highway Alliance, who are based within the Neighbourhood Services offices in Wokingham.
- 1.2 The scheme proposals comprise the removal of an existing uncontrolled pedestrian crossing facility in the form of dropped kerbs, tactile paving and a pedestrian refuge island, which will be replaced with a proposed Puffin pedestrian crossing facility. A short length of new footway on the western side of New Wokingham Road at the Puffin pedestrian crossing facility is also proposed. The proposed Zebra pedestrian crossing facility will be sited in New Wokingham Road, Crowthorne, to the immediate south of the Edgcumbe Park Drive junction.
- 1.3 The Audit Team membership was as follows:
- Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)
(Audit Team Leader) Director, Acorns Projects Limited
- David A. Bowie BSc (Hons), MCIHT, (HA Certificate of Competency - Feb 2014)
(Audit Team Member) Associate Consultant, Acorns Projects Limited
- 1.4 The Audit took place at the Eaton Bray office of Acorns Projects Limited during February and March 2018. The Audit was undertaken in accordance with the Audit Brief contained in the WSP Wokingham Highway Alliance E-Mail to Acorns Projects Limited dated the 26th February 2018. The Audit comprised an examination of the drawings provided by the Design Organisation and, are listed in Annex A.
- 1.5 The drawings consisted of a copy of the general arrangements (2 No. sheets), trial hole arrangement, traffic signal arrangement and, standard details (8 No. sheets). Copies of the scheme drawings at both A3 and A4 size were provided for the Audit Team's use. Road traffic collision data, vehicular traffic flow data, and, pedestrian and pedal cycle flow data has not been provided for the purposes of this Combined Stage 1 and 2 Road Safety Audit.

- 1.6** A visit to the site was undertaken during the afternoon of the 13th March 2018 by both Audit Team Members together. During the afternoon site visit, the weather was mild and overcast and, the existing carriageway surface was dry. Vehicular traffic conditions at the time of the afternoon site visit were observed to be light to moderate. One pedestrian but no pedal cyclists were observed during the afternoon site visit.
- 1.7** The terms of reference of the Audit are as described in HD 19/15. The Audit Team has examined and reported only on the road safety implications of the scheme as presented and, has not examined or verified the compliance of the designs to any other criteria. However, to clearly explain a safety problem or the recommendation made to resolve the identified problem, the Audit Team may, on occasion, have referred to a Design Standard without touching on technical audit.
- 1.8** No Departures from Design Standards have been reported by the Design Organisation.
- 1.9** Issues identified or observations made during this Combined Stage 1 and 2 Road Safety Audit and site inspection which the Terms of Reference exclude from this report, but which the Audit Team wishes to draw to the specific attention of the Audit Project Sponsor, i.e. the Local Highway Authority, Wokingham Borough Council, will be set out in a separate letter. These issues could include maintenance items and operational issues. The Audit Team has not identified any issues during this Combined Stage 1 and 2 Road Safety Audit and site inspection that are considered to be outside the Terms of Reference.

2.0 ITEMS RAISED AT THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

2.1 GENERAL

2.1.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.2 LOCAL ALIGNMENT

2.2.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.3 JUNCTIONS

2.3.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.4 NON MOTORISED USER PROVISION

2.4.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.5 ROAD SIGNS, CARRIAGEWAY MARKINGS & STREET LIGHTING

2.5.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

END OF REPORT - NO PROBLEMS IDENTIFIED OR RECOMMENDATIONS OFFERED IN THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

3.0 AUDIT TEAM STATEMENT

We certify that this Audit has been carried out in accordance with HD 19/15.

AUDIT TEAM LEADER

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)

Director

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 16th March 2018

AUDIT TEAM MEMBER

David A. Bowie BSc (Hons), MCIHT, (HA Certificate of Competency - Feb 2014)

Associate Consultant

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 16th March 2018

ANNEX A

ANNEX A

NEW WOKINGHAM ROAD, CRWOTHORNE

PROPOSED PUFFIN PEDESTRIAN CROSSING FACILITY

COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

LIST OF WOKINGHAM BOROUGH COUNCIL DRAWINGS SUBMITTED FOR AUDITING

DRAWING NO.	TITLE
NWR/GA/001 Rev A	General Arrangement Sheet 1 of 2
NWR/GA/002 Rev A	General Arrangement Sheet 2 of 2
NWR/TH/001 Rev A	Trial Hole Arrangement
NWR/TS/001 Rev A	Traffic Signals Arrangement
WSD/500/3 Rev B	Service Ducts
WSD/500/13 Rev B	Jointing Chambers For Traffic signal & Street Lighting Cables
WSD/900/2 Rev B	Pavement Construction Thickness Major Access Roads & Intermediate Roads (Less than 250 commercial vehicles per day)
WSD/1100/1 Rev A	Kerbs & Channels
WSD/1100/2 Rev A	Vehicular & Pedestrian Crossovers
WSD/1100/4 Rev B	Concrete Paviers & Flags & Signal Controlled Crossing Points
WSD/1100/13 Rev B	Footways, Cycleways & Verges
WSD/1200/1 Rev B	Traffic Signs



**WOKINGHAM
BOROUGH COUNCIL**

**ACORNS
PROJECTS
LIMITED**



**Silverdale Road, (Loddon), Earley, Wokingham
Proposed Puffin Pedestrian Crossing Facility
Combined Stage 1 and 2 Road Safety Audit**

For Wokingham Borough Council

Prepared by Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

FEBRUARY 2018

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants
Redwood House
3 Eaton Park
Eaton Bray
Bedfordshire
LU6 2SP

Telephone: 01525-222359
Mobile: 07860-629328
E-mail: abc@acornsprojects.com

Version No: 1.0

Document Location

Ensure that this document is current. Printed documents and locally copied files may become obsolete due to changes to the master document.

This is a controlled document. The source of the document can be found on the file server at location:

C:\Acorns\Clients\WokinghamBoroughCouncil\SilverdaleRoad(Loddon)EarleyWokingham\Reports\Silverdale Road(Loddon)EarleyWokinghamCombinedStage1&2RSAV1.0.doc

Revision History

This document has the following history:

Version No.	Version Date	Summary of Changes	Changes marked
1.0	12/02/2018	N/A	N/A

Approvals

This document requires the following approvals:

Name	Title
Adriano B. Cappella	Audit Team Leader
James Bedingfeld	Audit Team Member

Distribution

This document has also been distributed to:

Name	Title & Organisation
Patrick McElroy	Graduate Highways Engineer - WSP Wokingham Highway Alliance
Tim Allen	Associate - WSP Wokingham Highway Alliance

1.0 INTRODUCTION

1.1 This report results from a Combined Stage 1 and 2 Road Safety Audit carried out on the Silverdale Road, (Loddon), Earley, Wokingham, Proposed Puffin Pedestrian Crossing Facility Project, at the request of the Project Sponsor/Client Project Manager for Wokingham Borough Council, Neighbourhood Services, PO Box 153, Shute End, Wokingham, Berkshire, RG40 1WL. The Design Organisation is the WSP Wokingham Highway Alliance, who are based within the Neighbourhood Services offices in Wokingham.

1.2 The scheme proposals comprise the installation of a new Puffin pedestrian crossing facility, which will be sited in Silverdale Road, (Loddon), Earley, Wokingham, to the north of the Gipsy Lane junction.

1.3 The Audit Team membership was as follows:

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)
(Audit Team Leader) Director, Acorns Projects Limited

James Bedingfeld MCIHT
(Audit Team Member) Associate Consultant, Acorns Projects Limited

1.4 The Audit took place at the Eaton Bray office of Acorns Projects Limited during January and February 2018. The Audit was undertaken in accordance with the Audit Brief contained in the WSP Wokingham Highway Alliance E-Mail to Acorns Projects Limited dated the 19th January 2018. The Audit comprised an examination of the drawings provided by the Design Organisation and, are listed in Annex A.

1.5 The drawings consisted of a copy of the general arrangement, road markings and traffic signs arrangement, traffic signals arrangement and, standard details (7 No. sheets). Copies of the scheme drawings at both A3 and A4 size were provided for the Audit Team's use. Road traffic collision data, vehicular traffic flow data, and, pedestrian and pedal cycle flow data has not been provided for the purposes of this Combined Stage 1 and 2 Road Safety Audit.

1.6 A visit to the site was undertaken during the afternoon of the 30th January 2018 by both Audit Team Members together. During the afternoon site visit, the weather was reasonably mild, with sunny spells and, the existing carriageway surface was dry. Vehicular traffic conditions at the time of the afternoon site visit were observed to be light to moderate. Three pedestrians but no pedal cyclists were observed during the afternoon site visit.

- 1.7** The terms of reference of the Audit are as described in HD 19/15. The Audit Team has examined and reported only on the road safety implications of the scheme as presented and, has not examined or verified the compliance of the designs to any other criteria. However, to clearly explain a safety problem or the recommendation made to resolve the identified problem, the Audit Team may, on occasion, have referred to a Design Standard without touching on technical audit.
- 1.8** No Departures from Design Standards have been reported by the Design Organisation.
- 1.9** All Problems and Recommendations are referenced to the design drawings and the locations have been indicated on the A4 plan supplied for use by the Audit Team in Annex B.
- 1.10** Issues identified and observations made during this Combined Stage 1 and 2 Road Safety Audit and site inspection which the Audit Team wishes to draw to the specific attention of the Audit Project Sponsor, i.e. the Local Highway Authority, Wokingham Borough Council, will be set out in a separate letter. These issues could include maintenance items and operational issues. In this regard, the Audit Team have made reference to one issue identified and observation made as referred to in a Covering Letter to the Design Organisation, WSP Wokingham Highway Alliance, dated the 21st February 2018. This Covering Letter should be supplied to the Audit Project Sponsor, i.e. the Local Highway Authority, Wokingham Borough Council and, be considered in conjunction with this Combined Stage 1 and 2 Road Safety Audit Report.

2.0 ITEMS RAISED AT THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

2.1 GENERAL

2.1.1 PROBLEM

Locations 1 & 2 - The south western and north eastern channel lines of Silverdale Road (Drawing Nos. SILV/GA/001 Rev A).

Summary - Potential surface water ponding issues could result in a potential slip or trip hazard for pedestrians at the proposed Puffin pedestrian crossing facility.

The site visit has established at the location of the proposed Puffin pedestrian crossing facility, Silverdale Road is the subject of a reasonable downhill gradient for vehicles travelling in the south east bound direction. The site visit has also established the presence of existing surface water gullies as indicated at Locations 1 and 2.

The locations of the existing surface water drainage gullies are quite some distance from the proposed Puffin pedestrian crossing facility. Thus, it is possible that beyond the two surface water gullies identified, there could be significant amounts of surface water running along the south western and north eastern channel lines of Silverdale Road, towards the location of the proposed Puffin pedestrian crossing facility.

Concern arises that should surface water wash up onto the footway when the dropped kerbs for the proposed Puffin pedestrian crossing facility are installed, there could be a potential slip or trip hazard for pedestrians during wet and subsequent cold/freezing weather conditions, with the resultant slight potential increased risk that personal injury could be sustained by pedestrians, particularly for those pedestrians who may be blind or visually impaired.

RECOMMENDATION

It is Recommended that at the construction stage of the project, additional surface water drainage gullies should be installed within the south western and north eastern channel lines of Silverdale Road on both sides of the carriageway, immediately before the dropped kerbs of the proposed Puffin pedestrian crossing facility.

2.1.2 PROBLEM

Location 3 - The proposed Puffin pedestrian crossing facility (Drawing Nos. SILV/GA/001 Rev A & SILV/TS/001 Rev A).

Summary - No provision for a traffic signal maintenance engineer to stop in immediate proximity to the proposed Puffin pedestrian crossing facility.

The scheme proposals include the installation of a new Puffin pedestrian crossing facility, which will be sited in Silverdale Road, to the north of the Gipsy Lane junction. At this Combined Stage 1 and 2 Road Safety Audit, the scheme proposals do not appear to include the provision of a traffic signal maintenance engineer's layby or hardstanding area adjacent to the proposed Puffin pedestrian crossing facility. It will be important to ensure that an adequate and suitable facility is provided for the safety and benefit of site operatives, otherwise there could be a potential increased risk of vehicular/site operative collisions occurring, whereby site operatives are likely to sustain personal injury.

RECOMMENDATION

It is Recommended that a traffic signal maintenance engineer's layby or hardstanding area should be provided in proximity to the proposed Puffin pedestrian crossing facility. In determining a suitable location, the facility should be sited clear of the traffic lanes, traffic signal poles and signal heads and, any existing or proposed pedestrian and pedal cycle routes if possible.

2.2 LOCAL ALIGNMENT

2.2.1 PROBLEM

Locations 4 & 5 - The Silverdale Road south east bound approach to the proposed Puffin pedestrian crossing facility (Drawing Nos. SILV/GA/001 Rev A & SILV/TS/001 Rev A).

Summary - The vertical alignment and the slight crest in the Silverdale Road carriageway could result in an increased risk of stop line overshoots and subsequent vehicular/pedestrian collisions occurring when traffic signal changes take place or, in addition, an increased risk of nose to tail shunt type collisions occurring between a leading and any following vehicles on the south east bound approach to the proposed Puffin pedestrian crossing facility stop line.

The scheme proposals indicate that new Puffin pedestrian crossing facility will be introduced within the Silverdale Road carriageway. The site visit has established at the location of the proposed Puffin pedestrian crossing facility, Silverdale Road is the subject of a reasonable downhill gradient for vehicles travelling in the south east bound direction. In addition, prior to the change in the vertical alignment, there is also a slight crest in the Silverdale Road carriageway.

Concern arises that the combination of the slight crest in the carriageway and the reasonable change in vertical alignment could potentially impact upon a south east bound driver's forward visibility to the nearside primary and offside secondary traffic signal heads, whereby there could be a potential increased risk of stop line overshoots occurring when signal changes take place, with subsequent vehicular/pedestrian collisions occurring at the proposed Puffin pedestrian crossing facility. This could result in pedestrians potentially sustaining high levels of personal injury, particularly those pedestrians who may be blind or visually impaired.

In addition, there could be a potential increased risk of nose to tail shunt type collisions occurring between a leading and any following vehicles on the south east bound approach to the proposed Puffin pedestrian crossing facility stop line when signal changes take place.

RECOMMENDATION

It is Recommended that at 1.05 metres above carriageway level, the maximum geometric stopping sight distances that can be achieved on the south east bound approach to the nearside primary and offside secondary traffic signal heads should be accurately established and confirmed accordingly.

Should the maximum geometric stopping sight distance achievable on the south east bound approach to the primary traffic signal heads be established as being sub-standard, it is Recommended that high level traffic signal poles and traffic signal heads should be provided as part of the proposed Puffin pedestrian crossing facility installation.

2.3 JUNCTIONS

2.3.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.4 NON MOTORISED USER PROVISION

2.4.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

2.5 ROAD SIGNS, CARRIAGEWAY MARKINGS & STREET LIGHTING

2.5.1 No Problems identified in this category at this Combined Stage 1 and 2 Road Safety Audit.

END OF PROBLEMS IDENTIFIED AND RECOMMENDATIONS OFFERED IN THIS COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

3.0 AUDIT TEAM STATEMENT

We certify that this Audit has been carried out in accordance with HD 19/15.

AUDIT TEAM LEADER

Adriano B. Cappella IEng, FIHE, MCIHT, MSoRSA, (HA Certificate of Competency - Feb 2014)

Director

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 21st February 2018

AUDIT TEAM MEMBER

James Bedingfeld MCIHT

Associate Consultant

Acorns Projects Limited

Safety Traffic Project Management & Highway Engineering Consultants

Redwood House

3 Eaton Park

Eaton Bray

Bedfordshire

LU6 2SP

Signed :



Date : 21st February 2018

ANNEX A

ANNEX A

SILVERDALE ROAD, (LODDON), EARLEY, WOKINGHAM

PROPOSED PUFFIN PEDESTRIAN CROSSING FACILITY

COMBINED STAGE 1 AND 2 ROAD SAFETY AUDIT

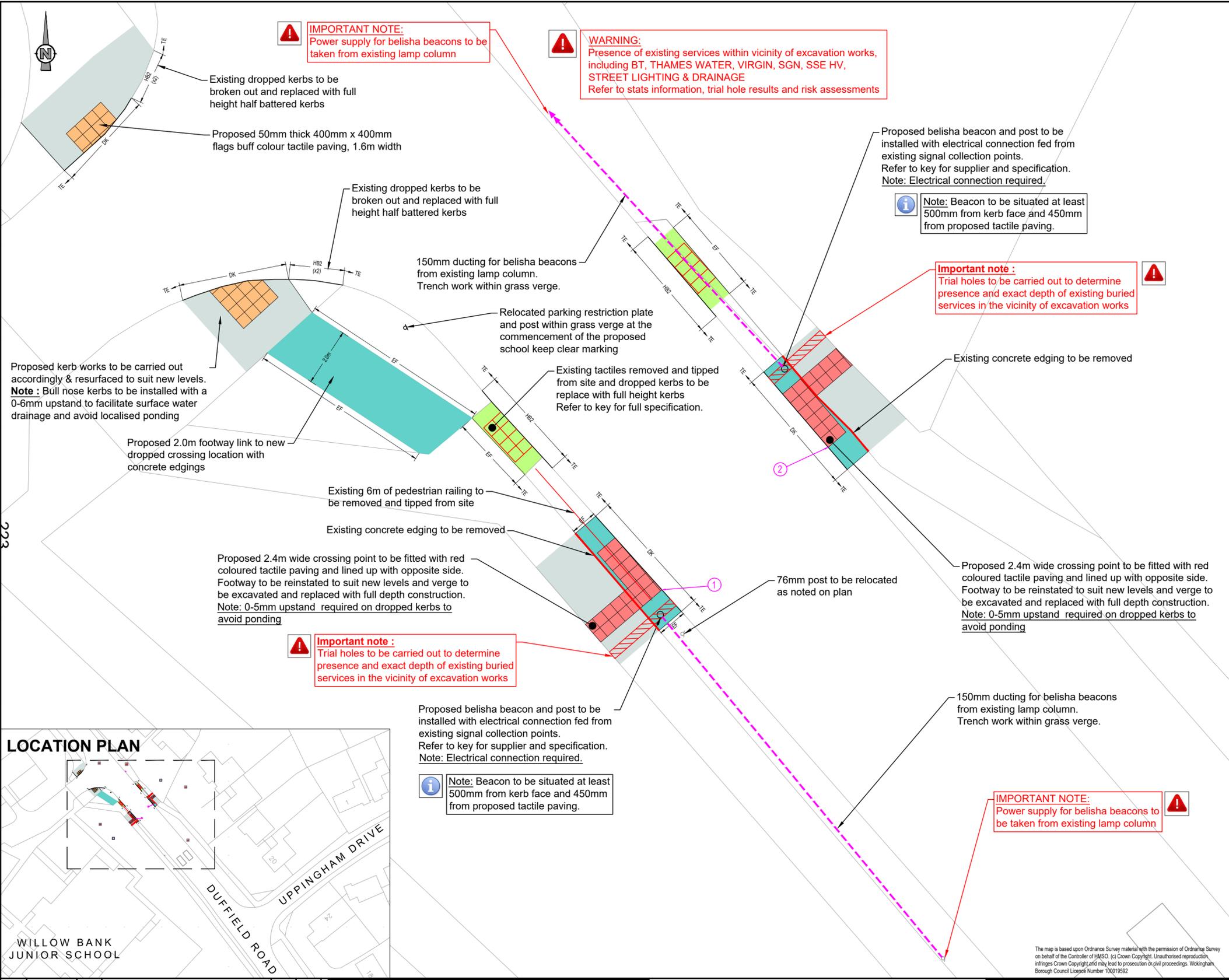
LIST OF WOKINGHAM BOROUGH COUNCIL DRAWINGS SUBMITTED FOR AUDITING

DRAWING NO.	TITLE
SILV/GA/001 Rev A	General Arrangement
SILV/GA/002 Rev A	Road Markings & Signs Arrangement
SILV/TS/001 Rev A	Traffic Signals Arrangement
WSD/500/3 Rev B	Service Ducts
WSD/500/13 Rev B	Jointing Chambers For Traffic signal & Street Lighting Cables
WSD/1100/1 Rev A	Kerbs & Channels
WSD/1100/2 Rev A	Vehicular & Pedestrian Crossovers
WSD/1100/4 Rev B	Concrete Paviers & Flags & Signal Controlled Crossing Points
WSD/1100/13 Rev B	Footways, Cycleways & Verges
WSD/1200/1 Rev B	Traffic Signs

ANNEX B

This page is intentionally left blank

WDC-FP2-DATA-ENG-WOKINGHAM HIGHWAY ALLIANCE TRAFFIC MANAGEMENT 201819 PROGRAMME CAPITAL PROJECTS SIGNAL SCHEMES DUFFIELD ROAD, WOODLEY DRAWINGS AUTOCAD DUFF_GA_001.DWG 14/02/2018 14:37:16 Patrick McElroy



Notes:

- All dimensions are in metres unless stated otherwise.
- This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
- Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
- Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
- All setting out on site to be agreed with Engineer.
- All traffic management arrangements shall be carried out in accordance with Traffic Signs Manual Chapter 8
- All works are to be in accordance with Wokingham Borough Council specifications and standard details

DO NOT SCALE

Key

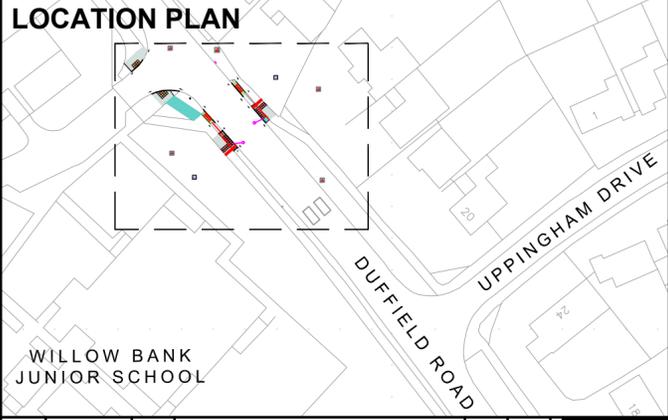
- Proposed dropped kerb with transitions, refer to standard detail WSD/1100/001
- Proposed full height kerb, refer to standard detail WSD/1100/001
- Proposed concrete edging kerb, refer to standard detail WSD/1100/001
- Tie into existing kerb line
- Existing footway construction to be broken out to a depth of 20mm (up to 70mm if required) and shall be prepared for an in-lay including an application of a weed killer. Proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate).
 Refer to standard detail WSD/1100/13
- Existing surface to be dug out to a depth of 220mm. proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate)
 - 150mm type 1 sub-base material to clause 803 refer to inset 3 and standard detail WSD/1100/13
 Note: Standard geotextile to be laid underneath footway construction, standard geotextile terram t1000 or similar
- Proposed area to be peppered and then filled with topsoil to 150mm depth, re-graded to suit new levels and grass seeded
- Items to be removed and tipped from side
- Proposed 150mm Ø ducting for electrical connection
- Proposed trial hole location

Proposed Belisha Beacon

Beacon:
LED Zebrite beacon
Supplier: Zebrite Ltd
Address: Unit 9, Mill House Farm, Billingford Road, North Elmham, Norfolk. NR20 5HN
Tel. 0845 003 7361

Belisha beacon post:
Signature CDM Zebbrasafe - ZEB4-CDM 4 Band - assembly post 3.7m OAL (600pd)
Supplier: Signature
Address: Hainge Road, Tividale, Oldbury, West Midlands. B69 2NY
Tel. 0121 5570234

SETTING OUT POINTS		
Number	Eastings	Northings
1	476868.499	174237.150
2	476874.205	174242.038



REV	DATE	BY	DESCRIPTION	CHK	APD
C	13/02/18	PMc	SCHOOL DROPPED CROSSING ENTRANCE REALIGNED FOLLOWING SAFETY AUDITOR RECOMMENDATIONS	SR	TRA
B	24/01/2018	PMc	CDM AMENDMENTS ADDED	SR	TRA
A	07/12/2017	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: **FOR INFORMATION ONLY**

WOKINGHAM BOROUGH COUNCIL
Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: **WOKINGHAM BOROUGH COUNCIL**

ARCHITECT:

PROJECT: **DUFFIELD ROAD, WOODLEY ZEBRA CROSSING INSTALLATION**

TITLE: **SIGNAL CROSSING SCHEMES 2018/19 GENERAL ARRANGEMENT**

SCALE @ A2: 1:100

CHECKED: SR

APPROVED: TRA

CAD FILE: DUFF_GA_001

DESIGN-DRAWN: PMc

DATE: November 2017

PROJECT No: 5049

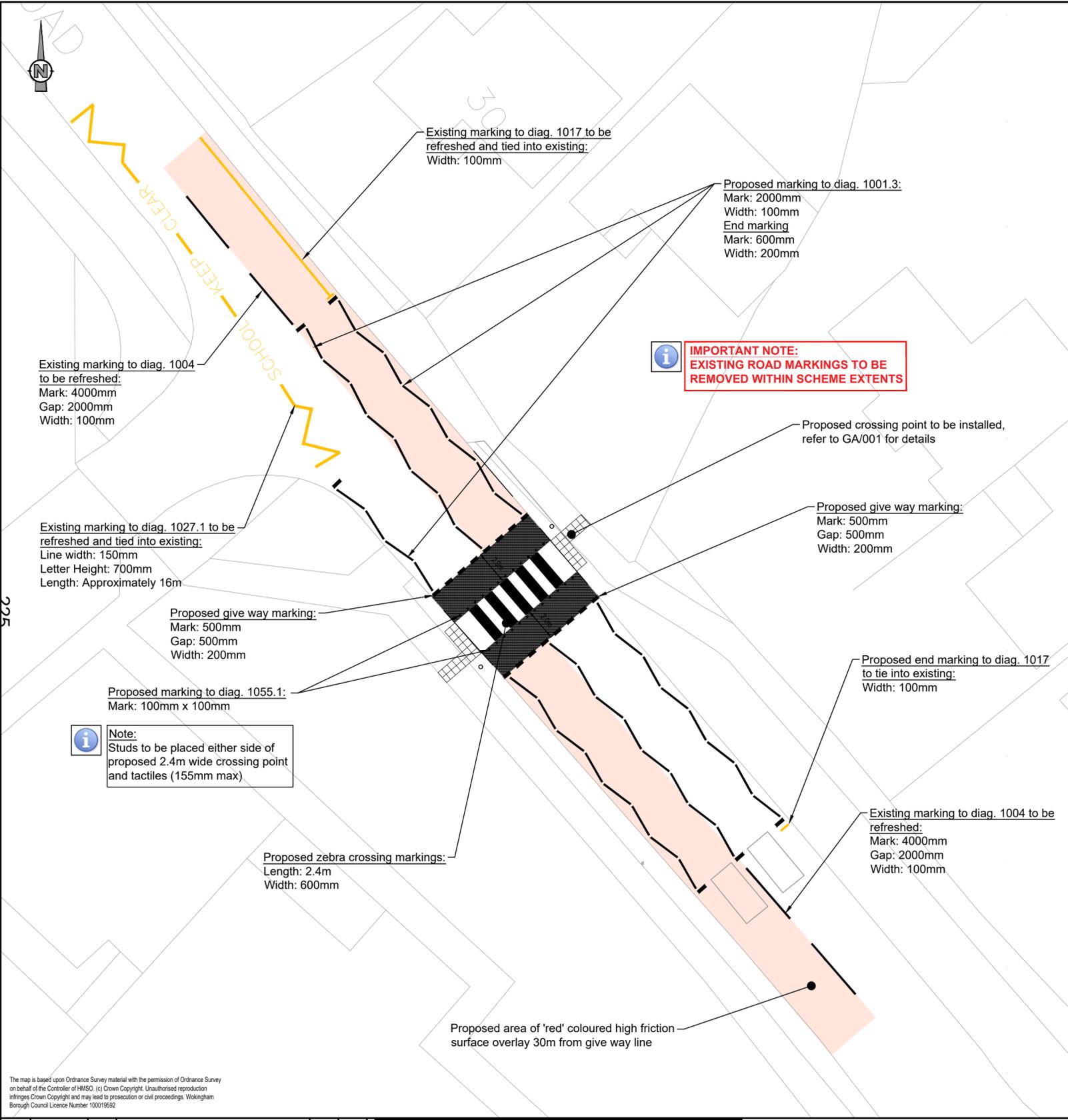
DRAWING No: DUFF/GA/001

REV: C

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction, infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

This page is intentionally left blank

W:\WDC-FP2-DATA\ENG\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\DUFFIELD ROAD, WOODLEY\DRAWINGS\AUTOCAD\DIFF_GA_002.DWG 14/02/2018 14:32:49 Patrick McElroy



- Notes**
- All dimensions are in metres unless stated otherwise.
 - This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
 - Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
 - All works are to be in accordance with Wokingham Borough Council specifications and standards.
 - Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
 - The works shall be programmed to ensure a clear footway is available for pedestrians throughout the works on or another side of the carriageway.
 - Diagram numbers to TSRGD refer to those set out in the Traffic Signs Regulations and General Directions 2002.
 - Mounting heights of all signs to be
 - Footway 2.1m
 - Cycleways 2.4m
 - Verges and non-pedestrian areas as directed by the engineer (normally) 1.8m.
 Refer to WBC standard detail WSD/1200/01 for further details.
 - All signs and street furniture to have a minimal lateral clearance of 450mm from all kerb faces. refer to WBC standard detail WSD/1200/01 for further details.
 - All non-illuminated signs and supplementary plates to be retro-reflective.
 - All signs to be class RA2 material.
 - All road markings are to be white thermoplastic screed unless stated otherwise.

DO NOT SCALE

Key

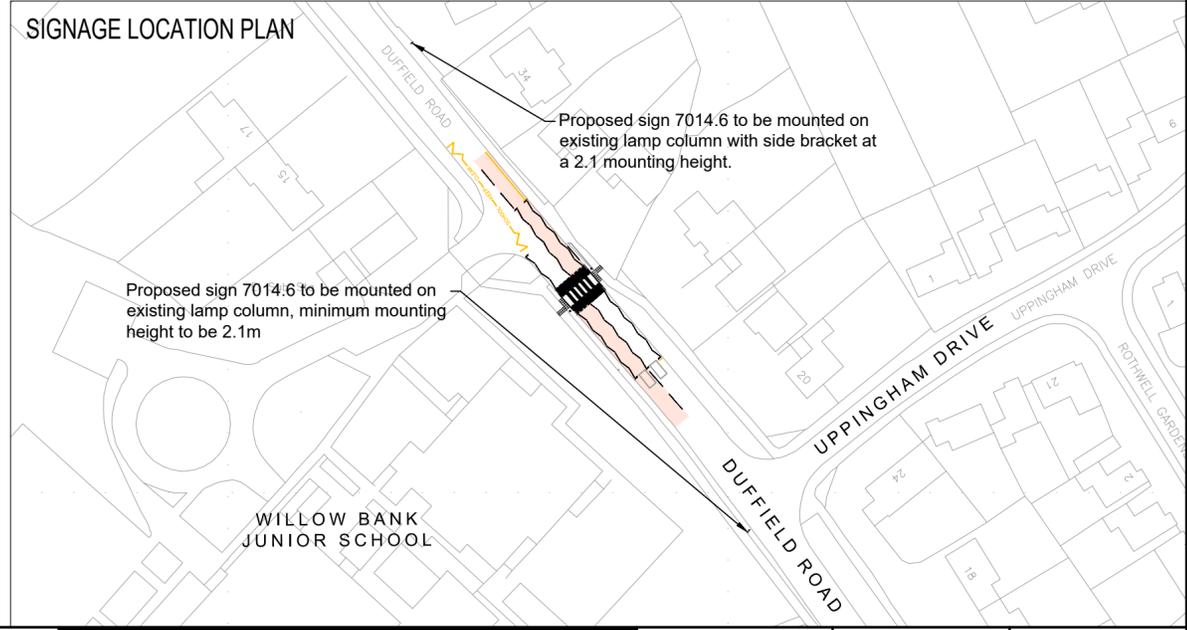
- Road markings to TSRGD specification (white screed)
- Road markings to TSRGD specification (yellow screed)
- New area of red high friction surface overlay (hot lay)
- New area of charcoal high friction surface overlay (hot lay)

Sign Schedule

Scheme Ref.	Duffield Road Ped Crossing 2018/19	
Sign Ref.	7014	x-height 62.5
Letter colour	WHITE	SIGN FACE
Background	RED	Width 775mm
Border	WHITE	Height 565mm
Material	Class RA2 (12899-1:2007)	Area 0.44m ²



Note: Diagram 7014 is a temporary sign and is to be removed after 3 months



The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

REV	DATE	BY	DESCRIPTION	CHK	APD
C	14/02/2018	PMc	SIGN DIAG 544 REMOVED	SR	TRA
B	24/01/2018	PMc	CDM AMENDMENTS ADDED	SR	TRA
A	11/12/2017	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: **ISSUED FOR CONSTRUCTION**



WOKINGHAM BOROUGH COUNCIL

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

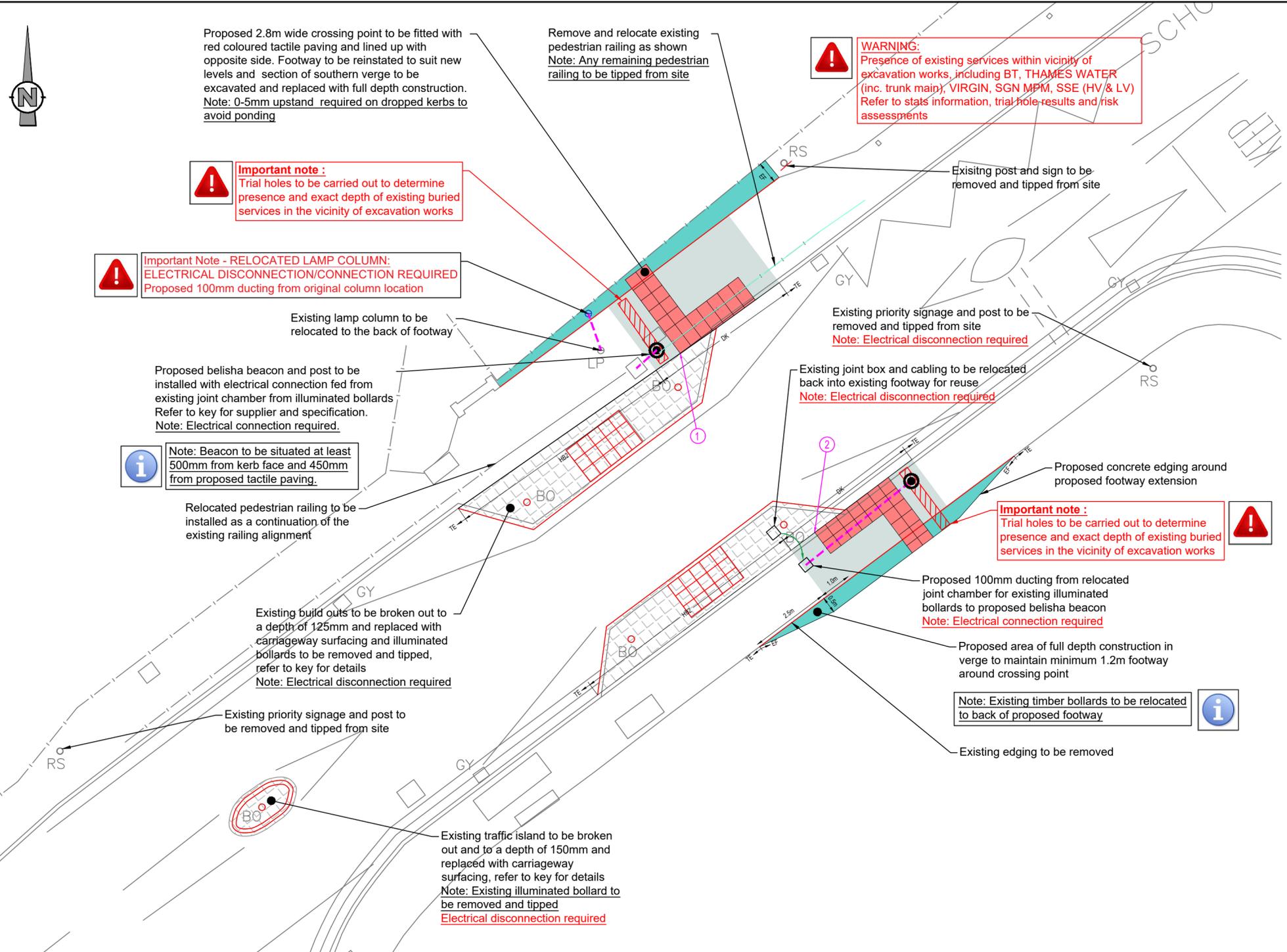
CLIENT: WOKINGHAM BOROUGH COUNCIL
ARCHITECT:

PROJECT: DUFFIELD ROAD, WOODLEY ZEBRA CROSSING INSTALLATION
TITLE: SIGNAL CROSSING SCHEMES 2018/19 ROAD MARKINGS, SIGNAGE & SURFACING ARRANGEMENT

SCALE @ A2: 1:200	CHECKED: SR	APPROVED: TRA
CAD FILE: DUFF_GA_002	DESIGN-DRAWN: PMc	DATE: January 2018
PROJECT No: 5049	DRAWING No: DUFF/GA/002	REV: C

This page is intentionally left blank

C:\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\HURRICANE WAY, WOODLEY\DRAWINGS\AUTOCAD\HURR_GA_001_B.DWG 27/03/2018 09:16:42 Tim Allen



Notes:

- All dimensions are in metres unless stated otherwise.
- This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
- Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
- Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
- All setting out on site to be agreed with Engineer.
- All traffic management arrangements shall be carried out in accordance with Traffic Signs Manual Chapter 8
- All works are to be in accordance with Wokingham Borough Council specifications and standard details

DO NOT SCALE

Key

- DK Proposed dropped kerb with transitions, refer to standard detail WSD/1100/001
- HB2 Proposed full height kerb, refer to standard detail WSD/1100/001
- EF Proposed concrete edging kerb, refer to standard detail WSD/1100/001
- TE Tie into existing kerb line
- Proposed red colour tactile paving, refer to standard details WSD/1100/02 & 04
- Existing footway construction to be broken out to a depth of 20mm (up to 70mm if required) and shall be prepared for an in-lay including an application of a weed killer. Proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate).
 Refer to standard detail WSD/1100/13
- Existing surface to be dug out to a depth of 220mm. proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate)
 - 150mm type 1 sub-base material to clause 803 refer to inset 3 and standard detail WSD/1100/13
 Note: Standard geotextile to be laid underneath footway construction, standard geotextile terram t1000 or similar
- Proposed area to be broken out to a depth of 150mm, and inlaid with 110mm binder course and 40mm surface course Refer to WSD/900/002
- Items to be removed and tipped from side
- Proposed 100mm Ø ducting for electrical connection
- Proposed trial hole location
- Proposed Belisha Beacon
 Beacon:
 LED Zebrite beacon
 Supplier: Zebrite Ltd
 Address: Unit 9, Mill House Farm, Billingford Road, North Elmham, Norfolk. NR20 5HN
 Tel. 0845 003 7361
- Belisha beacon post:
 Mallatite CENTRESAFE 4.4MT LED
 Supplier: Mallatite Ltd,
 Hardwick View Road, Holmewood Industrial Estate, Holmewood, Derbyshire S42 5SA
 Tel. 01246 593280

SETTING OUT POINTS		
Number	Eastings	Northings
1	477615.841	173549.687
2	477619.936	173544.235

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

REV	DATE	BY	DESCRIPTION	CHK	APD
B	26/03/2018	JM	ZEBRA CROSSING RELOCATED FOLLOWING SAFETY AUDIT RECOMMENDATION	SR	TRA
A	19/01/2018	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: **ISSUED FOR CONSTRUCTION**

WOKINGHAM BOROUGH COUNCIL
 Shute End, Wokingham, Berkshire RG40 1BN
 Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: **WOKINGHAM BOROUGH COUNCIL**

ARCHITECT:

PROJECT: **HURRICANE WAY, WOODLEY PEDESTRIAN CROSSING UPGRADE**

TITLE: **SIGNAL CROSSING SCHEMES 2018/19 GENERAL ARRANGEMENT SHEET 1 OF 2**

SCALE @ A2: 1:100	CHECKED: SR	APPROVED: TRA
CAD FILE: HURR_GA_001_B	DESIGN-DRAWN: PMc	DATE: February 2018
PROJECT No: 5049	DRAWING No: HURR/GA/001	REV: B

This page is intentionally left blank

DO NOT SCALE

Notes

- All dimensions are in metres unless stated otherwise.
- This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
- Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
- All works are to be in accordance with Wokingham Borough Council specifications and standards.
- Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
- The works shall be programmed to ensure a clear footway is available for pedestrians throughout the works on or another side of the carriageway.
- Diagram numbers to TSRGD refer to those set out in the Traffic Signs Regulations and General Directions 2002.
- Mounting heights of all signs to be
 - Footway 2.1m
 - Cycleways 2.4m
 - Verges and non-pedestrian areas as directed by the engineer (normally) 1.8m.
 Refer to WBC standard detail WSD/1200/01 for further details.
- All signs and street furniture to have a minimal lateral clearance of 450mm from all kerb faces. refer to WBC standard detail WSD/1200/01 for further details.
- All non-illuminated signs and supplementary plates to be retro-reflective.
- All signs to be class RA2 material.
- All road markings are to be white thermoplastic screed unless stated otherwise.

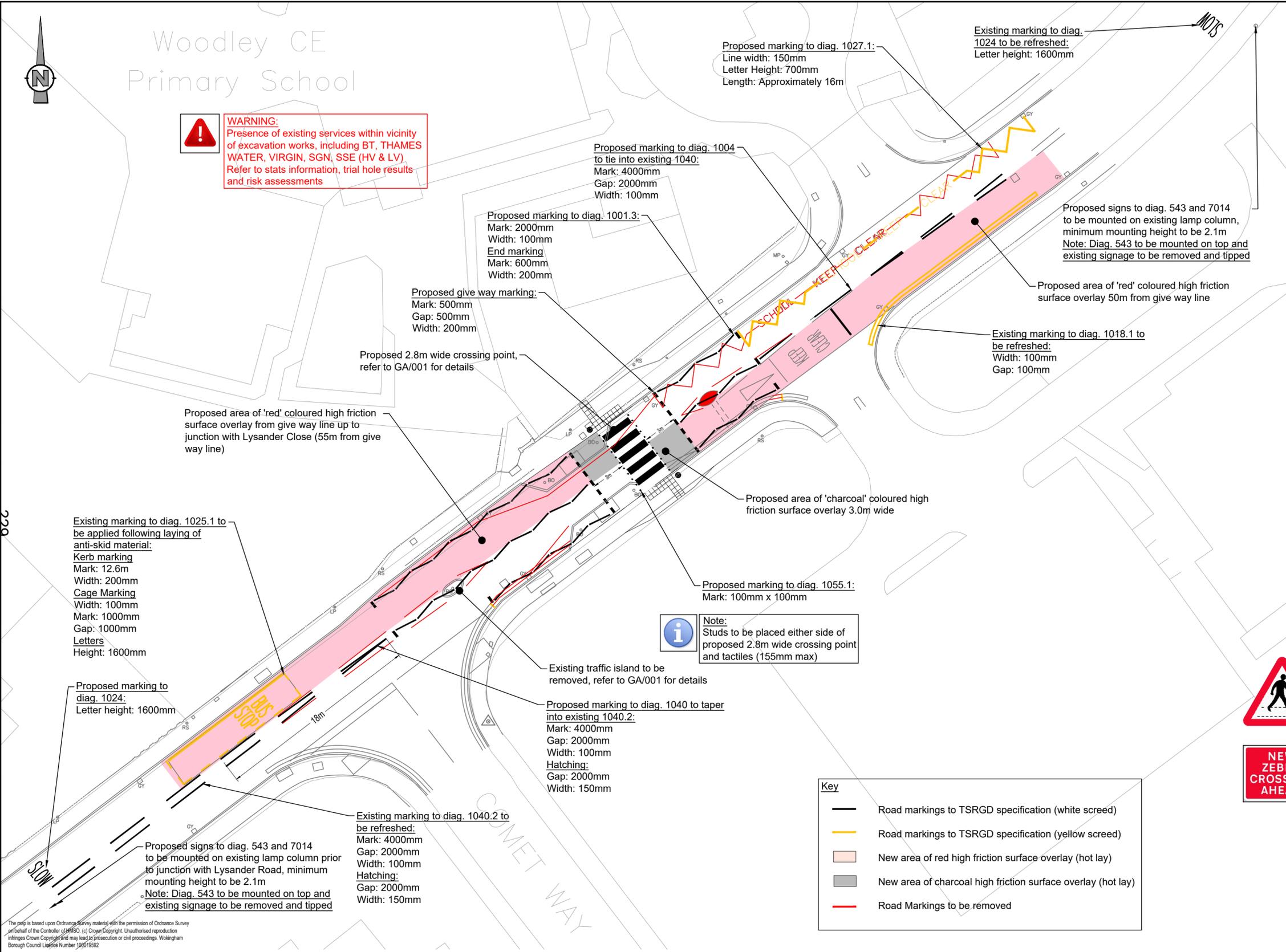
Sign Schedule

Sign Reference	544
Height	600mm
Width	679mm
Area *	0.24 m ²
Material	Class RA2 (12899-1:2007)
Mount Height	-
* Area reduced for rounded corners.	

Scheme Ref.	Hurricane Way Zebra Crossing 18/19	
Sign Ref.	7014	x-height 62.5
Letter colour	WHITE	SIGN FACE
Background	RED	Width 775mm
Border	WHITE	Height 565mm
Material	Class RA2 (12899-1:2007)	Area 0.44m ²

Note: Diagram 7014 is a temporary sign and is to be removed after 3 months

C:\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\HURRICANE WAY, WOODLEY\DRAWINGS\AUTOCAD\HURR_GA_002_B.DWG 26/03/2018 14:58:22 Tim Allen



WARNING:
Presence of existing services within vicinity of excavation works, including BT, THAMES WATER, VIRGIN, SGN, SSE (HV & LV)
Refer to stats information, trial hole results and risk assessments

Note:
Studs to be placed either side of proposed 2.8m wide crossing point and tactile (155mm max)

Key

- Road markings to TSRGD specification (white screed)
- Road markings to TSRGD specification (yellow screed)
- New area of red high friction surface overlay (hot lay)
- New area of charcoal high friction surface overlay (hot lay)
- Road Markings to be removed



REV	DATE	BY	DESCRIPTION	CHK	APD
B	26/03/2018	JM	ZEBRA CROSSING RELOCATED FOLLOWING SAFETY AUDIT RECOMMENDATION	SR	TRA
A	19/01/2018	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: ISSUED FOR CONSTRUCTION

CLIENT: WOKINGHAM BOROUGH COUNCIL

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

PROJECT: HURRICANE WAY, WOODLEY PEDESTRIAN CROSSING UPGRADE

ARCHITECT: SIGNAL CROSSING SCHEMES 2018/19 GENERAL ARRANGEMENT

TITLE: SHEET 2 OF 2

SCALE @ A2:	1:250	CHECKED:	SR	APPROVED:	TRA
CAD FILE:	HURR_GA_002_B	DESIGN-DRAWN:	PMc	DATE:	February 2018
PROJECT No:	5049	DRAWING No:	HURR/GA/002	REV:	B

This page is intentionally left blank

C:\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\KEEPHATCH ROAD, WOKINGHAM\DRAWINGS\AUTOCAD\KEEP_GA_001.DWG 12/03/2018 11:00:19 Tim Allen

DO NOT SCALE

Notes:

- All dimensions are in metres unless stated otherwise.
- This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
- Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
- Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
- All setting out on site to be agreed with Engineer.
- All traffic management arrangements shall be carried out in accordance with Traffic Signs Manual Chapter 8
- All works are to be in accordance with Wokingham Borough Council specifications and standard details

Key

- Proposed bull-nosed kerb, refer to standard detail WSD/1100/001
- Proposed full height kerb, refer to standard detail WSD/1100/001
- Proposed transition kerb, refer to standard detail WSD/1100/001
- Proposed R305mm quadrant, refer to standard detail WSD/1100/001
- Tie into existing kerb line
- Existing footway construction to be broken out to a depth of 20mm (up to 70mm if required) and shall be prepared for an in-lay. Proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate).
 Refer to standard detail WSD/1100/13
- Existing carriageway surfacing to be overlaid. Proposed footway construction shall be:
 - 30mm medium graded surface course (6mm nominal size aggregate)
 - 95mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate)
 Refer to standard detail WSD/1100/13
- Proposed red colour tactile paving, refer to standard details WSD/1100/02 & 04
- Proposed area of carriageway patching, refer to standard detail WSD/900/002
- Items to be removed and tipped from site
- Proposed 100mm Ø ducting for electrical connection
- Proposed trial hole location
- Area of vegetation to be cleared (including hedges & trees) up to 2.5m height
Note: Extent of clearance to be agreed with engineer on site
- Proposed Belisha Beacon
Beacon:
LED Zebrite beacon
Supplier: Zebrite Ltd
Address: Unit 9, Mill House Farm, Billingford Road, North Elmham, Norfolk. NR20 5HN
Tel. 0845 003 7361

Belisha beacon post:
Signature CDM ZebraSafe - ZEB4-CDM 4 Band - assembly post 3.7m OAL (600pd)
Supplier: Signature
Address: Hainge Road, Tividale, Oldbury, West Midlands. B69 2NY
Tel. 0121 5570234
- Proposed 'keep left' non-illuminated bollard, refer to WSD/1100/005 for placement



Area of vegetation to be cut back to increase stop stopping distance visibility on approach, 3.65m x 90m in length
Note: Refer to GA/002 & S002 for full extents and setting out details.

Proposed 2.4m wide dropped crossing with red coloured tactile paving to be laid in line with opposite side. HB2 kerbs to be replaced with bull-nose and transition kerbs. Surrounding area of footway to be resurfaced to suit new levels.
Note: 0-5mm upstand on bull-nosed kerbs to avoid localised ponding

Existing water cover to be incorporated into tactiles

Proposed belisha beacon to be placed 500mm from kerb face and edge of tactiles

Note: Power for the beacon on the western side of Keephatch Road to be drawn from lamp column indicated

Important note : Trial holes to be carried out to determine presence and exact depth of existing buried services in the vicinity of excavation works

Proposed duct box in verge, refer to WSD/500/013

Existing tactiles to be removed and tipped from site and dropped kerbs to be replaced with HB2 kerbs
Note: Surrounding area of footway to be resurfaced to suit new levels

IMPORTANT NOTE: ELECTRICAL CONNECTION REQUIRED
Proposed 100mm ducting from existing lamp column to proposed belisha beacons

Important note : Presence of existing services within vicinity of excavation works, including BT, THAMES WATER, VIRGIN, DRAINAGE, SGN, SSE LV & SEW
Refer to stats information, trial hole results and risk assessments

IMPORTANT NOTE: ELECTRICAL CONNECTION REQUIRED
Proposed 100mm ducting from existing lamp column to proposed belisha beacons

Proposed refuge island, 1.5m island width and 2.4m wide crossing point, refer to WSD/1100/005 for details. Proposed non illuminated keep left bollards to be installed as labelled and belisha beacons to be installed with ducting as shown

Note: Power for the beacon on the eastern side of Keephatch Road and on the island to be drawn from lamp column indicated

Important note : Trial holes to be carried out to determine presence and exact depth of existing buried services in the vicinity of excavation works

Proposed 2.4m wide dropped crossing with red coloured tactile paving to be laid in line with opposite side. HB2 kerbs to be replaced with bull-nose and transition kerbs. Surrounding area of footway to be resurfaced to suit new levels.
Note: 0-5mm upstand on bull-nosed kerbs to avoid localised ponding

Existing tactiles to be removed and tipped from site and dropped kerbs to be replaced with HB2 kerbs
Note: Surrounding area of footway to be resurfaced to suit new levels

Existing pedestrian refuge island and non-illuminated bollards to be removed and tipped from site. Surrounding carriageway to be resurfaced to suit existing levels

Existing cover to be raised/lowered to suit new levels

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

REV	DATE	BY	DESCRIPTION	CHK	APD
B	12/03/2018	JM	VISIBILITY SPLAY & NOTE AMENDED	SR	TRA
B	24/01/2018	PMc	CDM AMENDMENTS ADDED	SR	TRA
A	12/01/2018	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: ISSUED FOR CONSTRUCTION



WOKINGHAM BOROUGH COUNCIL
Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

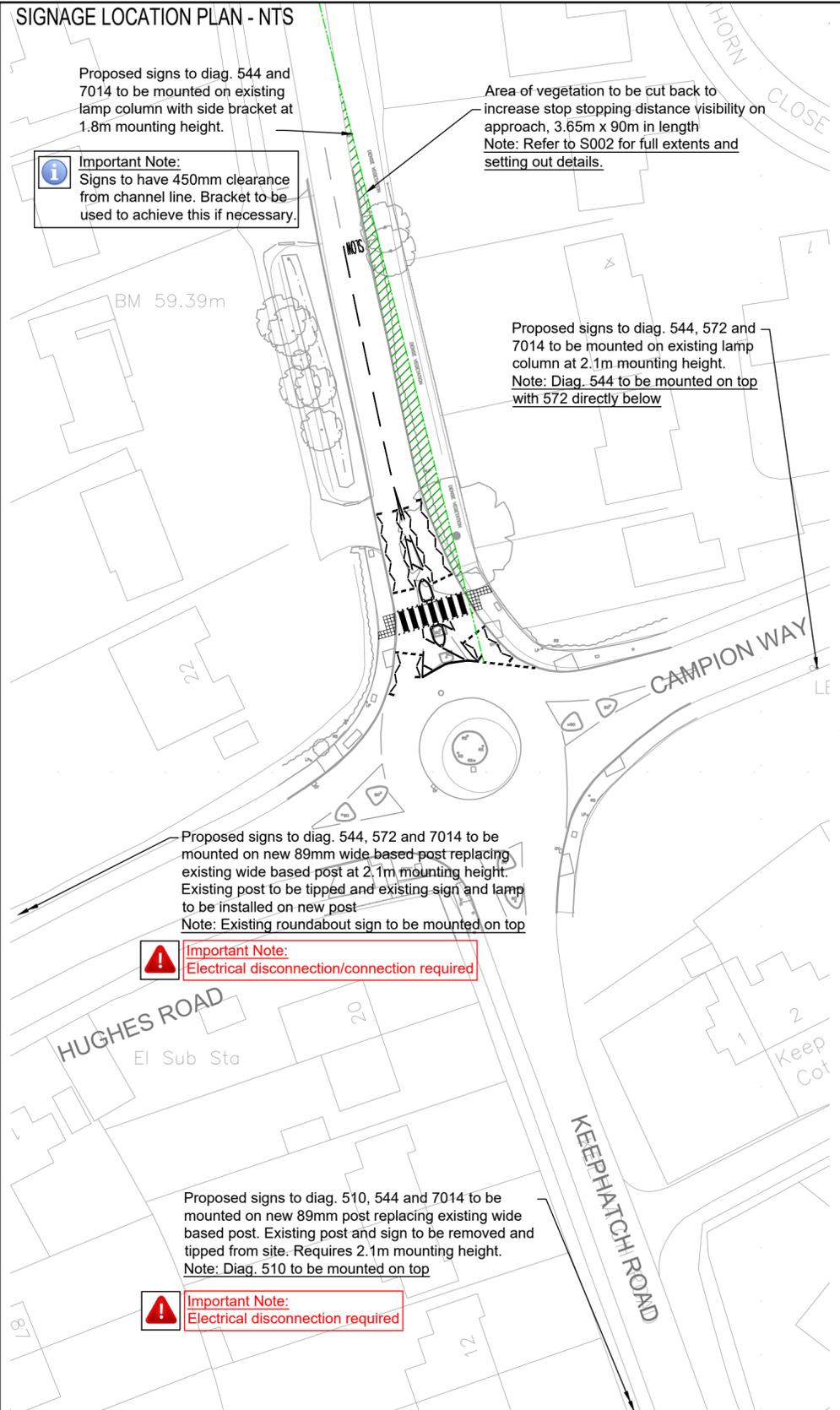
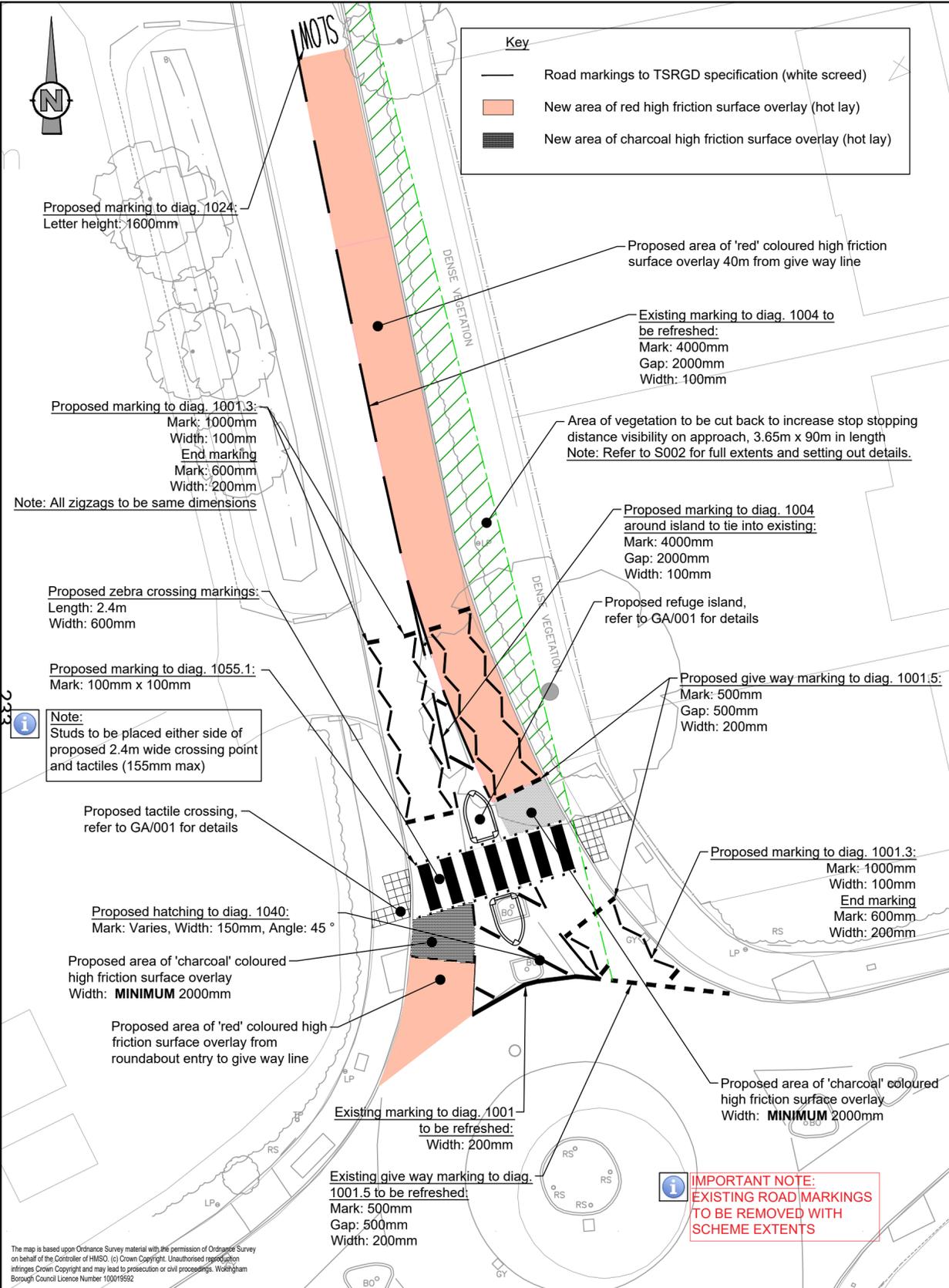
CLIENT:	WOKINGHAM BOROUGH COUNCIL
ARCHITECT:	

PROJECT:	KEEPHATCH ROAD, WOKINGHAM PEDESTRIAN CROSSING INSTALLATION
TITLE:	SIGNAL CROSSING SCHEMES 2018/19 GENERAL ARRANGEMENT

SCALE @ A2:	1:100	CHECKED:	SR	APPROVED:	TRA
CAD FILE:	KEEP_GA_001	DESIGN-DRAWN:	PMc	DATE:	December 2017
PROJECT No:	5049	DRAWING No:	KEEP/GA/001	REV:	C

This page is intentionally left blank

C:\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\KEEPHATCH ROAD, WOKINGHAM\DRAWINGS\AUTOCAD\KEEP_GA_002.DWG 12/03/2018 11:22:32 Tim Allen



DO NOT SCALE

- Notes**
- All dimensions are in metres unless stated otherwise.
 - This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
 - Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
 - All works are to be in accordance with Wokingham Borough Council specifications and standards.
 - Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
 - The works shall be programmed to ensure a clear footway is available for pedestrians throughout the works on or another side of the carriageway.
 - Diagram numbers to TSRGD refer to those set out in the Traffic Signs Regulations and General Directions 2002.
 - All signs and street furniture to have a minimal lateral clearance of 450mm from all kerb faces. refer to WBC standard detail WSD/1200/01 for further details.
 - All non-illuminated signs and supplementary plates to be retro-reflective.
 - All signs to be class RA2 material.
 - All road markings are to be white thermoplastic screed unless stated otherwise.

Sign Schedule

	Sign Reference: 510 Height: 600mm Width: 679mm Area: 0.24 m² Material: Class RA2 (12899-1:2007) Mount Height: - <small>* Area reduced for rounded corners.</small>
	Sign Reference: 544 Height: 600mm Width: 679mm Area: 0.24 m² Material: Class RA2 (12899-1:2007) Mount Height: - <small>* Area reduced for rounded corners.</small>
	Scheme Ref.: Keephatch Road Zebra Crossing 18/19 Sign Ref.: 7014 Letter colour: WHITE Background: RED Border: WHITE Material: Class RA2 (12899-1:2007) x-height: 62.5 SIGN FACE Width: 775mm Height: 565mm Area: 0.44m²
	Scheme Ref.: Keephatch Road Zebra Crossing 18/19 Sign Ref.: 572 Letter colour: BLACK Background: WHITE Border: BLACK Material: Class RA2 (12899-1:2007) x-height: 62.5 SIGN FACE Width: 375mm Height: 250mm Area: 0.09m²

- Diagram 7014 is a temporary sign and is to be removed no later than 3 months after construction is completed
- Diagram 572 to be used in either left or right orientation

REV	DATE	BY	DESCRIPTION	CHK	APD
C	12/03/2018	JM	VISIBILITY AND HIGH FRICTION SURFACING EXTENTS AMENDED	SR	TRA
B	24/01/2018	PMc	CDM AMENDMENTS ADDED	SR	TRA
A	12/01/2018	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: **ISSUED FOR CONSTRUCTION**

WOKINGHAM BOROUGH COUNCIL

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

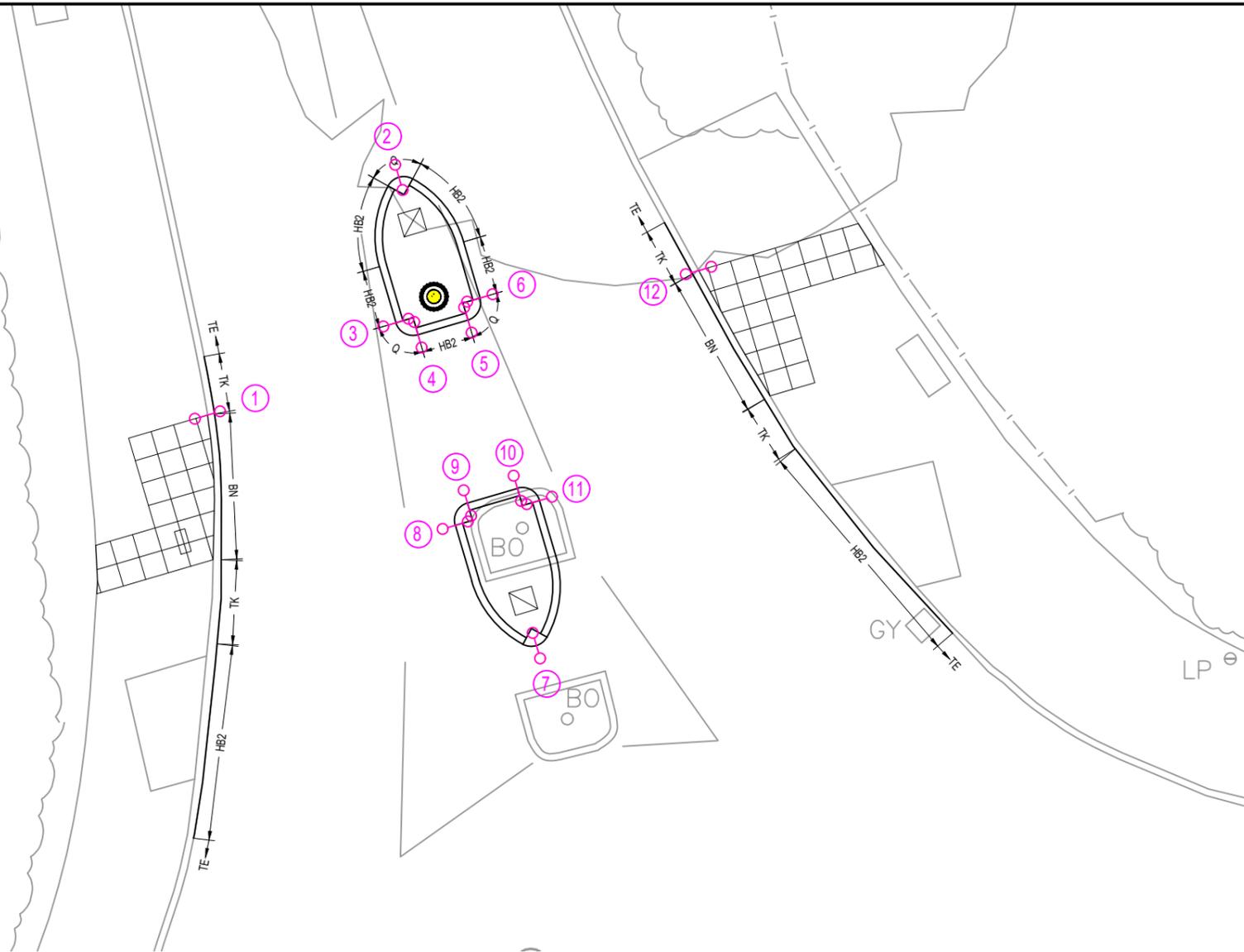
CLIENT:	WOKINGHAM BOROUGH COUNCIL
ARCHITECT:	

PROJECT:	KEEPHATCH ROAD, WOKINGHAM PEDESTRIAN CROSSING INSTALLATION
TITLE:	SIGNAL CROSSING SCHEMES 2018/19 ROAD MARKINGS & TRAFFIC SIGNS ARRANGEMENT

SCALE @ A2:	1:200	CHECKED:	SR	APPROVED:	TRA
CAD FILE:	KEEP_GA_002	DESIGN-DRAWN:	PMc	DATE:	December 2017
PROJECT No:	5049	DRAWING No:	KEEP/GA/002	REV:	C

This page is intentionally left blank

DO NOT SCALE



Key

- Setting out information as labelled
- Proposed tangent point

SETTING OUT POINTS		
Number	Eastings	Northings
1	482156.564	169318.786
2	482159.841	169322.847
3	482159.789	169320.364
4	482160.168	169320.158
5	482161.021	169320.409
6	482161.228	169320.787
7	482162.194	169314.842
8	482160.807	169316.901
9	482161.014	169317.280
10	482161.867	169317.531
11	482162.246	169317.324
12	482164.970	169321.257

Notes

1. All dimensions are in metres unless stated otherwise.
2. This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
3. Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
4. Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
5. Where applicable, existing manhole covers and utility covers are to be adjusted to new surfacing levels before the final surfacing takes place.
6. The works shall be programmed to ensure a clear footway is available for pedestrians throughout the works on or another side of the carriageway.
7. All setting out to be agreed on site with Engineer.
8. All traffic management arrangements shall be carried out in accordance with traffic signs manual chapter 8.
9. Refer to KEEP/GA/001 series for details of external works.
10. Refer to KEEP/GA/002 series for details of road markings layout.

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

A	12/01/18	PMc	FIRST ISSUED	SR	TRA
REV	DATE	BY	DESCRIPTION	CHK	APD
DRAWING STATUS: ISSUED FOR CONSTRUCTION					

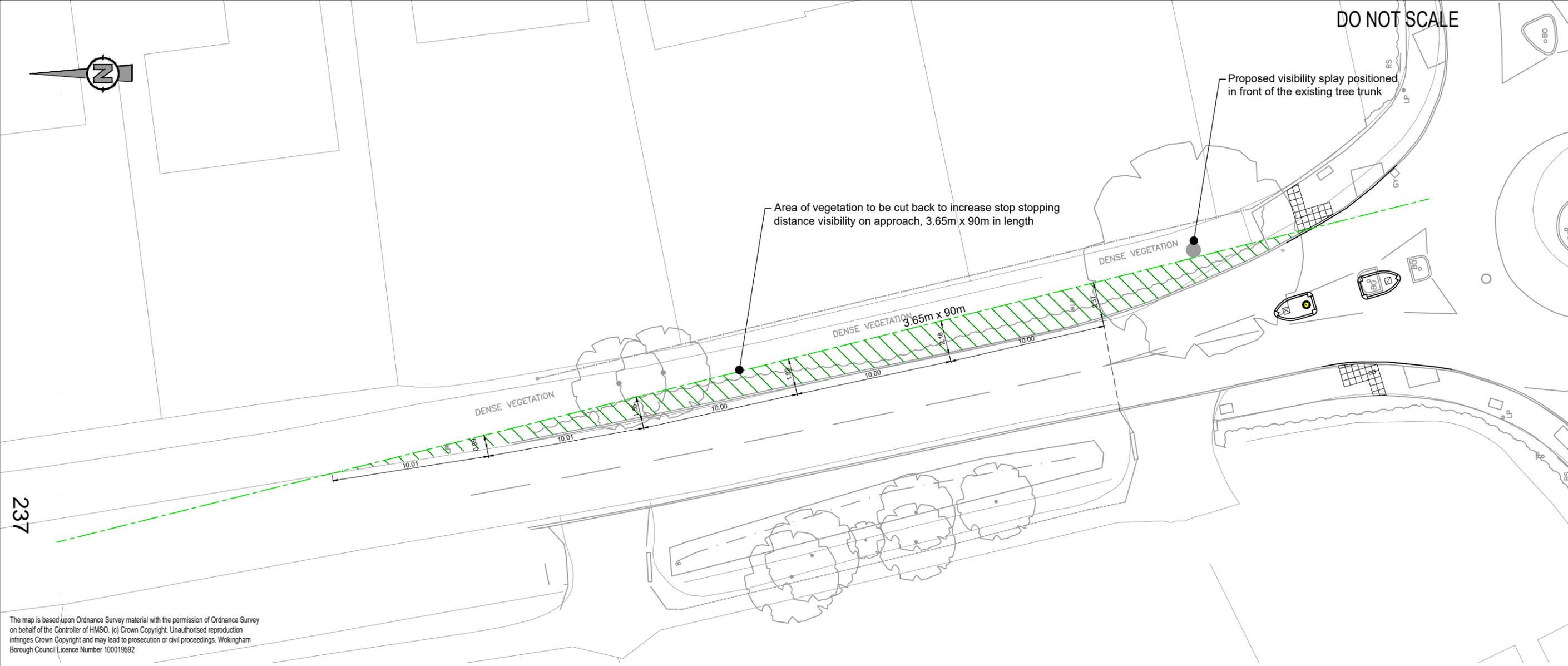


Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL	PROJECT: KEEPHATCH ROAD, WOKINGHAM PEDESTRIAN CROSSING INSTALLATION	SCALE @ A3: 1:100	CHECKED: SR	APPROVED: TRA
ARCHITECT:	TITLE: SIGNAL CROSSING SCHEMES 2018/19 SETTING OUT ARRANGEMENT	CAD FILE: KEEP_SO_001	DESIGN-DRAWN: PMc	DATE: December 2017
		PROJECT No: 5049	DRAWING No: KEEP/SO/001	REV: A
© WSP Group plc				

This page is intentionally left blank

C:\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\KEEPHATCH ROAD, WOKINGHAM\DRAWINGS\AUTOCAD\KEEP_SO_002.DWG 12/03/2018 12:11:42 Tim Allen



The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

Notes

1. All dimensions are in metres unless stated otherwise.
2. This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
3. Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
4. Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
5. Where applicable, existing manhole covers and utility covers are to be adjusted to new surfacing levels before the final surfacing takes place.
6. The works shall be programmed to ensure a clear footway is available for pedestrians throughout the works on or another side of the carriageway.
7. All setting out to be agreed on site with Engineer.
8. All traffic management arrangements shall be carried out in accordance with traffic signs manual chapter 8.

A	12.03.18	JM	FIRST ISSUED	SR	TRA
REV	DATE	BY	DESCRIPTION	CHK	APD
DRAWING STATUS: ISSUED FOR CONSTRUCTION					



WOKINGHAM BOROUGH COUNCIL

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL
ARCHITECT:

PROJECT: KEEPHATCH ROAD, WOKINGHAM PEDESTRIAN CROSSING INSTALLATION
TITLE: SIGNAL CROSSING SCHEMES 2018/19 VEGETATION CLEARANCE SETTING OUT

SCALE @ A3: 1:250	CHECKED: SR	APPROVED: TRA
CAD FILE: KEEP_SO_002	DESIGN-DRAWN: JM	DATE: March 2018
PROJECT No: 5049	DRAWING No: KEEP/SO/002	REV: A
© WSP Group plc		

This page is intentionally left blank

DO NOT SCALE



W:\WDC-FP2\DATA\ENG\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\LONDON ROAD TWYFORD\DRAWINGS\AUTOCAD\LON_GA_001.DWG 15/02/2018 11:04:36 Patrick McElroy

Important Note:
 Trial holes to be carried out to determine presence and exact depth of existing buried services in the vicinity of excavation works

Refer to drawing TS/001 for signal specifications

Proposed Traffic Signal Controller with base and feeder pillar
 Refer to drawing TS/001 for signal specifications including ducting and power supply

Important note:
 Presence of existing services within vicinity of excavation works, including BT, THAMES WATER, VIRGIN, SGN LP & SSE (HV & LV)
 Refer to stats information, trial hole results and risk assessments

Important Note:
 Existing tree to be cut back to highway boundary to a minimum height of 3.0m

- Notes:**
- All dimensions are in metres unless stated otherwise.
 - This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
 - Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
 - Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
 - All setting out on site to be agreed with Engineer.
 - All traffic management arrangements shall be carried out in accordance with Traffic Signs Manual Chapter 8
 - All works are to be in accordance with Wokingham Borough Council specifications and standard details

IMPORTANT NOTE:
 SCHEME TO BE CARRIED OUT IN CONJUNCTION WITH PLANNED STRUCTURAL MAINTENANCE SCHEME 2018/19 LONDON ROAD PHASE 1, REFER TO LONDON/GA/001 FOR DETAILS
 CONTRACTOR TO BE MAKE SURE BOTH SCHEMES INTERLINK WITH SIGNAL, ROAD MARKING AND SIGNAL WORKS

Existing wide based post and sign to be removed and tipped from site
 Note: Electrical disconnection required

Existing tactile paving in refuge island to be removed and tipped from site. Area to be resurfaced to suit carriageway levels.
 Refer to standard detail WSD/900/002

Important note:
 Trial holes to be carried out to determine presence and exact depth of existing buried services in the vicinity of excavation works

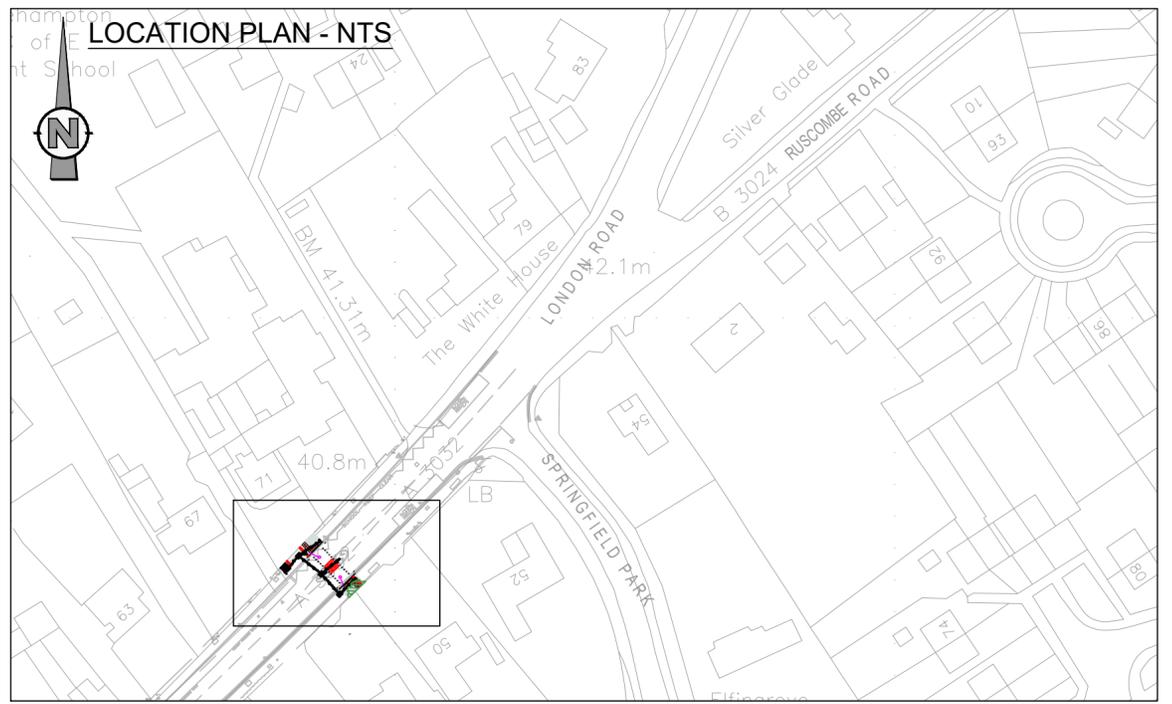
Existing crossing point to be widened to 2.4m centrally to existing islands. Existing tactiles to be removed and tipped from site and proposed red coloured tactile paving to be fitted and lined up with opposite side. Footway to be reinstated to suit new levels and existing kerbs to be replaced.
 Note: 0-5mm upstand required on dropped kerbs to avoid ponding

- Key**
- DK Proposed dropped kerb with transitions, refer to standard detail WSD/1100/001
 - TE Tie into existing kerb line
 - Existing footway construction to broken out to a depth of 20mm (up to 70mm if required) and shall be prepared for an in-lay.
 Proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate).
 Refer to standard detail WSD/1100/13
 - Proposed red colour tactile paving, refer to standard details WSD/1100/02 & 04
 - Items to be removed and tipped from site
 - Proposed trial hole location
 - x New 100mm duct no. as indicated.
 - Large duct box (600mm x 450mm)
 - Area of vegetation to be cut back up to 5.5m
 Note: Extents to be agreed with engineer on site

Setting out key:

SETTING OUT POINTS		
Number	Eastings	Northings
1	478989.781	176146.311
2	478983.167	176152.846

Proposed setting out point



REV	DATE	BY	DESCRIPTION	CHK	APD
C	15/02/2018	PMc	VEGETATION REMOVAL AMENDMENT ADDED	SR	TRA
B	31/01/2018	PMc	CDM & TRAFFIC SIGNALS AMENDMENTS ADDED	AL	TRA
A	16/01/2018	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: ISSUED FOR CONSTRUCTION

WOKINGHAM BOROUGH COUNCIL
 Shute End, Wokingham, Berkshire RG40 1BN
 Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL
 ARCHITECT:

PROJECT: LONDON ROAD, TWYFORD
 SIGNALISED CROSSING INSTALLATION
 TITLE: SIGNAL CROSSING SCHEMES 2018/19
 GENERAL ARRANGEMENT

SCALE @ A2: 1:100	CHECKED: SR	APPROVED: TRA
CAD FILE: LON_GA_001	DESIGN-DRAWN: PMc	DATE: January 2018
PROJECT No: 5049	DRAWING No: LON/GA/001	REV: C

This page is intentionally left blank

DO NOT SCALE

Notes

1. All dimensions are in metres unless stated otherwise.
2. This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
3. Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
4. All works are to be in accordance with Wokingham Borough Council specifications and standards.
5. Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
6. The works shall be programmed to ensure a clear footway is available for pedestrians throughout the works on or another side of the carriageway.
7. Diagram numbers to TSRGD refer to those set out in the Traffic Signs Regulations and General Directions 2002.
8. Mounting heights of all signs to be
 - Footway 2.1m
 - Cycleways 2.4m
 - Verges and non-pedestrian areas as directed by the engineer (normally) 1.8m.
 Refer to WBC standard detail WSD/1200/01 for further details.
9. All signs and street furniture to have a minimal lateral clearance of 450mm from all kerb faces. refer to WBC standard detail WSD/1200/01 for further details.
10. All non-illuminated signs and supplementary plates to be retro-reflective.
11. All signs to be class RA2 material.
12. All road markings are to be white thermoplastic screed unless stated otherwise.



IMPORTANT NOTE:
SCHEME TO BE DONE IN CONJUNCTION WITH PLANNED STRUCTURAL MAINTENANCE SCHEME 2016/17 LONDON ROAD PHASE 1, REFER TO LONDON/GA/001 FOR DETAILS

Sign Schedule

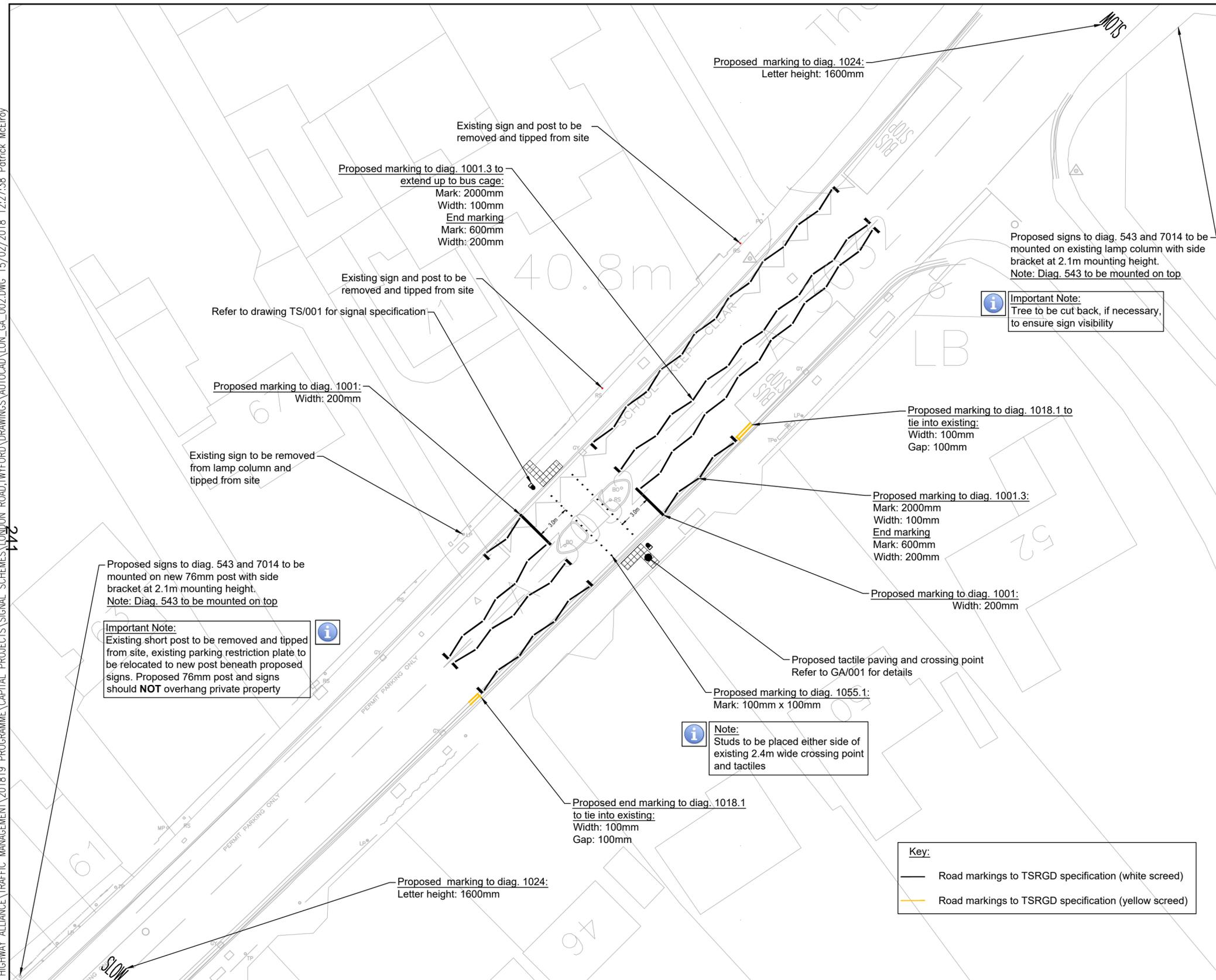


Sign Reference	543
Height	600mm
Width	679mm
Area *	0.24 m ²
Material	Class RA2 (12899-1:2007)
Mount Height	2300
* Area reduced for rounded corners.	



Scheme Ref.	London Road, Twyford 18/19	
Sign Ref.	7014	x-height 62.5
Letter colour	WHITE	SIGN FACE
Background	RED	Width 775mm
Border	WHITE	Height 565mm
Material	Class RA2 (12899-1:2007)	Area 0.44m ²

Diagram 7014 is a temporary sign and is to be removed no later than 3 months after construction is completed



Key:

- Road markings to TSRGD specification (white screed)
- Road markings to TSRGD specification (yellow screed)

W:\WDC-FP2\DATA\ENG\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\LONDON ROAD TWYFORD\DRAWINGS\AUTOCAD\LON_GA_002.DWG 15/02/2018 12:27:38 Patrick McElroy

REV	DATE	BY	DESCRIPTION	CHK	APD
B	07.02.2018	PMc	CDM & TRAFFIC SIGNALS AMENDMENTS ADDED	SR	TRA
A	17.01.2018	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: **ISSUED FOR CONSTRUCTION**

WOKINGHAM BOROUGH COUNCIL
 Shute End, Wokingham, Berkshire RG40 1BN
 Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: **WOKINGHAM BOROUGH COUNCIL**

ARCHITECT:

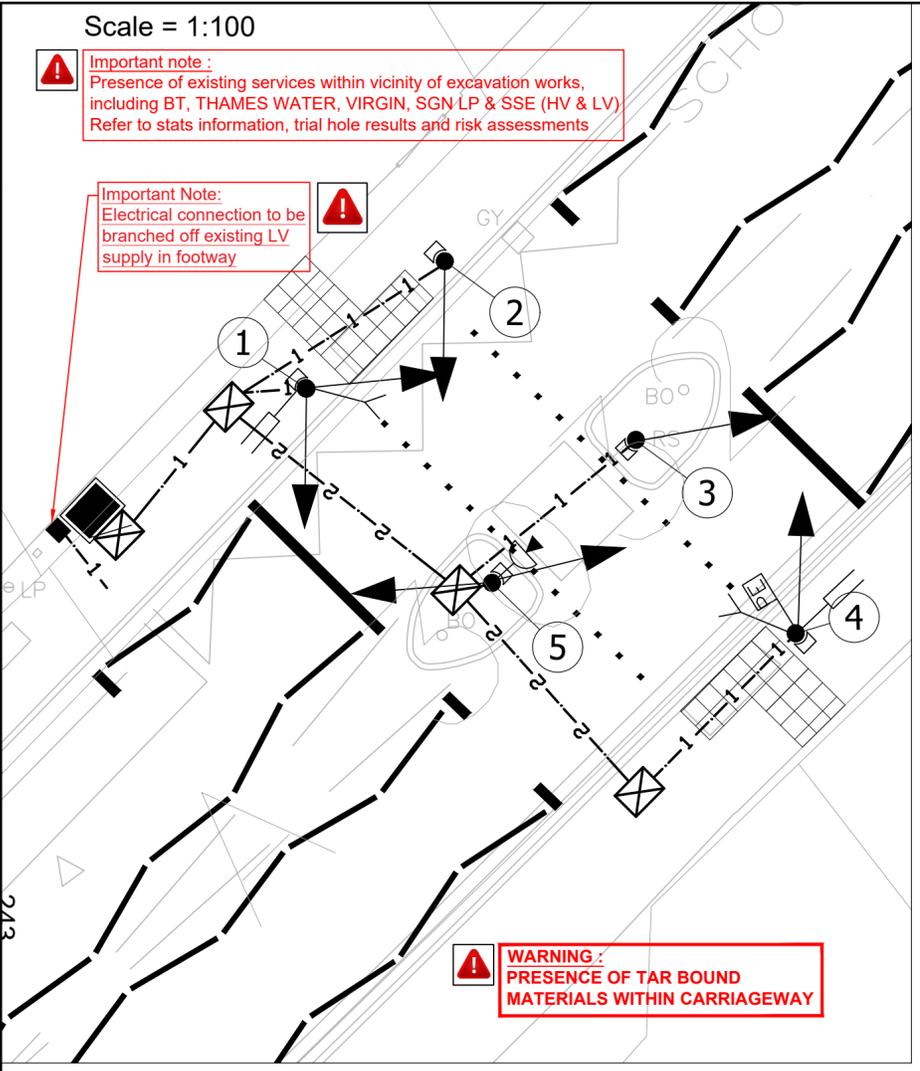
PROJECT: **LONDON ROAD, TWYFORD
 SIGNALISED CROSSING INSTALLATION**

TITLE: **SIGNAL CROSSING SCHEME 2018/19
 ROAD MARKINGS & TRAFFIC SIGNALS ARRANGEMENT**

SCALE @ A2:	1:250	CHECKED:	SR	APPROVED:	TRA
CAD FILE:	LON_GA_002	DESIGN-DRAWN:	PMc	DATE:	January 2018
PROJECT No:	5049	DRAWING No:	LON/GA/002	REV:	A

This page is intentionally left blank

W:\WDC-FP2\DATA\ENG\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\LONDON ROAD, TWYFORD\DRAWINGS\AUTOCAD\LON_TS_001.DWG 15/02/2018 11:05:14 Patrick McElroy

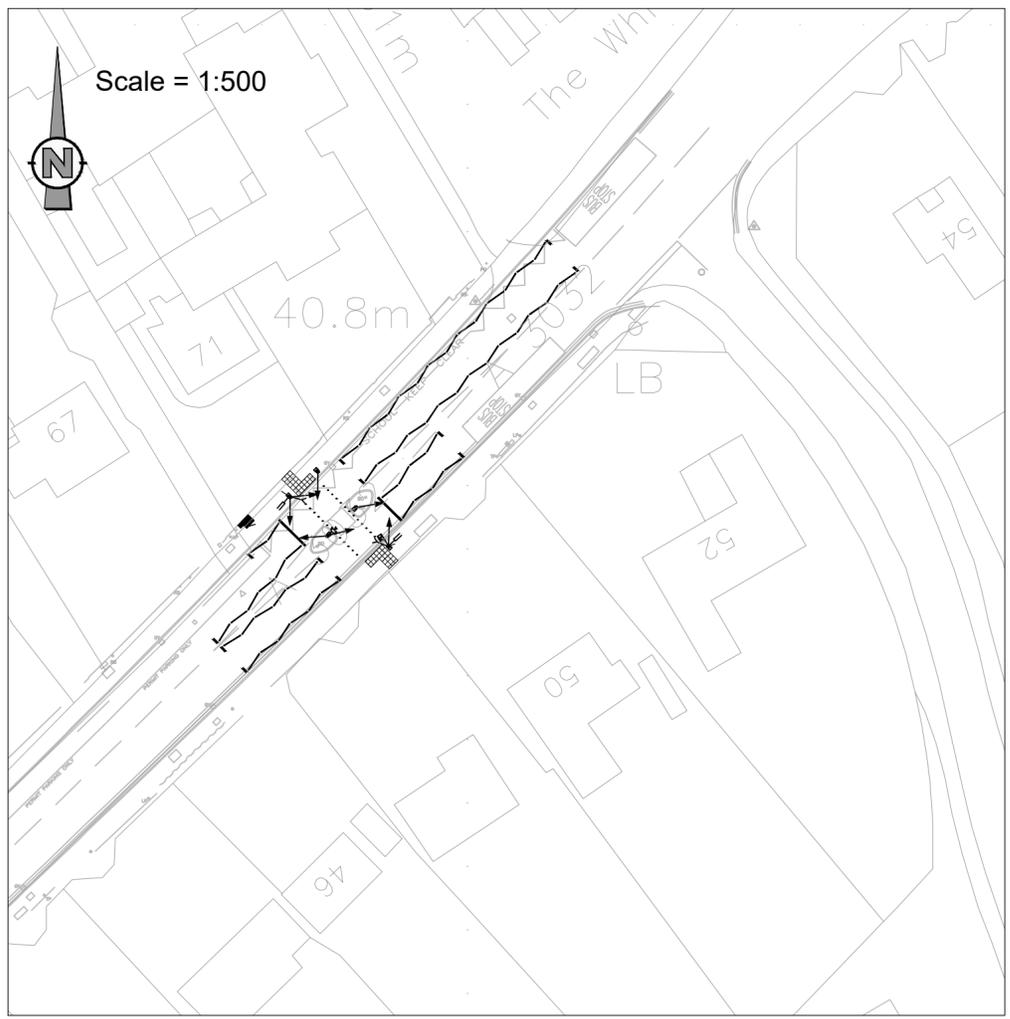


Important note:
 Presence of existing services within vicinity of excavation works, including BT, THAMES WATER, VIRGIN, SGN LP & SSE (HV & LV) Refer to stats information, trial hole results and risk assessments

Important Note:
 Electrical connection to be branched off existing LV supply in footway

WARNING:
 PRESENCE OF TAR BOUND MATERIALS WITHIN CARRIAGEWAY

IMPORTANT NOTE:
 SCHEME TO BE DONE IN CONJUNCTION WITH PLANNED STRUCTURAL MAINTENANCE SCHEME 2016/17 LONDON ROAD PHASE 1, REFER TO LONDON/GA/001 FOR DETAILS



DO NOT SCALE

- KEY**
- Signal pole & pole no.
 - RAG primary
 - Puffin nearside combined push button unit inc tactile and audible
 - On-crossing detector
 - Microwave Vehicle Detector (MVD)
 - Photocell for dimming
 - New 100mm duct no. as indicated.
 - Traffic signal controller with NAL base
 - Electricity supply feeder pillar
 - NAL RS115 DF retention socket
 - Large duct box (600mm x 450mm)

NOTES

1. Drawing to be read in conjunction with the works specification
2. Pre commencement meeting required between signals contractor/ civils contractor and WBC Engineer(s), meeting to be organised by civils contractor
3. The pole setting out details are only a guide, exact positions of poles, ducts, etc. shall be determined onsite by the Signals Engineer.
4. Ducting from RS115 NAL units to adjacent chamber and detector links on approaches to be 1x100mm unless indicated otherwise. all other runs shall be as indicated by number.
5. All new ducting shall be polypropelene, orange in color, smooth bore with 'TRAFFIC SIGNALS' imprinted on the duct. draw cords shall be included in all ducts.
6. All pedestrian nearside units shall have narrow field of view masks installed.
7. Where required pedestrian detection shall be mounted on extended brackets to ensure clear visibility of required detection area.
8. The Photocell for dimming shall be located on a signal head where it will not be affected by street lighting.
9. New signs to diag 7014 'NEW TRAFFIC SIGNALS AHEAD' are required on all signaled approaches, sign to be removed after 3 months by the contractor.
10. All signal heads to be ELV LED type. Where specified on the pole setting out table, poles to be rotated to 45° to ensure correct clearance of street furniture to edge of carriageway.
11. An ORANGE 100mm duct is required between controller and Haldo pillar. 50mm BLACK duct is required if SSE
12. All chambers, boxes and retention sockets to be NAL Ltd, as approved by WBC.
13. All equipment to be installed to manufacturers specifications unless otherwise agreed with the WBC engineer.
14. All poles to have pole numbers affixed prior to SAT.
15. Refer to WBC standard details WSD/500 series for ducting and chamber specifications

Pole and Socket type and setting out details

Socket Number	Pole Type	Distance - Pole Face to Stopline	Distance - Pole Face to Kerb Edge	Pole Rotation Requirements (see note 9)	NAL Retention Socket Type
1	4m standard (black)	0.5m to studs	0.5m	Rotate 45°	RSII5 Duck Foot
2	4m standard (black)	0.5m to studs	0.5m	Rotate 45°	RSII5 Duck Foot
3	4m standard (black)	0.5m to studs	0.5m	Rotate 45°	RSII5 Duck Foot
4	4m standard (black)	0.5m to studs	0.5m	Rotate 45°	RSII5 Duck Foot
5	4m standard (black)	0.5m to studs	0.5m	Rotate 45°	RSII5 Duck Foot

- Notes:**
1. Small form cabinet to be used
 2. Controller to be dual stream

REV	DATE	BY	DESCRIPTION	CHK	APD
B	31/01/2018	PMc	CDM & TRAFFIC SIGNALS AMENDMENTS ADDED	AL	TRA
A	17/01/2018	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: **ISSUED FOR CONSTRUCTION**

WOKINGHAM BOROUGH COUNCIL

Shute End, Wokingham, Berkshire RG40 1BN
 Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT:	WOKINGHAM BOROUGH COUNCIL
PROJECT:	LONDON ROAD, TWYFORD SIGNALISED CROSSING INSTALLATION
ARCHITECT:	SIGNAL CROSSING SCHEME 2018/19 TRAFFIC SIGNALS SPECIFICATION

SCALE @ A2:	1:100	CHECKED:	SR	APPROVED:	TRA
CAD FILE:	LON_TS_001	DESIGN-DRAWN:	PMc	DATE:	January 2018
PROJECT No:	5049	DRAWING No:	LON/TS/001	REV:	B

This page is intentionally left blank

DO NOT SCALE

Important Note - RELOCATED LAMP COLUMN:
ELECTRICAL DISCONNECTION/CONNECTION REQUIRED
 Proposed 100mm ducting from relocated column to existing LV line in footway.
 Electrical connection to be branched off existing LV supply.

WARNING:
OVERHEAD CABLES IN VICINITY OF WORKS

- Key**
- Proposed red colour tactile paving, refer to standard details WSD/1100/02 & 04
 - Existing footway construction to be broken out to a depth of 20mm (up to 70mm if required) and shall be prepared for an in-lay. Proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate).
 Refer to standard detail WSD/1100/13
 - Items to be removed and tipped from site
 - Siding to be undertaken
 - Proposed trial hole location
 - Proposed 100mm Ø ducting for electrical connection
 Refer to standard detail WSD/500/003

Proposed Belisha Beacon
 Beacon:
 LED Zebrite beacon
 Supplier: Zebrite Ltd
 Address: Unit 9, Mill House Farm, Billingford Road, North Elmham, Norfolk. NR20 5HN
 Tel. 0845 003 7361

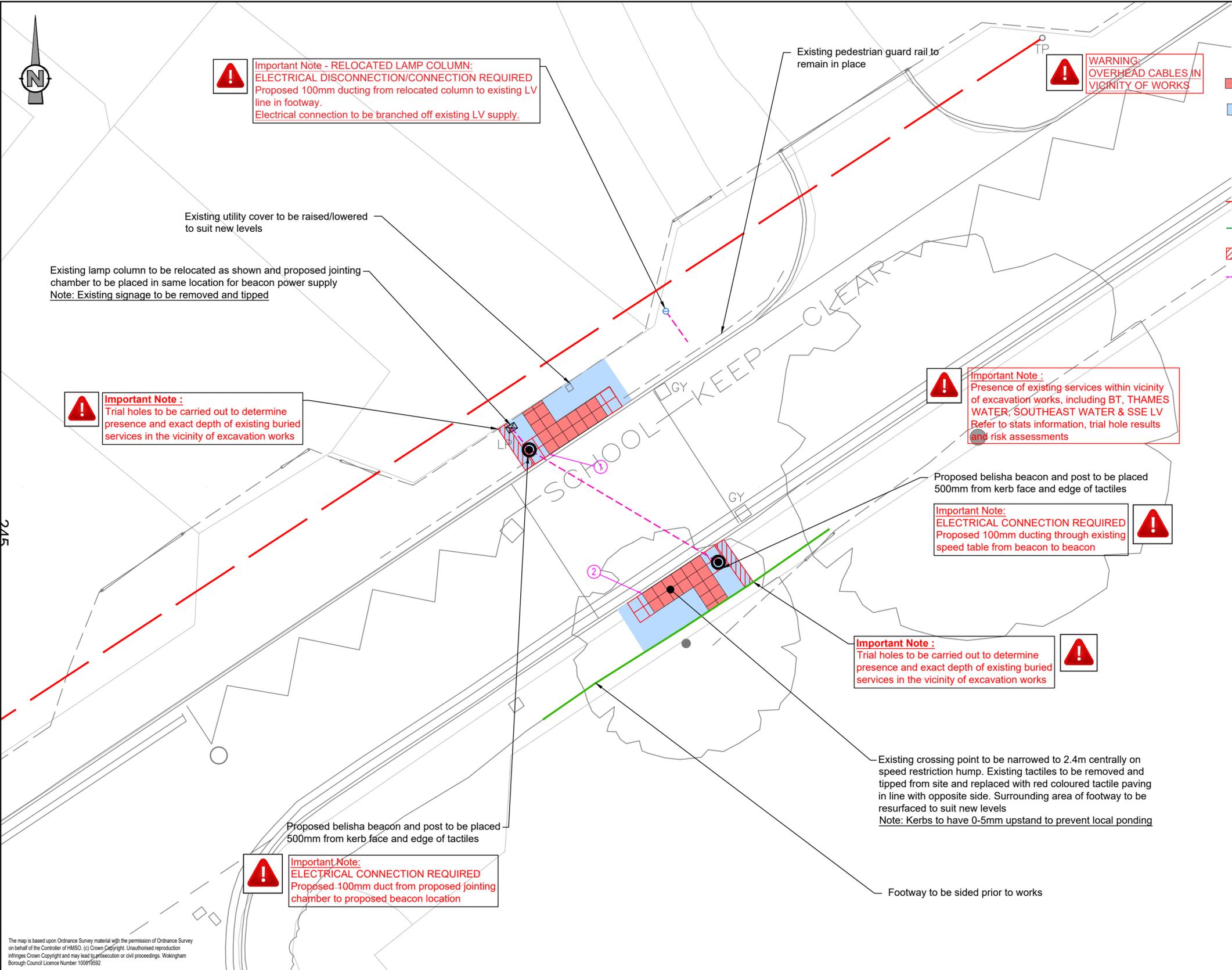
Belisha beacon post:
 Signature CDM Zebrosafe - ZEB4-CDM 4 Band - assembly post 3.7m OAL (600pd)
 Supplier: Signature
 Address: Hainge Road, Tividale, Oldbury, West Midlands. B69 2NY
 Tel. 0121 5570234

- Notes:**
1. All dimensions are in metres unless stated otherwise.
 2. This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
 3. Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
 4. Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
 5. All setting out on site to be agreed with Engineer.
 6. All traffic management arrangements shall be carried out in accordance with Traffic Signs Manual Chapter 8
 7. All works are to be in accordance with Wokingham Borough Council specifications and standard details

SETTING OUT POINTS		
Number	Eastings	Northings
1	480261.841	168801.124
2	480265.018	168796.332

Setting out points are at the front of tactile paving and back of kerb

C:\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\2018\19 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\MURRAY ROAD, WOKINGHAM\DRAWINGS\AUTOCAD\MURR_GA_001.DWG 19/02/2018 13:57:27 Trm Allen



REV	DATE	BY	DESCRIPTION	CHK	APD
A	30/01/2018	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: **ISSUED FOR CONSTRUCTION**



WOKINGHAM BOROUGH COUNCIL

Shute End, Wokingham, Berkshire RG40 1BN
 Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT:	WOKINGHAM BOROUGH COUNCIL
ARCHITECT:	

PROJECT:	MURRAY ROAD, WOKINGHAM PEDESTRIAN CROSSING INSTALLATION
TITLE:	SIGNAL SCHEMES 2018/19 GENERAL ARRANGEMENT

SCALE @ A2:	1:100	CHECKED:	SR	APPROVED:	TRA
CAD FILE:	MURR_GA_001	DESIGN-DRAWN:	PMc	DATE:	January 2018
PROJECT No:	5049	DRAWING No:	MURR/GA/001	REV:	A

This page is intentionally left blank

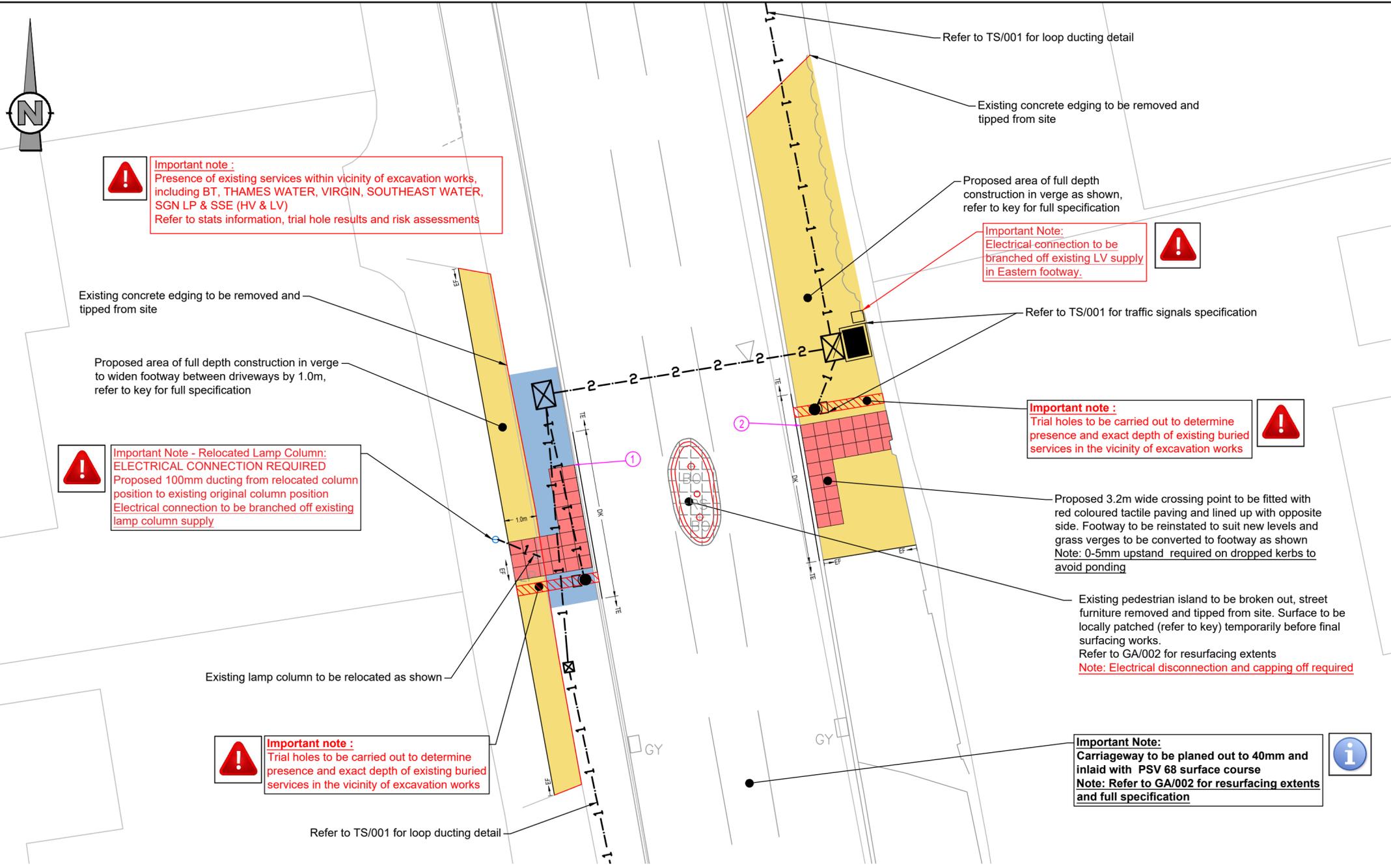
This page is intentionally left blank

C:\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\NEW WOKINGHAM ROAD, CROWTHORNE\DRAWINGS\AUTOCAD\NWR_CA_001.DWG 16/03/2018 10:35:44 Iulia Marin

DO NOT SCALE

Key

-  Proposed dropped kerb with transitions, refer to standard detail WSD/1100/001
-  Proposed precast concrete edging kerb, refer to standard detail WSD/1100/001
-  Tie into existing kerb line
-  Proposed red colour tactile paving, refer to standard details WSD/1100/02 & 04
-  Existing footway construction to be broken out to a depth of 20mm (up to 70mm if required) and shall be prepared for an in-lay. Proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate).
 Refer to standard detail WSD/1100/13
-  Existing surface to be dug out to a depth of 220mm, and prepared for an application of weed killer. Proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate)
 - 150mm type 1 sub-base material to clause 803
 Refer to standard detail WSD/1100/13
 Note: Standard geotextile to be laid underneath footway construction, standard geotextile terram T1000 or similar
-  Proposed area of carriageway patching, refer to standard detail WSD/900/002
-  Items to be removed and tipped from site
-  Proposed trial hole location
-  New 100mm duct no. as indicated.
-  Large duct box (600mm x 450mm)
-  Small duct box (300mm x 300mm)



Important note :
 Presence of existing services within vicinity of excavation works, including BT, THAMES WATER, VIRGIN, SOUTHEAST WATER, SGN LP & SSE (HV & LV)
 Refer to stats information, trial hole results and risk assessments

Important Note:
 Electrical connection to be branched off existing LV supply in Eastern footway.

Important note :
 Trial holes to be carried out to determine presence and exact depth of existing buried services in the vicinity of excavation works

Important note :
 Trial holes to be carried out to determine presence and exact depth of existing buried services in the vicinity of excavation works

Important Note:
 Carriageway to be planed out to 40mm and inlaid with PSV 68 surface course
 Note: Refer to GA/002 for resurfacing extents and full specification

Important Note - Relocated Lamp Column:
 ELECTRICAL CONNECTION REQUIRED
 Proposed 100mm ducting from relocated column position to existing original column position
 Electrical connection to be branched off existing lamp column supply

- Notes:**
1. All dimensions are in metres unless stated otherwise.
 2. This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
 3. Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
 4. Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
 5. All setting out on site to be agreed with Engineer.
 6. All traffic management arrangements shall be carried out in accordance with Traffic Signs Manual Chapter 8
 7. All works are to be in accordance with Wokingham Borough Council specifications and standard details

SETTING OUT POINTS		
Number	Eastings	Northings
1	483348.280	164512.844
2	483355.228	164514.054

 Proposed tangent point

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

A	09/02/2018	Pmc	FIRST ISSUED	SR	TRA
REV	DATE	BY	DESCRIPTION	CHK	APD
DRAWING STATUS: ISSUED FOR CONSTRUCTION					



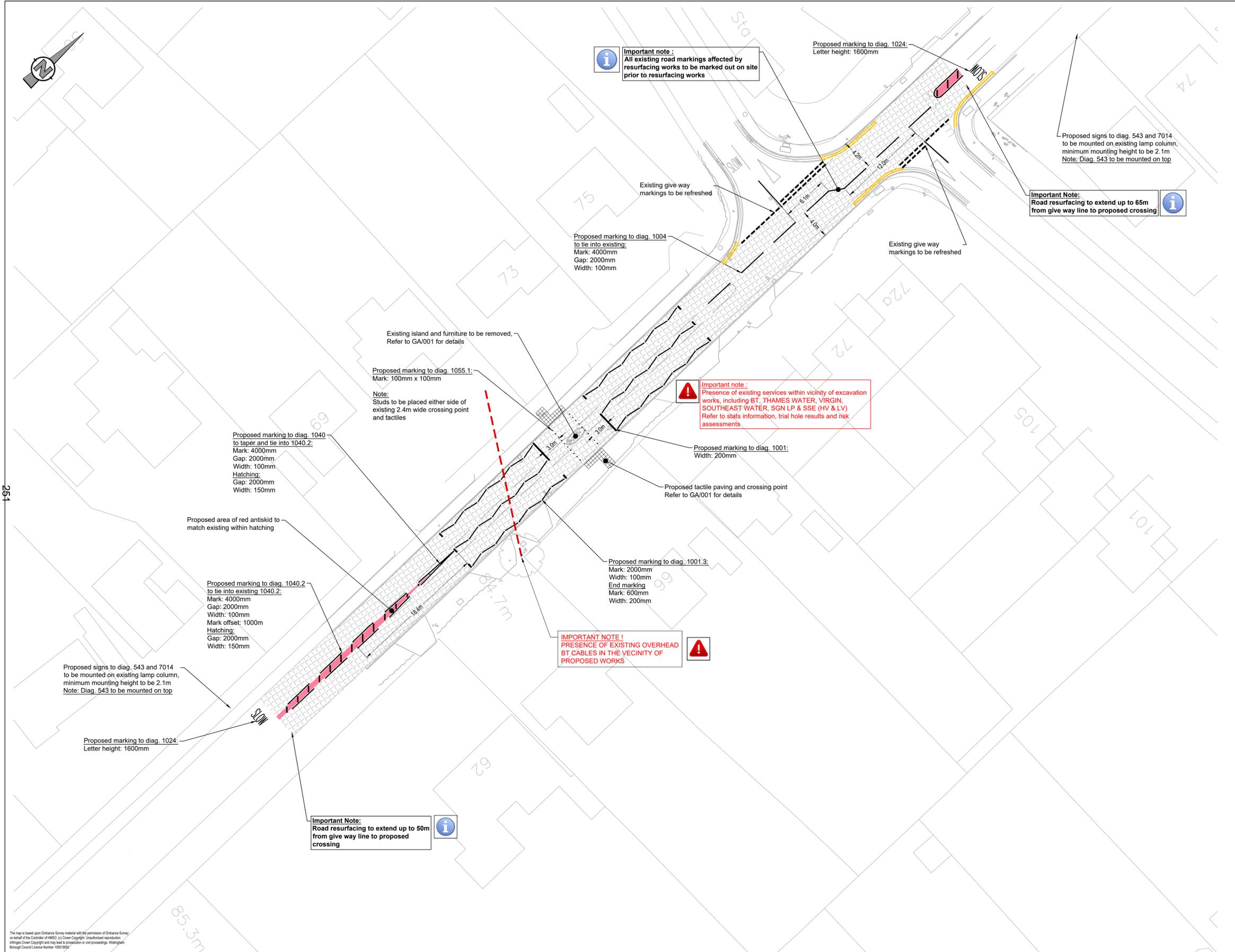
**WOKINGHAM
BOROUGH COUNCIL**

Shute End, Wokingham, Berkshire RG40 1BN
 Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL	PROJECT: NEW WOKINGHAM ROAD, CROWTHORNE PEDESTRIAN CROSSING INSTALLATION
ARCHITECT:	TITLE: SIGNAL CROSSING SCHEMES 2018/19 GENERAL ARRANGEMENT SHEET 1 OF 2

SCALE @ A2: 1:100	CHECKED: SR	APPROVED: TRA	
CAD FILE: NWR_GA_001	DESIGN-DRAWN: Pmc	DATE: February 2018	
PROJECT No: 5049	DRAWING No: NWR/GA/001	REV: A	

This page is intentionally left blank



- Notes**
- All dimensions are in metres unless stated otherwise.
 - This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
 - Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
 - All works are to be in accordance with Wokingham Borough Council specifications and standards.
 - Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
 - The works shall be programmed to ensure a clear footway is available for pedestrians throughout the works on or another side of the carriageway.
 - Diagram numbers to TSRGD refer to those set out in the Traffic Signs Regulations and General Directions 2002.
 - Mounting heights of all signs to be
 - Footway 2.1m
 - Cycleways 2.4m
 - Verges and non-pedestrian areas as directed by the engineer (normally) 1.8m.
 Refer to WBC standard detail WSD/1200/01 for further details.
 - All signs and street furniture to have a minimal lateral clearance of 450mm from all kerb faces. refer to WBC standard detail WSD/1200/01 for further details.
 - All non-illuminated signs and supplementary plates to be retro-reflective.
 - All signs to be class RA2 material.
 - All road markings are to be white thermoplastic screed unless stated otherwise.

- Key**
- Road markings to TSRGD specification (white screed)
 - Road markings to TSRGD specification (yellow screed)
 - New area of red high friction surface overlay (hot lay)
 - Proposed road construction:
Cold mill planing to 40mm and inlay with:
40mm thick thin surface course system to clause 942, site category Q/R, stress level 3, texture depth of 1.5mm. maximum AAV 12 and minimum PSV 68.

Sign Schedule

Sign Reference	543
Height	750mm
Width	750mm
Area *	0.29 m ²
Material	Class RA2 (12899-1:2007)
Mount Height	2300
* Area reduced for rounded corners.	

Scheme Ref.	NWR Ped Crossing 2018/19	x-height	62.5
Sign Ref.	7014	SIGN FACE	
Letter colour	WHITE	Width	775mm
Background	RED	Height	565mm
Border	WHITE	Area	0.44m ²
Material	Class RA2 (12899-1:2007)		

Note: Diagram 7014 is a temporary sign and is to be removed no later than 3 months after construction is completed

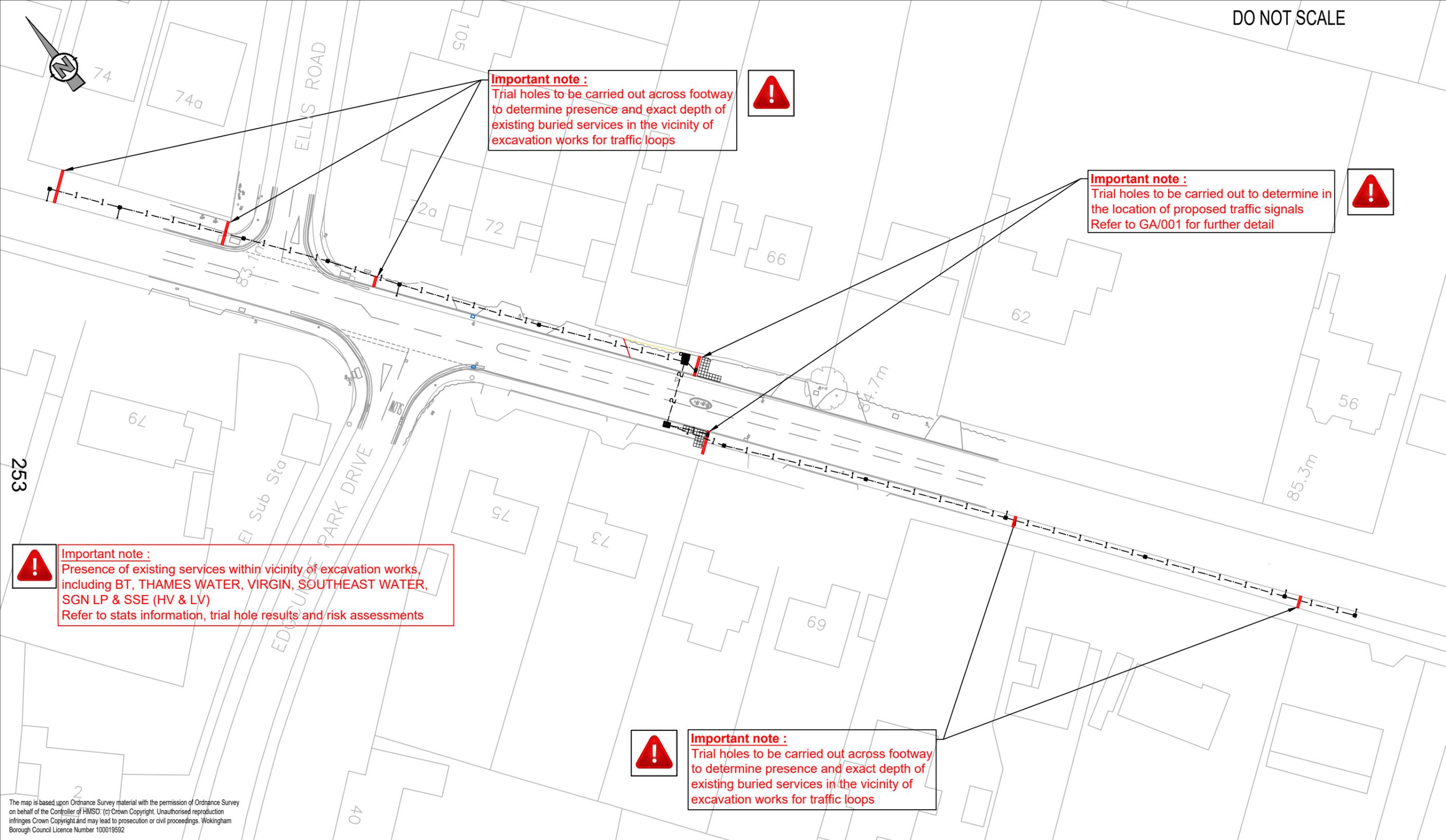
The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. © Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number: 10001952

 Shute End, Wokingham, Berkshire RG40 1BN Tel: (0118) 974 6000 Web: www.wokingham.gov.uk	CLIENT: WOKINGHAM BOROUGH COUNCIL	PROJECT: NEW WOKINGHAM ROAD, CROWTHORNE PEDESTRIAN CROSSING INSTALLATION	SCALE @ A1: 1:250	CHECKED: SR	APPROVED: TRA
ARCHITECT: TITLE: SIGNAL CROSSING SCHEMES 2018/19 GENERAL ARRANGEMENT SHEET 2 OF 2	CAD FILE: NWR_GA_002	DESIGN-DRAWN: PMc	DATE: February 2018	PROJECT No: 5049	DRAWING No: NWR/GA/002
REV: A 09/02/2018 PMc FIRST ISSUED REV: DATE BY DESCRIPTION CHK APD	DRAWING STATUS: ISSUED FOR CONSTRUCTION	REV: A	REV: A	REV: A	REV: A

This page is intentionally left blank

DO NOT SCALE

C:\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\NEW WOKINGHAM ROAD, CROWTHORNE\DRAWINGS\AUTOCAD\NWR_TH_001.DWG 16/03/2018 10:42:52 Iulia Margin



The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

A	20/02/18	PMc	FIRST ISSUED	SR	TRA
REV	DATE	BY	DESCRIPTION	CHK	APD
DRAWING STATUS: FOR INFORMATION ONLY					



WOKINGHAM BOROUGH COUNCIL

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT:
WOKINGHAM BOROUGH COUNCIL

ARCHITECT:

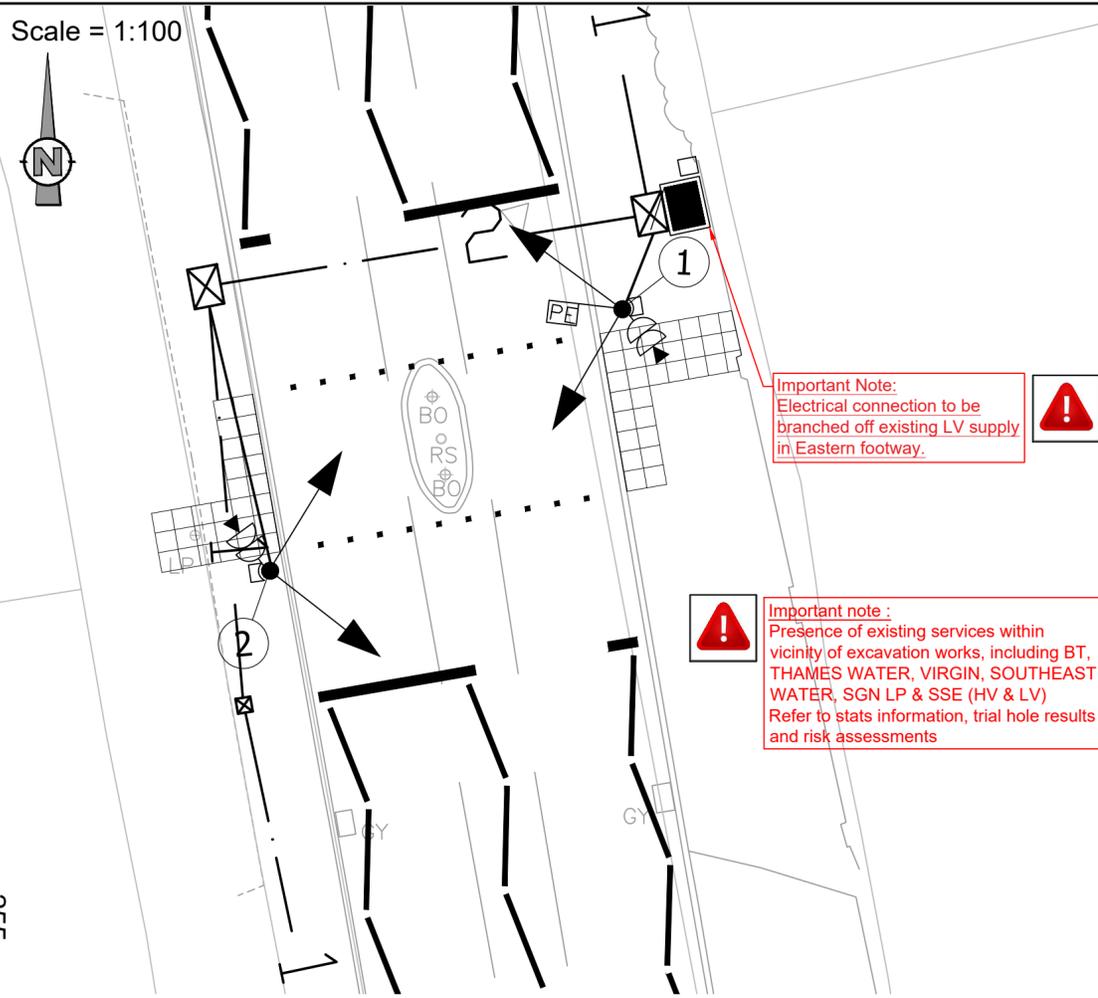
PROJECT:
**NEW WOKINGHAM ROAD, CROWTHORNE
PEDESTRIAN CROSSING INSTALLATION**

TITLE:
**SIGNAL CROSSING SCHEMES 2018/19
TRIAL HOLE ARRANGEMENT**

SCALE @ A3: 1:500	CHECKED: SR	APPROVED: TRA
CAD FILE: NWR_TH_001	DESIGN-DRAWN: PMc	DATE: February 2018
PROJECT No: 5049	DRAWING No: NWR/TH/001	REV: A
© WSP Group plc		

This page is intentionally left blank

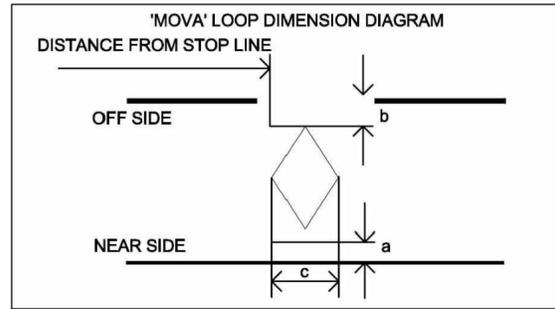
C:\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\NEW WOKINGHAM ROAD_CROWTHORNE\DRAWINGS\AUTOCAD\NWR_TS_001.DWG 16/03/2018 10:45:30 Julia Margin



Pole and Socket type and setting out details					
Socket Number	Pole Type	Distance - Pole Face to Stopline	Distance - Pole Face to Kerb Edge	Pole Rotation Requirements (see note 9)	NAL Retention Socket Type
1	4m standard (black)	0.5m to studs	0.5m	Rotate 45°	RS115 Duck Foot
2	4m standard (black)	0.5m to studs	0.5m	Rotate 45°	RS115 Duck Foot

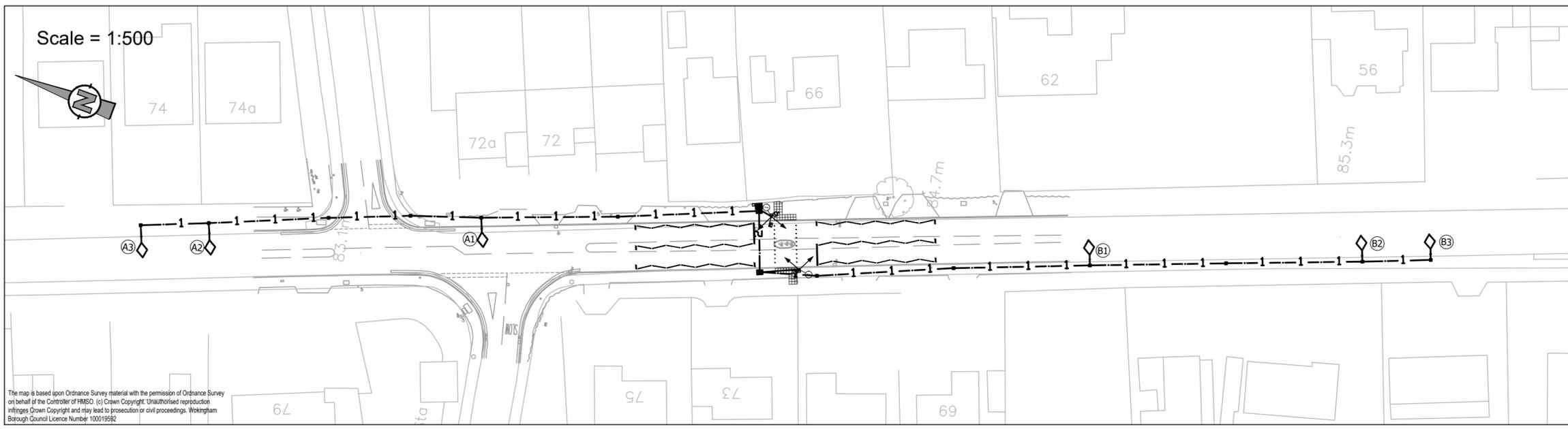
- Notes:**
- Small form cabinet to be used
 - Controller to be dual stream

Loop Details					
Loop Number	Approximate Location (FSL)	Distance from nearside (a)	Distance from nearside (b)	Loop Width (c)	Loop type
A1	40	0.4	0.4	1.7	Diamond
A2	80	0.4	0.4	1.7	Diamond
A3	90	0.4	0.4	1.7	Diamond
B1	40	0.4	0.4	1.7	Diamond
B2	80	0.4	0.4	1.7	Diamond
B3	90	0.4	0.4	1.7	Diamond



DO NOT SCALE

- NOTES**
- Drawing to be read in conjunction with the works specification
 - Pre commencement meeting required between signals contractor/ civils contractor and WBC Engineer(s), meeting to be organised by civils contractor
 - The pole setting out details are only a guide, exact positions of poles, ducts, etc. shall be determined onsite by the Signals Engineer.
 - Ducting from RS115 NAL units to adjacent chamber and detector links on approaches to be 1x100mm unless indicated otherwise. all other runs shall be as indicated by number.
 - All new ducting shall be polypropelene, orange in color, smooth bore with 'TRAFFIC SIGNALS' imprinted on the duct. draw cords shall be included in all ducts.
 - All pedestrian nearside units shall have narrow field of view masks installed.
 - Where required pedestrian detection shall be mounted on extended brackets to ensure clear visibility of required detection area.
 - The Photocell for dimming shall be located on a signal head where it will not be affected by street lighting.
 - New signs to diag 7014 'NEW TRAFFIC SIGNALS AHEAD' are required on all signaled approaches, sign to be removed after 3 months by the contractor.
 - All signal heads to be ELV LED type. Where specified on the pole setting out table, poles to be rotated to 45° to ensure correct clearance of street furniture to edge of carriageway.
 - An ORANGE 100mm duct is required between controller and Haldo pillar. 50mm BLACK duct is required if SSE
 - All chambers, boxes and retention sockets to be NAL Ltd, as approved by WBC.
 - All equipment to be installed to manufacturers specifications unless otherwise agreed with the WBC engineer.
 - All poles to have pole numbers affixed prior to SAT.
 - Refer to WBC standard details WSD/500 series for ducting and chamber specifications



- KEY**
- Signal pole & pole no.
 - ➔ RAG primary
 - ☎ Puffin nearside combined push button unit inc tactile and audible
 - On-crossing detector
 - PE Photocell for dimming
 - x — New 100mm duct no. as indicated.
 - ☑ Traffic signal controller with NAL base
 - ◆ Electricity supply feeder pillar
 - ☐ NAL RS115 DF retention socket
 - ☒ Large duct box (600mm x 450mm)
 - ☒ Small duct box (300mm x 300mm)
 - Carriageway Loopbox
 - ◇ Inductive Loop

REV	DATE	BY	DESCRIPTION	CHK	APD
A	09/02/2018	PMc	FIRST ISSUED	SR	TRA
DRAWING STATUS: ISSUED FOR CONSTRUCTION					

WOKINGHAM BOROUGH COUNCIL

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL

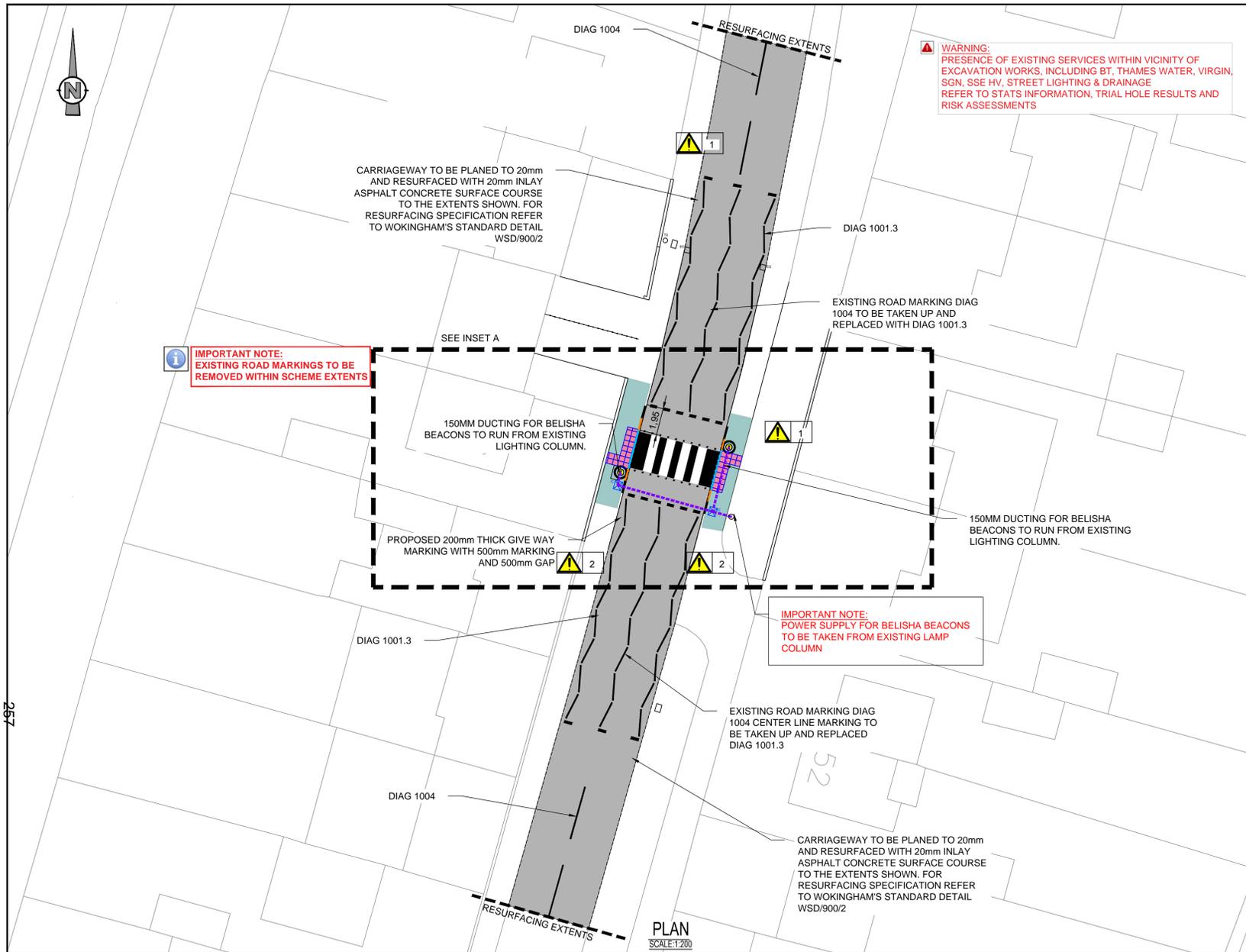
ARCHITECT:

PROJECT: NEW WOKINGHAM ROAD, CROWTHORNE PEDESTRIAN CROSSING INSTALLATION

TITLE: SIGNAL CROSSING SCHEMES 2018/19 TRAFFIC SIGNALS ARRANGEMENT

SCALE @ A2: 1:100	CHECKED: SR	APPROVED: TRA
CAD FILE: NWR_TS_001	DESIGN-DRAWN: PMc	DATE: February 2018
PROJECT No: 5049	DRAWING No: NWR/TS/001	REV: A

This page is intentionally left blank



HEALTH AND SAFETY INFORMATION

WORK SHALL ONLY BE CARRIED OUT BY SUITABLY TRAINED INDIVIDUALS, FOLLOWING AN INDUCTION. SIGNIFICANT OR EXCEPTIONAL RISKS ARE IDENTIFIED BELOW AND WITHIN THE DRAWING. FOR FURTHER CLARIFICATION RELATING TO THE ISSUES IDENTIFIED, CONTACT THE DESIGNER OR REFER TO HAZARD RISK REGISTER.

CONSTRUCTION PHASE

1. UNDERGROUND STREET LIGHTING ELECTRICAL CABLE.
2. GAS MAIN, DEPTH UNKNOWN.
3. UNDERGROUND BT CABLE
4. PATROL ROUTE FOR NEAR BY SCHOOL

MAINTENANCE / OPERATION / DECOMMISSIONING / DEMOLITION PHASES

1. PLEASE REFER TO THE HAZARD RISK REGISTER AND THE MAINTENANCE AND REPAIR STRATEGY FOR DETAILS OF THE RISKS ASSOCIATED WITH THE MAINTENANCE, OPERATION, DECOMMISSIONING AND DEMOLITION PHASES.
2. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH THE HEALTH AND SAFETY FILE(S) PRODUCED AND HANDED OVER ON COMPLETION OF THIS PROJECT.
3. RESIDUAL HAZARDS ARE LISTED HERE AND REFERENCED ON THE AS-BUILT DRAWING.

KEY

- EXISTING LIGHTING COLUMN
- EXISTING BT CHAMBER
- ⊗ EG EXISTING GULLY
- TS EXISTING TRAFFIC SIGN

- EXISTING FOOTWAY CONSTRUCTION TO BROKEN OUT AND PREPARED FOR AN INLAY INCLUDING AN APPLICATION OF A WEED KILLER. FOOTWAY CONSTRUCTION WHICH SHALL BE:
 - 20MM MEDIUM GRADED SURFACE COURSE (8MM NOMINAL SIZE AGGREGATE)
 - 50MM DENSE BITUMEN MACADAM BINDER COURSE TO CLAUSE 906 (20MM NOMINAL SIZE AGGREGATE).
 - 150mm TYPE 1 SUB BASE
 REFER TO STANDARD DETAIL WSD/1100/13

TRANSITION KERB FROM H2 TO BULL NOSE

BULL NOSE KERB

H2 KERB

Proposed Belisha Beacon

Beacon:
LED Zebrite beacon
Supplier: Zebrite Ltd
Address: Unit 9, Mill House Farm, Billingford Road, North Elmham, Norfolk, NR20 5HN
Tel: 0845 003 7361

Belisha beacon post:
Signature CDM Zebrasafe - ZEB4-CDM 4 Band - assembly post 3.7m OAL (600pd)
Supplier: Signature
Address: Hainge Road, Tivdale, Oldbury, West Midlands. B69 2NY
Tel: 0121 5570234

PROPOSED 300x300mm STREET LIGHTING INSPECTION CHAMBER

ROAD MARKINGS

DIAG 1004 - MARK 4000mm
GAP 2000mm
WIDTH 100mm

DIAG 1001.3 - MARK: 2000mm
WIDTH: 100mm
END MARKING: MARK 600mm
WIDTH: 200mm

DIAG 1055.1 - STUD 100mm x 100mm

ZEBRA CROSSING - LENGTH: 6000mm
WIDTH: 3000mm

SIGN SCHEDULE

NEW ZEBRA CROSSING AHEAD			
Scheme Ref.	Duffield Road Ped Crossing 2018/19		
Sign Ref.	7014	x-height	62.5
Letter colour	WHITE	SIGN FACE	
Background	RED	Width	775mm
Border	WHITE	Height	565mm
Material	Class RA2 (12899-1:2007)	Area	0.44m ²

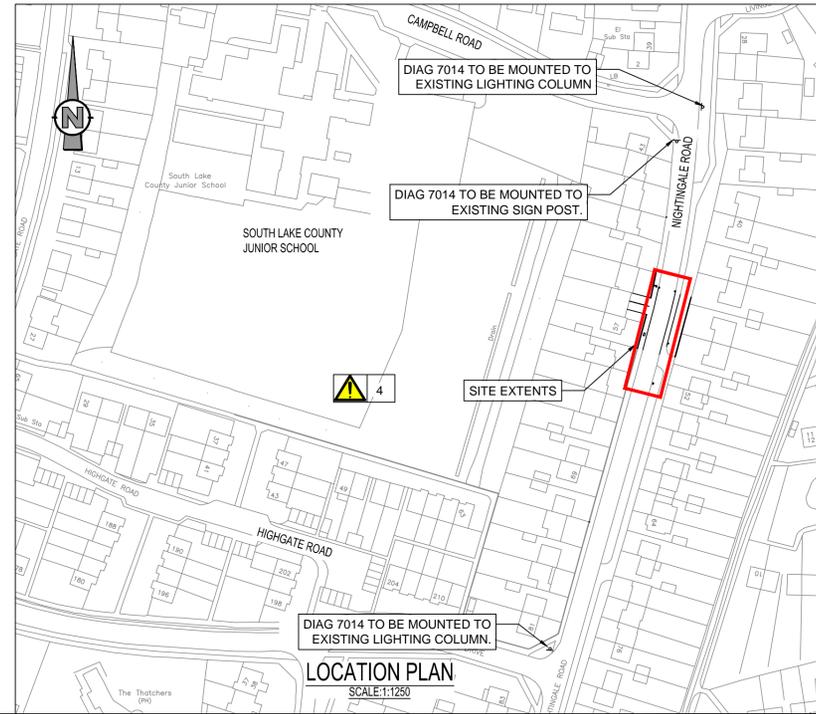
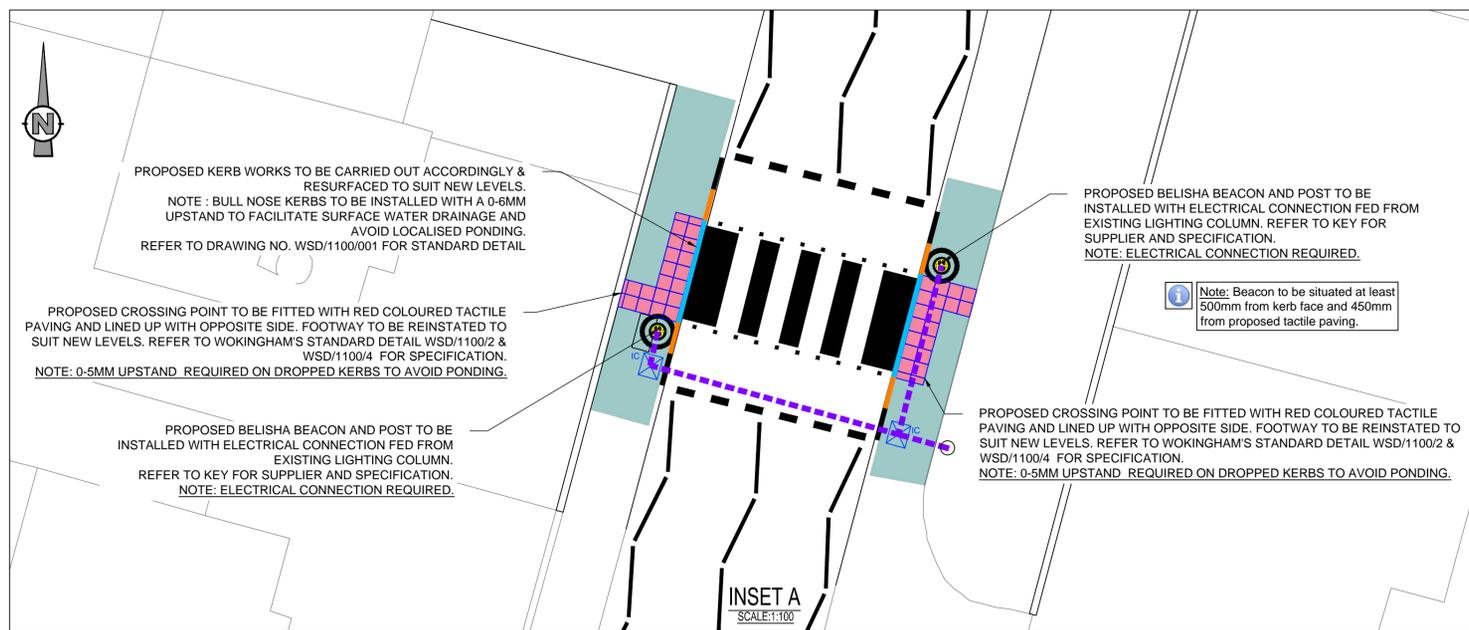
Note: Diagram 7014 is a temporary sign and is to be removed after 3 months

DO NOT SCALE

NOTES

1. ALL DIMENSIONS ARE IN METRES UNLESS STATED OTHERWISE.
2. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT ENGINEERING DETAILS, DRAWINGS AND SPECIFICATION.
3. ANY DISCREPANCIES SHOULD BE REPORTED TO THE ENGINEER IMMEDIATELY, SO THAT CLARIFICATION CAN BE SOUGHT PRIOR TO THE COMMENCEMENT OF WORKS.
4. ALL WORKS ARE TO BE IN ACCORDANCE WITH WOKINGHAM BOROUGH COUNCIL SPECIFICATIONS AND STANDARDS.
5. CONTRACTOR TO ESTABLISH ALL UTILITY AND DRAINAGE LOCATIONS AND COORDINATE SAFE WORKING PROCEDURES BEFORE ANY EXCAVATION WORKS TAKE PLACE.
6. THE WORKS SHALL BE PROGRAMMED TO ENSURE A CLEAR FOOTWAY IS AVAILABLE FOR PEDESTRIANS THROUGHOUT THE WORKS ON OR ANOTHER SIDE OF THE CARRIAGEWAY.
7. DIAGRAM NUMBERS TO TSRGD REFER TO THOSE SET OUT IN THE TRAFFIC SIGNS REGULATIONS AND GENERAL DIRECTIONS 2016.
8. MOUNTING HEIGHTS OF ALL SIGNS TO BE
 - FOOTWAY 2.1M
 - CYCLEWAYS 2.4M
 - VERGES AND NON-PEDESTRIAN AREAS AS DIRECTED BY THE ENGINEER (NORMALLY) 1.8M.
 REFER TO WBC STANDARD DETAIL WSD/1200/01 FOR FURTHER DETAILS.
9. ALL SIGNS AND STREET FURNITURE TO HAVE A MINIMAL LATERAL CLEARANCE OF 450MM FROM ALL KERB FACES. REFER TO WBC STANDARD DETAIL WSD/1200/01 FOR FURTHER DETAILS.
10. ALL NON-ILLUMINATED SIGNS AND SUPPLEMENTARY PLATES TO BE RETRO-REFLECTIVE.
11. ALL SIGNS TO BE CLASS RA2 MATERIAL.
12. ALL ROAD MARKINGS ARE TO BE WHITE THERMOPLASTIC SCREED UNLESS STATED OTHERWISE
13. SERVICE COVERS TO BE RAISED OR LOWERED TO NEW FORMATION LEVEL.

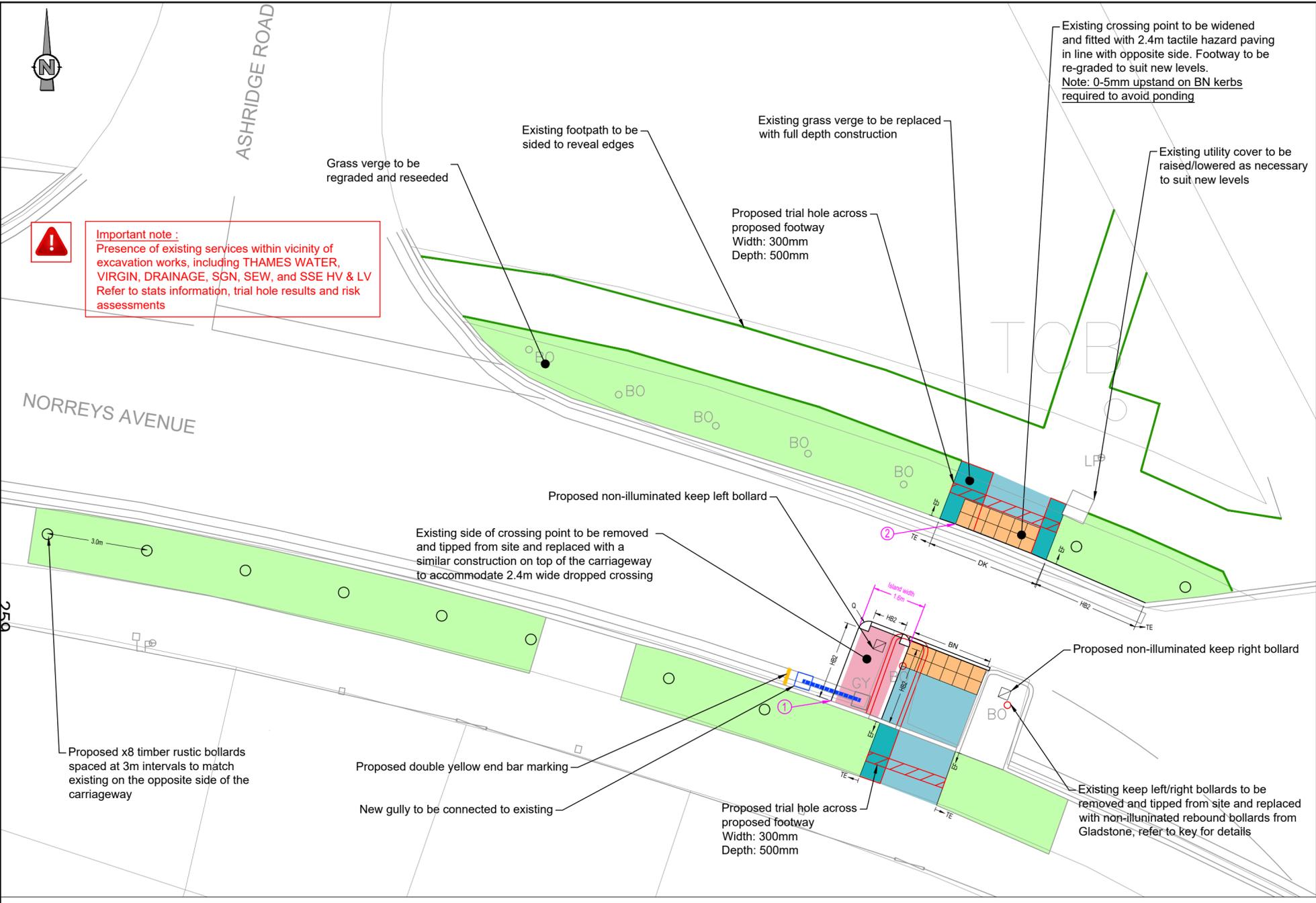
267



P01	02/08/2018	GS	FIRST ISSUE	MH	SG
REV	DATE	BY	DESCRIPTION	CHK	APP
DRAWING STATUS: S2 - FOR INFORMATION					
 1 Capital Quarter, Tyndall St, Cardiff, CF10 4BZ, UK T+ 44 (0) 292 076 9200 wsp.com					
CLIENT: WOKINGHAM BOROUGH COUNCIL					
SITE/PROJECT: NIGHTINGALE ROAD, WOODLEY ZEBRA CROSSING INSTALLATION					
TITLE: SIGNAL CROSSING SCHEMES 2018/19 GENERAL ARRANGEMENT					
SCALE @ A1:	AS SHOWN	CHECKED:	ET	APPROVED:	MH
PROJECT NO:	70048976	DESIGNED:	GS	DATE:	July 18
DRAWING NO:	NIGH/GA/001				REV:
					P01
© WSP UK Ltd					

This page is intentionally left blank

W:\WDC-FP2-DATA\ENG\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\NORREYS AVENUE\DRAWINGS\AUTOCAD\NORR_GA_001.DWG 14/12/2017 13:58:38 Patrick McElroy



DO NOT SCALE

Key

- Proposed full height kerb, refer to standard detail WSD/1100/001
- Proposed bull nose kerb, refer to standard detail WSD/1100/001
- Proposed dropped kerb with transitions, refer to standard detail WSD/1100/001
- Proposed precast concrete edging kerb, refer to standard detail WSD/1100/001
- Tie into existing kerb line
- Proposed buff colour tactile paving, refer to standard details WSD/1100/02 & 04
- Existing surface to be dug out to a depth of 220mm. proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate)
 - 150mm type 1 sub-base material to clause 803 refer to inset 3 and standard detail WSD/1100/13
 Note: Standard geotextile to be laid underneath footway construction, standard geotextile terram t1000 or similar
- Existing footway construction to be broken out to a depth of 20mm (up to 70mm if required) and shall be prepared for an in-lay including an application of a weed killer. Proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate)
 Refer to standard detail WSD/1100/13
- Existing surface to be dug out to a depth of 220mm or topped up on existing carriageway surface. Proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate)
 - 150mm type 1 sub-base material to clause 803 refer to inset 3 and standard detail WSD/1100/13
 Refer to standard detail WSD/1100/13
- Proposed area to be filled with topsoil to original depth, regraded to suit proposed levels and grass seeded
- Proposed trial hole location
- Siding to be carried out
- Item to be removed and tipped from site
- New gully frame and gully pot. Connection to existing gully as shown, refer to WSD/500/001 & 004
- Proposed keep left/right (as applicable) non-illuminated rebound bollard

- Notes:**
1. All dimensions are in metres unless stated otherwise.
 2. This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
 3. Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
 4. Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
 5. All setting out on site to be agreed with Engineer.
 6. All traffic management arrangements shall be carried out in accordance with Traffic Signs Manual Chapter 8
 7. All works are to be in accordance with Wokingham Borough Council specifications and standard details

SETTING OUT POINTS			
Number	Eastings	Northings	Notes
1	481849.277	169300.884	Front of HB2 Kerb
2	481852.990	169306.148	

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019562

A	14/12/2017	Pmc	FIRST ISSUED	SR	TRA
REV	DATE	BY	DESCRIPTION	CHK	APD
DRAWING STATUS: ISSUED FOR CONSTRUCTION					

**WOKINGHAM
BOROUGH COUNCIL**

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: <p style="text-align: center;">WOKINGHAM BOROUGH COUNCIL</p>	PROJECT: <p style="text-align: center;">NORREYS AVENUE, WOKINGHAM PEDESTRIAN CROSSING UPGRADE</p>
ARCHITECT: 	TITLE: <p style="text-align: center;">CAPITAL SCHEMES 2018/19 GENERAL ARRANGEMENT</p>

SCALE @ A2:	1:100	CHECKED:	SR	APPROVED:	TRA
CAD FILE:	NORR_GA_001	DESIGN-DRAWN:	Pmc	DATE:	November 2017
PROJECT No:	5049	DRAWING No:	NORR/GA/001	REV:	A

This page is intentionally left blank

DO NOT SCALE

Key

-  Proposed dropped kerb with transitions, refer to standard detail WSD/1100/001
-  Proposed precast concrete edging kerb, refer to standard detail WSD/1100/001
-  Tie into existing kerb line
-  Proposed red colour tactile paving, refer to standard details WSD/1100/02 & 04
-  Existing footway construction to be broken out to a depth of 20mm (up to 70mm if required) and shall be prepared for an in-lay. Proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate).
 Refer to standard detail WSD/1100/13
-  Existing surface to be dug out to a depth of 220mm. proposed footway construction shall be:
 - 20mm medium graded surface course (6mm nominal size aggregate)
 - 50mm dense bitumen macadam binder course to clause 906 (20mm nominal size aggregate)
 - 150mm type 1 sub-base material to clause 803
 Refer to standard detail WSD/1100/13
 Note: Standard geotextile to be laid underneath footway construction, standard geotextile terram T1000 or similar
-  Area of vegetation to be cleared (including hedges & trees) up to 2.5m height
Extent of clearance to be agreed with engineer on site
-  Items to be removed and tipped from side
-  Proposed trial hole location
-  New 100mm duct no. as indicated.
-  Large duct box (600mm x 450mm)

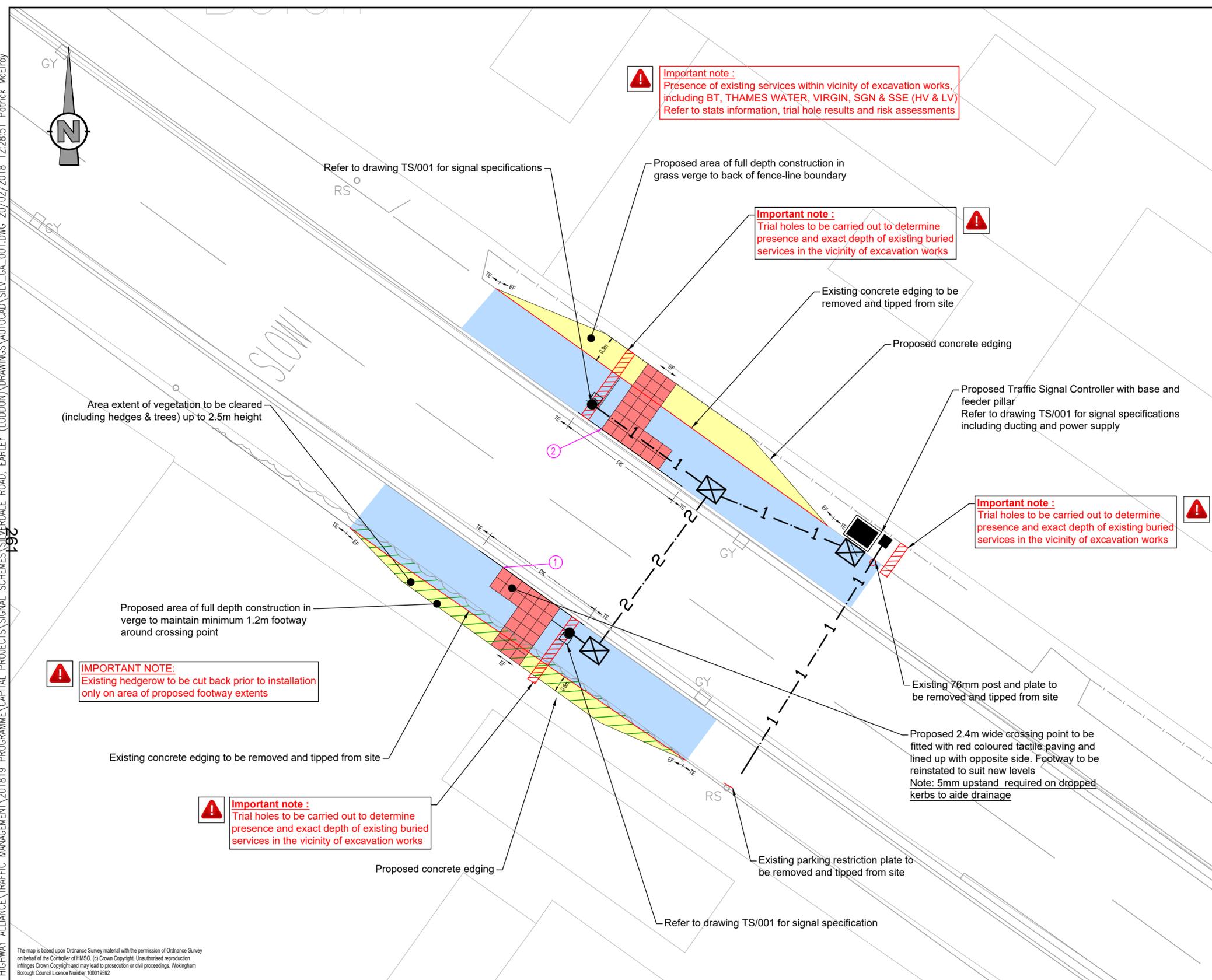
Notes:

1. All dimensions are in metres unless stated otherwise.
2. This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
3. Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
4. Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
5. All setting out on site to be agreed with Engineer.
6. All traffic management arrangements shall be carried out in accordance with Traffic Signs Manual Chapter 8
7. All works are to be in accordance with Wokingham Borough Council specifications and standard details

SETTING OUT POINTS

Number	Eastings	Northings
1	475829.263	171032.915
2	475832.640	171037.662

Setting out points are at the front of tactile paving and back of kerb



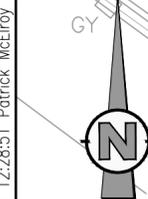
Important note :
Presence of existing services within vicinity of excavation works, including BT, THAMES WATER, VIRGIN, SGN & SSE (HV & LV)
Refer to stats information, trial hole results and risk assessments

Important note :
Trial holes to be carried out to determine presence and exact depth of existing buried services in the vicinity of excavation works

Important note :
Trial holes to be carried out to determine presence and exact depth of existing buried services in the vicinity of excavation works

IMPORTANT NOTE:
Existing hedgerow to be cut back prior to installation only on area of proposed footway extents

Important note :
Trial holes to be carried out to determine presence and exact depth of existing buried services in the vicinity of excavation works



W:\WDC-FP2-DATA\ENG\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\SILVERDALE ROAD, EARLEY (LODDON)\DRAWINGS\AUTOCAD\SILV_GA_001.DWG 20/02/2018 12:28:51 Patrick McElroy

REV	DATE	BY	DESCRIPTION	CHK	APD
C	20/02/2018	PMc	DUCTING ADDED	SR	TRA
B	24/01/2018	PMc	CDM AMENDMENTS ADDED	SR	TRA
A	21/11/2017	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: ISSUED FOR CONSTRUCTION



WOKINGHAM BOROUGH COUNCIL
Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT: WOKINGHAM BOROUGH COUNCIL
ARCHITECT:

PROJECT: SILVERDALE ROAD (LODDON), EARLEY PEDESTRIAN CROSSING INSTALLATION
TITLE: SIGNAL SCHEMES 2018/19 GENERAL ARRANGEMENT

SCALE @ A2: 1:100	CHECKED: SR	APPROVED: TRA
CAD FILE: SILV_GA_001	DESIGN-DRAWN: PMc	DATE: November 2017
PROJECT No: 5049	DRAWING No: SILV/GA/001	REV: C

This page is intentionally left blank

DO NOT SCALE

Notes

- All dimensions are in metres unless stated otherwise.
- This drawing should be read in conjunction with all other relevant engineering details, drawings and specification.
- Any discrepancies should be reported to the engineer immediately, so that clarification can be sought prior to the commencement of works.
- All works are to be in accordance with Wokingham Borough Council specifications and standards.
- Contractor to establish all utility and drainage locations and coordinate safe working procedures before any excavation works take place.
- The works shall be programmed to ensure a clear footway is available for pedestrians throughout the works on or another side of the carriageway.
- Diagram numbers to TSRGD refer to those set out in the Traffic Signs Regulations and General Directions 2002.
- Mounting heights of all signs to be
 - Footway 2.1m
 - Cycleways 2.4m
 - Verges and non-pedestrian areas as directed by the engineer (normally) 1.8m.
 Refer to WBC standard detail WSD/1200/01 for further details.
- All signs and street furniture to have a minimal lateral clearance of 450mm from all kerb faces. refer to WBC standard detail WSD/1200/01 for further details.
- All non-illuminated signs and supplementary plates to be retro-reflective.
- All signs to be class RA2 material.
- All road markings are to be white thermoplastic screed unless stated otherwise.

Sign Schedule



Sign Reference	543
Height	600mm
Width	679mm
Area *	0.24 m ²
Material	Class RA2 (12899-1:2007)
Mount Height	2300
* Area reduced for rounded corners.	



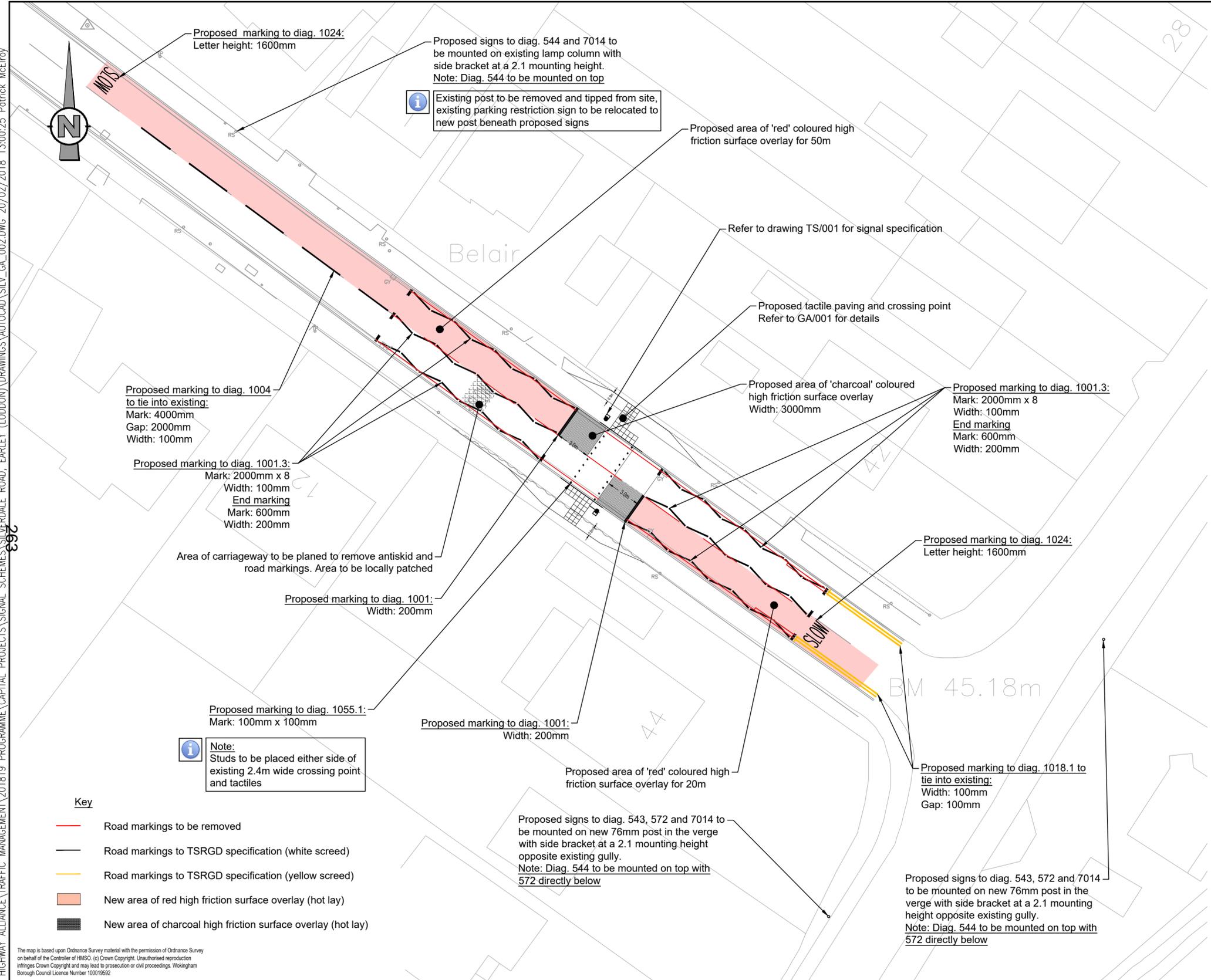
Scheme Ref.	Silverdale Road (Loddon) 18/19	
Sign Ref.	7014	x-height 62.5
Letter colour	WHITE	SIGN FACE
Background	RED	Width 775mm
Border	WHITE	Height 565mm
Material	Class RA2 (12899-1:2007)	Area 0.44m ²



Scheme Ref.	Silverdale Road (Loddon) 18/19	
Sign Ref.	572	x-height 62.5
Letter colour	BLACK	SIGN FACE
Background	WHITE	Width 375mm
Border	BLACK	Height 250mm
Material	Class RA2 (12899-1:2007)	Area 0.09m ²



- Diagram 7014 is a temporary sign and is to be removed no later than 3 months after construction is completed
- Diagram 572 to be used in either left or right orientation



Key

- Road markings to be removed
- Road markings to TSRGD specification (white screed)
- Road markings to TSRGD specification (yellow screed)
- New area of red high friction surface overlay (hot lay)
- New area of charcoal high friction surface overlay (hot lay)

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

REV	DATE	BY	DESCRIPTION	CHK	APD
B	24/01/2018	PMc	CDM AMENDMENTS ADDED	SR	TRA
A	21/11/2017	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: ISSUED FOR CONSTRUCTION

WOKINGHAM BOROUGH COUNCIL
 Shute End, Wokingham, Berkshire RG40 1BN
 Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT:	WOKINGHAM BOROUGH COUNCIL
ARCHITECT:	

PROJECT:	SILVERDALE ROAD (LODDON), EARLEY PEDESTRIAN CROSSING INSTALLATION
TITLE:	SIGNAL SCHEMES 2018/19 ROAD MARKINGS & SIGNS ARRANGEMENT

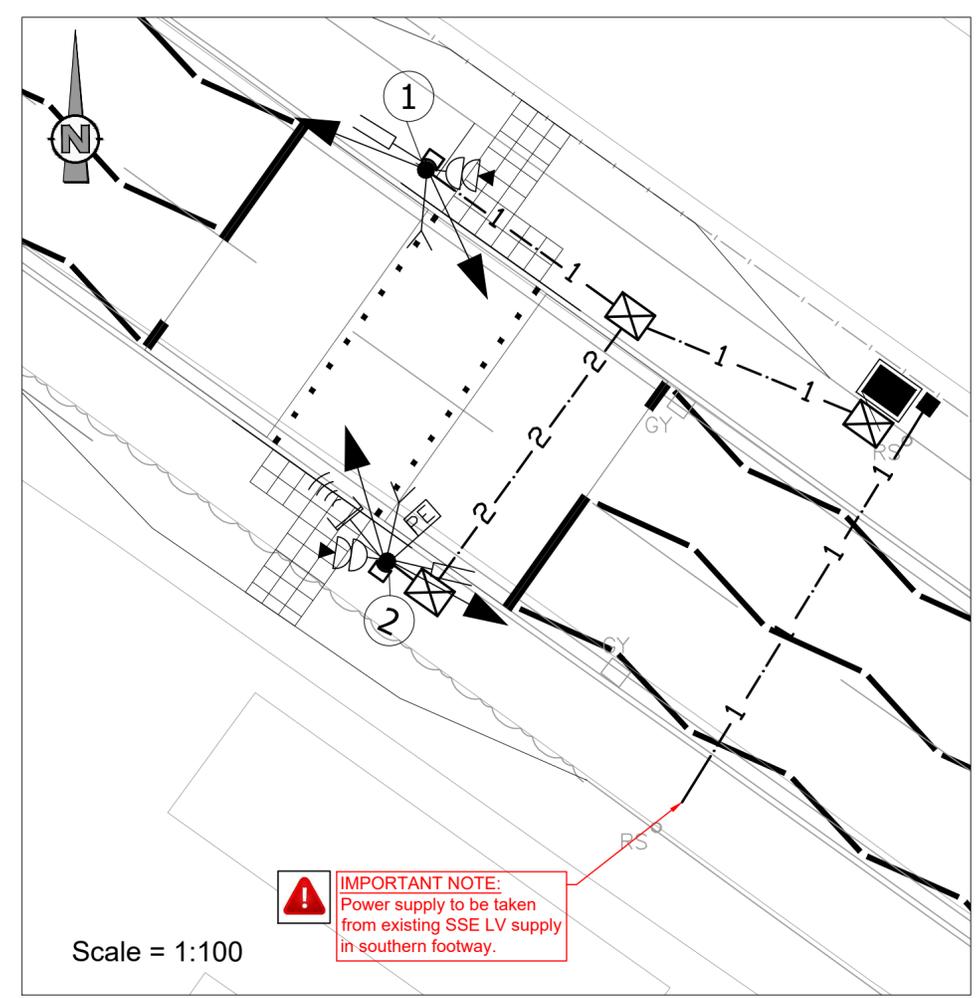
SCALE @ A2:	1:200	CHECKED:	SR	APPROVED:	TRA
CAD FILE:	SILV_GA_002	DESIGN-DRAWN:	PMc	DATE:	November 2017
PROJECT No:	5049	DRAWING No:	SILV/GA/002	REV:	B

W:\WDC-FP2\DATA\ENG\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\SILVERDALE ROAD, EARLEY (LODDON)\DRAWINGS\AUTOCAD\SILV_GA_002.DWG 20/02/2018 13:00:25 Patrick McElroy

This page is intentionally left blank

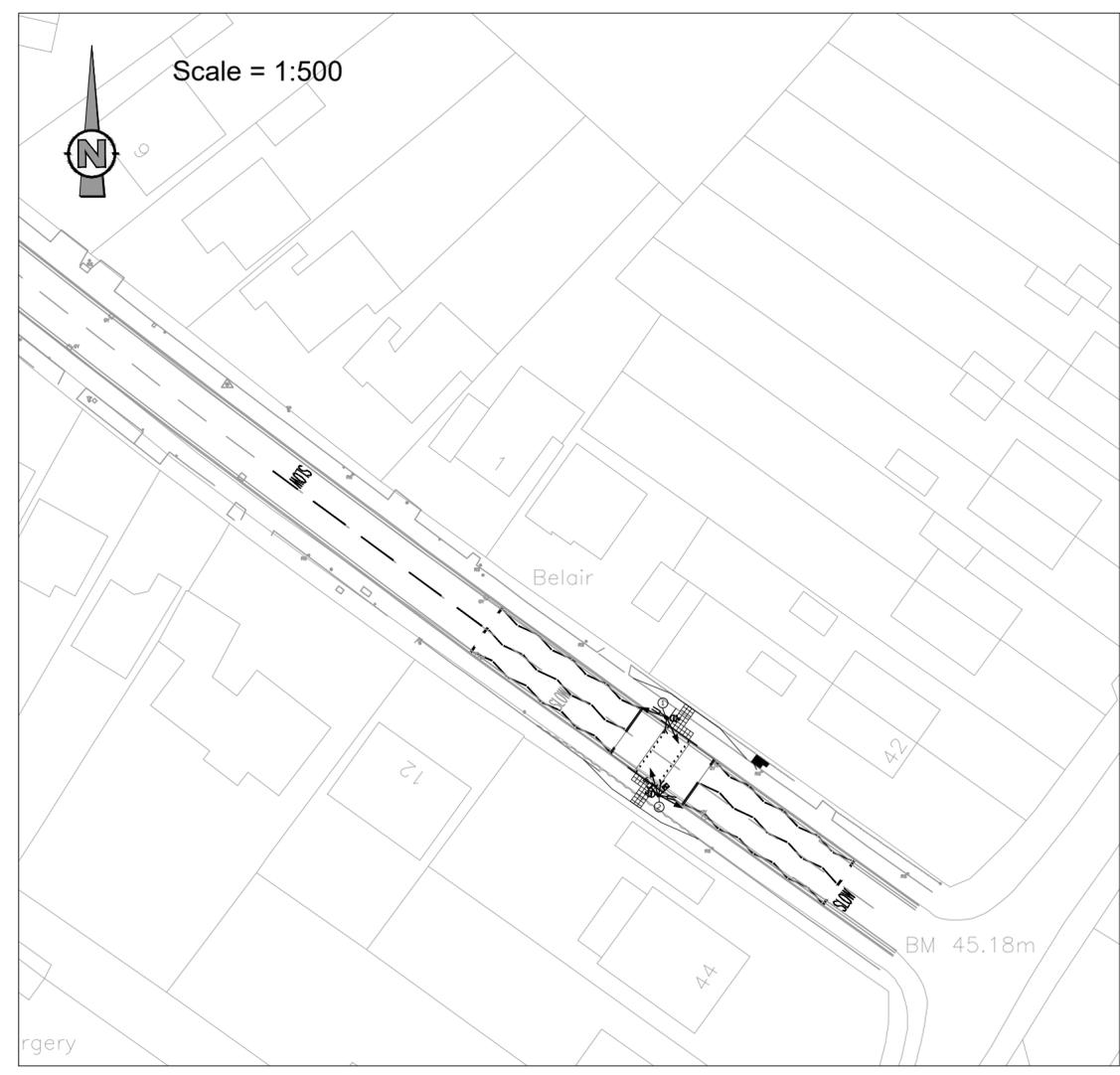
W:\WDC-FP2\DATA\ENG\WOKINGHAM HIGHWAY ALLIANCE\TRAFFIC MANAGEMENT\201819 PROGRAMME\CAPITAL PROJECTS\SIGNAL SCHEMES\SILVERDALE ROAD, EARLEY (LODDON)\DRAWINGS\AUTOCAD\SILV_TS_001.DWG 20/02/2018 12:29:20 Patrick McElroy

DO NOT SCALE



- KEY:**
- (n) Signal pole & pole no.
 - ➔ RAG primary
 - ⊖ Puffin nearside combined push button unit inc tactile and audible
 - ⊖ On-crossing detector
 - ⊖ Microwave Vehicle Detector (MVD)
 - ⊖ Photocell for dimming
 - x - New 100mm duct no. as indicated.
 - ⊖ Traffic signal controller with NAL base
 - ⬢ Electricity supply feeder pillar
 - ⊖ NAL RS115 DF retention socket
 - ⊖ Large duct box (600mm x 450mm)

- NOTES:**
- Drawing to be read in conjunction with the works specification
 - Pre commencement meeting required between signals contractor/ civils contractor and WBC Engineer(s), meeting to be organised by civils contractor
 - The pole setting out details are only a guide, exact positions of poles, ducts, etc. shall be determined onsite by the Signals Engineer.
 - Ducting from RS115 NAL units to adjacent chamber and detector links on approaches to be 1x100mm unless indicated otherwise. all other runs shall be as indicated by number.
 - All new ducting shall be polypropelene, orange in color, smooth bore with 'TRAFFIC SIGNALS' imprinted on the duct. draw cords shall be included in all ducts.
 - All pedestrian nearside units shall have narrow field of view masks installed.
 - Where required pedestrian detection shall be mounted on extended brackets to ensure clear visibility of required detection area.
 - The Photocell for dimming shall be located on a signal head where it will not be affected by street lighting.
 - New signs to diag 7014 'NEW TRAFFIC SIGNALS AHEAD' are required on all signaled approaches, sign to be removed after 3 months by the contractor.
 - All signal heads to be ELV LED type. Where specified on the pole setting out table, poles to be rotated to 45° to ensure correct clearance of street furniture to edge of carriageway.
 - An ORANGE 100mm duct is required between controller and Haldo pillar. 50mm BLACK duct is required if SSE
 - All chambers, boxes and retention sockets to be NAL Ltd, as approved by WBC.
 - All equipment to be installed to manufacturers specifications unless otherwise agreed with the WBC engineer.
 - All poles to have pole numbers affixed prior to SAT.
 - Refer to WBC standard details WSD/500 series for ducting and chamber specifications



Pole and Socket type and setting out details					
Socket Number	Pole Type	Distance - Pole Face to Stopline	Distance - Pole Face to Kerb Edge	Pole Rotation Requirements (see note 9)	NAL Retention Socket Type
1	4m standard (black)	0.5m to studs	0.5m	Rotate 45°	RSII5 Duck Foot
2	4m standard (black)	0.5m to studs	0.5m	Rotate 45°	RSII5 Duck Foot

Note: Small form cabinet to be used

The map is based upon Ordnance Survey material with the permission of Ordnance Survey on behalf of the Controller of HMSO. (c) Crown Copyright. Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings. Wokingham Borough Council Licence Number 100019592

REV	DATE	BY	DESCRIPTION	CHK	APD
B	20/02/2018	PMc	TRAFFIC SIGNALS AMENDMENTS	AL	TRA
A	21/11/2017	PMc	FIRST ISSUED	SR	TRA

DRAWING STATUS: **ISSUED FOR CONSTRUCTION**

WOKINGHAM BOROUGH COUNCIL

Shute End, Wokingham, Berkshire RG40 1BN
Tel: (0118) 974 6000 Web: www.wokingham.gov.uk

CLIENT:	WOKINGHAM BOROUGH COUNCIL
ARCHITECT:	

PROJECT:	SILVERDALE ROAD (LODDON), EARLEY PEDESTRIAN CROSSING INSTALLATION
TITLE:	SIGNAL SCHEMES 2018/19 TRAFFIC SIGNALS ARRANGEMENT

SCALE @ A2:	AS SHOWN	CHECKED:	SR	APPROVED:	TRA
CAD FILE:	SILV_TS_001	DESIGN-DRAWN:	PMc	DATE:	November 2017
PROJECT No:	5049	DRAWING No:	SILV/TS/001	REV:	B

This page is intentionally left blank

Appendix B Designers Response

Scheme Name: Duffield Road, Woodley

Highway Scheme

Stage 1+2 Road Safety Audit

Date Completed: February 2018

Para. No.	Problem Accepted Yes / No	Recommendation Accepted Yes / No	Alternative Measure (describe)	Other Remarks
2.4.1				<p>Problem summary: The existing verge area to the immediate north west of the proposed Zebra pedestrian crossing facility in Duffield Road (Drawing No. DUFF/GA/001 Rev A). Accommodating an existing pedestrian desire line</p> <p>Recommendation: It is Recommended that a short length of new footway of a minimum of 1.8 metres, but preferably 2.0 metres in width, should be provided between the location of the proposed Zebra pedestrian crossing facility and the uncontrolled pedestrian crossing facility across the vehicular access/egress for the Willow Bank Infant and Junior School.</p> <p>In addition, it is Recommended that the existing dropped kerbs should be checked for compliance in respect of kerb upstands (0 to 6 mm) and, that tactile paving should be installed on both sides of the vehicular access/egress for the Willow Bank Infant and Junior School, thus providing full pedestrian connectivity leading to and from the location of the proposed Zebra pedestrian crossing facility.</p> <p>It may also be advisable to take up and relocate the existing yellow pedestrian guardrail adjacent to the existing mature tree, such that it is parallel to the Duffield Road channel line. This will help ensure that having crossed the vehicular access/egress for the Willow Bank Infant and Junior School that pedestrians will remain on the Recommended new length of footway and not stray straight into the adjacent vehicular carriageway.</p>
	Yes	Yes		<p>Recommendation accepted regarding formalising the footway on the desire line and upgrading of the crossing point. As part of the design process we have relocated the crossing point nearer to the main school gated access and away from the junction to encourage/guide children away from crossing on the main junction. Pedestrian railings to remain as part of the guidance for children to be directed to the school pedestrian entrance.</p>

This page is intentionally left blank

Appendix B Designers Response

Scheme Name: Hurricane Way, Woodley

Highway Scheme

Stage 1+2 Road Safety Audit

Date Completed: March 2018

Para. No.	Problem Accepted Yes / No	Recommendation Accepted Yes / No	Alternative Measure (describe)	Other Remarks
2.4.1				<p>Problem summary: The removal of the kerb build outs will reduce the available footway areas for the anticipated platoons and numbers of parents and young school children, particularly at the end of the school day and, should parents and children be inadvertently forced into the vehicular carriageway, there could be an increased risk of vehicular/pedestrian conflicts and subsequent collisions occurring, whereby pedestrians could potentially sustain high levels of personal injury</p> <p>Recommendation: It is Recommended that the existing kerb build out on the northern side of Hurricane Way, i.e., outside the school, should be retained in order to accommodate the anticipated platoons and numbers of parents and young school children congregating on the footway, prior to attempting to cross the Hurricane Way carriageway. It is also Recommended that an increased area of footway/landing pad on the southern side of Hurricane Way should be constructed by utilising the existing verge area identified during the site visit, together with the retention of the existing give way arrangement for west bound vehicular traffic. The retention of both the kerb build out on the northern side of Hurricane Way and the give way arrangement for west bound vehicular traffic will continue to influence vehicular speeds where most needed at the location of the proposed Zebra pedestrian crossing facility.</p> <p>Alternatively, should it be ascertained that wider footways or landing pads cannot be retained or provided as part of a proposed Zebra pedestrian crossing layout, it is Recommended that the existing layout should be retained, particularly as the crossing width is much narrower and, that the existing vehicular speed reduction measures comprising the kerb build outs and the west bound give way arrangement will continue to significantly influence vehicular speeds past the crossing point.</p>
	Yes	No		<p>Problem accepted however the recommendation of retaining the build out with the new crossing installation could cause vehicle priority issues with regards to vehicles stopping to let pedestrians cross. Upon an internal review we have taken into consideration the raised concerns and reviewed the scheme again. To incorporate the comments and maintain safety we have relocated the crossing further</p>

				<p>east and utilised the existing verge on the northern side to increase the footway width (approximately 2.9m). This will aid pedestrian capacity at the crossing whilst allowing room for pedestrian flows to pass. The existing pedestrian railings have been extended to contain children near the main school entrance/exit.</p>
--	--	--	--	---

Appendix B Designers Response

Scheme Name: Keephatch Road, Wokingham

Highway Scheme

Stage 1+2 Road Safety Audit

Date Completed: March 2018

Para. No.	Problem Accepted Yes / No	Recommendation Accepted Yes / No	Alternative Measure (describe)	Other Remarks
General 1.0	Yes	Partially		<p>Item: The site visit has established that the existing carriageway surface at the location indicated is in a poor condition, with evidence of structural issues, crazing and potholes appearing, which now presents potential safety issues for powered two wheelers or pedal cyclists. You may wish to rectify the carriageway surface condition at this location when construction works take place.</p>
				<p>Response: Recommendation noted however this element of works does not fall under the scheme works due to budget constraints. Comments will be passed on the WBC Asset Manager for consideration for future works to be included within the structural maintenance programme. Any localised potholes to be filled in as a short term solution within the crossing facility.</p>
Main report 2.2.1	Yes	Yes		<p>Problem summary: The existing dense vegetation on the eastern side of Keephatch Road could, in the future scenario, impact upon the intervisibility between south bound vehicles and pedestrians seeking to cross from the eastern to the western side of Keephatch Road</p> <p>Recommendation: It is Recommended that the identified existing dense vegetation should be completely removed, to a point where a suitable and appropriate permanent forward visibility envelope for a 30 mph speed limit/design speed will be provided, thus ensuring that intervisibility between vehicles and pedestrians will not be impacted upon in the future scenario. In adopting this Recommendation, it should be remembered that cutting back the dense vegetation on a cyclic basis will also no longer be an on-going maintenance liability for the Highway Authority.</p>
				<p>Response: Recommendation is accepted. Visibility splay adjusted on the general arrangement and a setting out drawing created to outline dimensions for the vegetation clearing extents. The visibility splay line is based on the SSD taken from TD9/93, design speed 60kph and the specification outlined in Para 8.3 from TD 16/87 for the forward</p>

				visibility on the northern approach to the roundabout for a 7.3m single carriageway.
2.2.2	Yes	Yes		<p>Problem summary: The close proximity of the proposed Zebra pedestrian crossing facility to the north bound exit from the Champion Way/Hughes Road/Keephatch Road roundabout could result in an increased risk of nose to tail shunt type collisions occurring between a leading and any following vehicles within the circulatory area of the roundabout</p> <p>Recommendation: It is Recommended that ideally and if possible, the proposed Zebra pedestrian crossing facility should be sited further to the north. As this will result in the crossing being slightly off the ideal pedestrian desire line, visi-rail guard railing panels may be required around the kerblines from Champion Way and Hughes Road into the Keephatch Road northern arm of the roundabout, in order to physically guide and encourage pedestrians to utilise the designated Zebra crossing point.</p> <p>In addition, it is Recommended that red high friction surfacing should be applied between the inscribed circle diameter of the roundabout and the Zebra crossing stop line as opposed to the charcoal coloured high friction surfacing currently indicated, as this will visually enhance the presence of the proposed Zebra pedestrian crossing facility.</p> <p>In further mitigation of the potential need for drivers to have to brake suddenly or heavily, the charcoal coloured high friction surfacing should be extended and also be applied within the inscribed circle diameter of the roundabout, for a suitable and appropriate distance and area.</p>
				<p>Response: Recommendation is partially accepted.</p> <p>Due to site constraints, there is not an option to relocate the crossing point further north and by doing so may – as highlighted – take pedestrians away from the desire line and encourage them to cross on other islands.</p> <p>With regard to the Recommended that red high friction surfacing should be applied between the inscribed circle diameter of the roundabout and the Zebra crossing stop line, this will be taken forward and drawings amended to suit.</p> <p>Upon completion of the scheme, the operational use of the crossing will be monitored before committing to installing high friction surfacing to the circulatory carriageway.</p>

Appendix B Designers Response

Scheme Name: London Road, Twyford

Highway Scheme

Stage 1+2 Road Safety Audit

Date Completed: February 2018

Para. No.	Problem Accepted Yes / No	Recommendation Accepted Yes / No	Alternative Measure (describe)	Other Remarks
2.1.1	Yes	No		<p>Problem summary: No provision for a traffic signal maintenance engineer to stop in immediate proximity to the proposed Puffin pedestrian crossing facility.</p> <p>Recommendation: It is Recommended that a traffic signal maintenance engineer's layby or hardstanding area should be provided in proximity to the proposed Puffin pedestrian crossing facility. In determining a suitable location, the facility should be sited clear of the traffic lanes, traffic signal poles and signal heads and, any existing or proposed pedestrian and pedal cycle routes if possible.</p>
				<p>Response: Ideally on any new installation of any traffic signal installation – especially on newer development schemes – a maintenance bay is included as part of the design. However when installing within an existing location with existing constraints it is not always possible for this item to be included. This location is within close proximity of a town centre which has many constraints including parking traffic restrictions and resident parking features. Unfortunately on this scheme there is nowhere practical within this location for an addition of a maintenance bay however on the occasion of any future maintenance, the Council will occupy the resident parking bays with a temporary TRO.</p>
2.1.2	Yes	Yes		<p>Problem summary: Reducing the potential for skidding and enhancing braking capacity on the north east bound and south west bound approaches to the proposed Puffin pedestrian crossing facility</p> <p>Recommendation: It is Recommended that at the construction stage of the project, a high skid resistant surface course material (formerly referred to as the wearing course), should be provided on the north east bound and south west bound approaches to the new Puffin pedestrian crossing facility, which should include the alignment where pedestrians will actually be crossing as surfacing defects are also evident in this area.</p> <p>It is suggested that as a minimum, the length of high skid resistant surface course material (formerly referred to as the wearing course), should draw a relationship between the potential vehicular approach speeds and, the minimum stopping distances identified in the Highway Code, which confirms a shortest/overall stopping</p>

				<p>distance for a 30 mph limit as a linear distance of 75 feet or approximately 23 metres.</p> <p>The shortest/overall stopping distances assume dry road conditions, a car with good brakes and an alert driver. It is acknowledged that generally, a longer linear distance is or can be provided in these circumstances.</p> <p>Alternatively, it is Recommended that the existing carriageway surface is the subject of suitable and adequate repairs prior to the application of a traditional red high friction anti-skid surface material. However, it should be noted that the Audit Team do have reservations regarding this lifespan of this Recommendation, due to the significant levels of existing carriageway surface defects that would be present beneath a traditional red high friction anti-skid surface application</p>
				<p>Response:</p> <p>This scheme is in conjunction with our structural maintenance team who are carrying out resurfacing works at the same location.</p> <p>The surfacing works are to be carried out after the installation of the pedestrian crossing with the new markings being laid on the new surface.</p> <p>The structural maintenance surfacing scheme covers the required extents and the surfacing will be laid with a high PSV to negate the requirement for anti-skid material.</p> <p>A note is present on the main general arrangement highlighting this element and the contractor is aware if carrying out the two schemes together as one entity.</p>
2.2.1	Yes	Partially		<p>Problem summary: The presence of the existing on-street parking bays (when occupied), the existing on-carriageway bus cage (when occupied), and, the existing overhanging vegetation are likely to result in a restriction or limited forward visibility to the nearside primary traffic signal heads. This could result in an increased risk of stop line overshoots and subsequent vehicular/pedestrian collisions occurring when traffic signal changes take place or, in addition, an increased risk of nose to tail collisions occurring between a leading and any following vehicles on the approach to the proposed Puffin pedestrian crossing facility stop lines.</p> <p>Recommendation: It is Recommended that at 1.05 metres above carriageway level, the maximum geometric stopping sight distances that can be achieved on the north east bound and south west bound approaches to the nearside primary traffic signal heads should be accurately established and confirmed accordingly.</p> <p>Should the maximum geometric stopping sight distance achievable on the north east bound and south west bound approaches to the primary traffic signal heads be established as being sub-standard, it is Recommended that the linear length of the existing on-street parking bays should be truncated and, that the existing on-carriageway bus cage should be</p>

				<p><i>relocated, in order to improve a drivers forward visibility to the nearside primary traffic signal heads. It is also Recommended that the identified overhanging vegetation at Location 6 should be cut right back or completely removed as necessary/appropriate, in order to improve a drivers forward visibility to the nearside primary traffic signal head.</i></p>
				<p>Response: The standard specifies that each traffic lane should have a clear vision of at least one primary signal associated with its particular movement, from a distance equivalent to the site stopping distance. Although, at times, the nearside signal head could be blocked by either a stationary bus or parked vehicle, the primary signal head on the central island will be in clear view to all approaching vehicles on each of the main approaches.</p> <p>Concerns raised within the audit are acknowledged however due to the existing constraints, the bus stop location is unable to be relocated and the parking bays cannot be reduced at this time. However it is recommended as part of this response, that upon the installation of the signalised crossing that these elements should be reviewed as part of a Stage 3 road safety audit to determine if these locations pose a concern to the vehicular approach.</p> <p>.</p> <p>It should be noted that an uncontrolled pedestrian crossing point in this location has been operational for a number of years. By upgrading this location to a signalised crossing should make the location safer for pedestrians to cross and to highlight the crossing point to vehicular traffic whilst reducing speeds on the approach to the signals.</p> <p>In regards to the overhanging vegetation, this will be addressed on the drawing to trim back to the boundary line at a suitable height.</p>

This page is intentionally left blank

Appendix B Designers Response

Scheme Name: Murray Road, Wokingham

Highway Scheme

Stage 1+2 Road Safety Audit

Date Completed: February 2018

Para. No.	Problem Accepted Yes / No	Recommendation Accepted Yes / No	Alternative Measure (describe)	Other Remarks
<i>General 1.0</i>	Yes	Yes		<i>The scheme Drawing No. MURR/GA/001 Rev A for Murray Road, has the title Keephatch Road in the Project box. You may wish to amend this to reflect the Murray Road project</i>
				<i>Noted and revised on drawing</i>
<i>General 2.0</i>	Yes	Yes		<i>The site visit has established the presence of an existing school crossing patrol sign and post within the northern footway of Murray Road, to the south west of the proposed Zebra crossing facility. It is assumed that the existing school crossing patrol sign and post will be superfluous to requirements when the Zebra crossing facility is operational and, thus, should be removed accordingly.</i>
				These are being removed by Traffic Management team prior to the works being carried out.
<i>General 3.0</i>	Yes	Partially		<i>The site visit has established that there is no tactile paving present at the junction for the adjacent West Oak Care Home junction along the southern side of Murray Road. You may wish to consider installing tactile paving and adjusting the dropped kerbs as necessary as part of the proposed Zebra crossing facility project.</i>
				Observation accepted, however these are outside the scheme extents and will not be included as part of the works. However, these type of items will be picked up as part of a new future strategy to make school routes safer by Traffic Management team.
<i>General 4.0</i>	Yes	Yes		<i>The site visit has established that surface water ponding is occurring along the northern kerblines of Murray Road, to the east of the Walter Infant school vehicular access and leading into the channel line of Oxford Road. You may wish to investigate and rectify this situation as soon as practicable.</i>
				Observation will be forwarded on to our Drainage team.

This page is intentionally left blank

Appendix B Designers Response

Scheme Name: Silverdale Road, Earley

Highway Scheme

Stage 1+2 Road Safety Audit

Date Completed: February 2018

Para. No.	Problem Accepted Yes / No	Recommendation Accepted Yes / No	Alternative Measure (describe)	Other Remarks
2.1.1	No	No		<p>Problem summary: Potential surface water ponding issues could result in a potential slip or trip hazard for pedestrians at the proposed Puffin pedestrian crossing facility.</p> <p>Recommendation: It is Recommended that at the construction stage of the project, additional surface water drainage gullies should be installed within the south western and north eastern channel lines of Silverdale Road on both sides of the carriageway, immediately before the dropped kerbs of the proposed Puffin pedestrian crossing facility</p>
				<p>Due to the adequate longitudinal gradient (averaging out 1 in 20 across the scheme extents), it is highly unlikely that the channel surface water flow would stray onto the pavement as the main directional is down not across.</p> <p>Existing note on the drawing will be revised to outline that the dropped kerb should be installed 5mm to aide drainage.</p>
2.2.1	Yes	No		<p>Problem summary: No provision for a traffic signal maintenance engineer to stop in immediate proximity to the proposed Puffin pedestrian crossing facility</p> <p>Recommendation: It is Recommended that a traffic signal maintenance engineer's layby or hardstanding area should be provided in proximity to the proposed Puffin pedestrian crossing facility. In determining a suitable location, the facility should be sited clear of the traffic lanes, traffic signal poles and signal heads and, any existing or proposed pedestrian and pedal cycle routes if possible.</p>
				<p>Ideally on any new installation of any traffic signal installation – especially on newer development schemes – a maintenance bay is included as part of the design. However when installing within an existing location with existing constraints it is not always possible for this item to be included.</p> <p>This location is within close proximity to private residential drives and limited highway land.</p> <p>Unfortunately on this scheme there is nowhere practical within this location for an addition of a maintenance bay.</p>
2.3.1	No	No		<p>Problem summary: The vertical alignment and the slight crest in the Silverdale Road carriageway could result in an increased risk of stop line overshoots and subsequent vehicular/pedestrian collisions occurring when traffic signal changes take place or, in addition, an increased risk of nose to tail shunt type collisions</p>

				<p>occurring between a leading and any following vehicles on the south east bound approach to the proposed Puffin pedestrian crossing facility stop line</p> <p>Recommendation: It is Recommended that at 1.05 metres above carriageway level, the maximum geometric stopping sight distances that can be achieved on the south east bound approach to the nearside primary and offside secondary traffic signal heads should be accurately established and confirmed accordingly.</p> <p>Should the maximum geometric stopping sight distance achievable on the north east bound and south west bound approaches to the primary traffic signal heads be established as being sub-standard, it is Recommended that high level traffic signal poles and traffic signal heads should be provided as part of the proposed Puffin pedestrian crossing facility installation.</p>
				<p>Overlooking the existing topographical survey levels, there is no crest present on the centreline within gom on the eastbound approach. However the perception from site (regarding the crest) maybe where the steepest of the gradient plateau's out to a reduced gradient.</p> <p>We feel the overall forward visibility is within the prescribed visibility envelope and can be seen on either approach.</p> <p>Due to the steepness on the site, we have – on the eastbound approach – over specified the site stopping distance to 50m (for 30mph residential road) with anti-skid material.</p>
General 1.0	Yes	Partially		<p><i>The site visit has established the Silverdale Road carriageway surface has evidence of crazing and cracking on the approaches to the proposed Puffin crossing facility. As it is proposed to apply red coloured high friction surfacing on the approaches to the proposed Puffin crossing facility, concern arises that the existing identified carriageway surface defects may impact upon the lifespan of the high friction surfacing, unless remedial work is undertaken in advance. Should the lifespan of the high friction surfacing be impacted upon by the existing carriageway surface condition, there could be a resultant potential reduced braking capacity for vehicles in the future, particularly on the downhill south east bound approach to the proposed Puffin crossing facility.</i></p>
				<p>The observation is noted and we will get the contractor, prior to the works, to establish the suitability of laying the anti-skid on the existing surface and guaranteeing the life expectancy of the works. If the contractor deems the surface not acceptable, then a new wearing course inlay will be provided within the scheme extents.</p>

CALL IN SCHOOL CROSSING PATROLLER

Cllr Keith Baker, former Executive Member for Highways.

This call in is based around some key issues which are incorrect therefore making the call in invalid.

Decision to remove the patrollers was made in 2015.

The petitioners of this call-in accept that the decision to remove the funding for school crossing patrollers was made in the 2015/16 Medium Term Financial Plan. (Please see their supporting information 3.c) It is common sense that the removal of funding would translate to removal of the discretionary service.

The question to be asked, if this was an issue, is why did the petitioners not raise this issue in 2015? If there is a problem now then there was a problem then but there was no sign of any action from the opposition.

Silence continued

On 1st March 2017 a press release was issued publicising that the long-awaited change to the School Crossing was starting with phase 1 being consulted on. Phase 1 was removing any school crossing patroller who at that time were operating on an existing pedestrian crossing.

On May 3rd 2017 a press release was issued at the end of the consultation confirming that the planned removal would go ahead.

At this point there was no comment and phase1 was fully implemented.

This is not a cost saving

It is abundantly clear that the cost of the discretionary school crossing patroller service is a much lower cost to that of the capital cost of putting in a pedestrian crossing. This confirms that the rationale was to improve the safety of children and others crossing the road. A pedestrian crossing provides a safer crossing than an individual stepping out into the road.

It has a series of by products as well as follows:

- It has become increasingly difficult to recruit new school crossing patrollers when the incumbent retires. This has meant many schools without any safe crossing. Phase 2 has addressed that issue with the provision of a pedestrian crossing where it was not possible to recruit a new patroller.
- A pedestrian crossing is available 24 hours a day and not just at school times thus giving all residents a safe crossing.

Pedestrian Crossings are not as safe as an individual stepping out into the middle of the road.

The experience of the phase 1 implementation is that safety has indeed increased with less incidents occurring compared to the period when the patrollers were in place.

If the removal of the school crossing patrollers from phase 1 was acceptable because there was a pedestrian crossing in place then it should follow that phase 2 must be acceptable as long as there is a pedestrian crossing in their place.

SPECIFICS.

- 1 The decision has contravened Section 1.4.2 a) of the Council's constitution, in that the action being proposed is not proportionate to the desired outcome.
 - a. A blanket decision has been made to replace all remaining School Crossing Patrols with pedestrian crossings, despite the different characteristics and requirements of the locations.
 - **ANSWER:** *all locations have had an independent safety audit which has resulted in different types of pedestrian crossings being installed to reflect the characteristics and requirements of the locations.*
 - b. The desired outcome appears to be to save money yet the costs both of the crossings and the school crossing patrol have not been fully or correctly stated. None of the other options quoted had any costs provided.
 - **ANSWER:** *see the comment on cost savings on initial sheet. Safety should not be a cost driven action, there should be no cost constraints on safety improvements. If that means a safer solution costs more money then the council should absorb that extra cost.*
 - c. The decision was made on the basis of costs for crossings which have not yet been designed-as a redesign is taking place at 4 sites due to the first design not being suitable; it is therefore also unclear whether a crossing is actually the right answer for those locations, as well as the costs being unknown.
 - **ANSWER:** *I refer to the independent safety audit for each site which clearly indicates a pedestrian crossing is the safest option. Again please see my comment on cost savings on initial sheet.*

- 2 The decision has contravened Section 1.4.2 b) of the Council's constitution, in that due consultation and the taking of professional advice from officers has not occurred.
 - a. The consultation was not complete at the point at which the decisions to withdraw the service were made
 - **ANSWER:** *As the petitioners have acknowledged in their supporting evidence the decision to withdraw the service was made in 2015.*
 - b. The consultation was not carried out at an early stage, and was not meaningful, and contravened the council's own rules on consultation
 - **ANSWER:** *"meaningful" is a very subjective word. The consultation was no different from the phase 1 consultation back in 1st March 2017. It was not deemed to be "not meaningful" then so not sure what has changed to make it "not meaningful" now.*
 - b. There was no consultation with ward members on the withdrawal of the service
 - **ANSWER:** *Every councillor will have received notification of the consultation press release dated January 22nd 2018 so it is not correct to imply that ward members were unaware of the consultation. Any one of the councillors were free to contact me in my former role of Executive Member for Highways but none did.*
 - c. The replacement crossings were designed without reference to ward members and before the consultation was carried out- and ward members were not even informed until mid-July that the crossings had been designed.
 - **ANSWER:** *this is a classic "chicken and egg" conundrum. The provisional design of the crossings had to be completed in order to answer the basic questions which would inevitably come from the ward members. Obvious ones would be what type of crossing is it going to be? Where is it going to be located? And so on.*

- d. Wokingham Town Council's consultation response has not been taken into consideration
- **ANSWER:** *all consultation responses are taken into consideration. Unfortunately not all can be acted upon as they are not relevant to the consultation in question.*
- e. Letters putting the staff at risk of redundancy were issued before the consultation was concluded
- **ANSWER:** *The HR process requires that an "at risk" notice is issued to the school crossing patrollers. In order to try and ensure that all pedestrian crossings were installed in time the "at risk" notices had to be issued when they were. Receiving an "at risk" letter does not mean the individual would actually be made redundant.*
- f. The council's responses to the points made by respondents are inadequate
- **ANSWER:** *Officers have responded in a professional way as you would expect them to. The petitioners say they are inadequate but that is a highly subjective view and probably reflects their views that the responses are wrong.*
- g. At least one crossing was scheduled to be installed before the decision on the outcome of the consultation took place
- **ANSWER:** *There is nothing to say in the constitution that a pedestrian crossing cannot be installed independently of any other consideration. In fact, a couple of the planned crossings are being installed where no school crossing patroller exists due to recruitment difficulties. If the outcome of the consultation was to rescind the 2015 decision then the crossing in question would be still in place.*
- h. At least one safety audit was done during the school holidays
- j. No Equality Impact Assessments accompanied the information in the report
- k. It is not clear that all guidelines for the provision of safe crossings have been observed
- **ANSWER:** *officers have clearly stated that each site would be assessed in accordance to Road Safety GB's "School Crossing Patrol Service Guidelines" and they have confirmed they have done so. This is a clear accusation that these officers are not telling the truth which is a serious allegation as this is clearly a safety issue. I suggest the petitioners provide their evidence as to why they are making this allegation.*
- l. It is not clear that all the evidence comparing the appropriateness of SCP versus crossings has been taken into account
- **ANSWER:** *this is not an either or situation. The council's aim is to provide the safest way for children, their parents and other residents to cross the road safely. Officers' views are that a pedestrian crossing, especially one that is pedestrian controlled will always be safer than an individual simply stepping out into the road and stopping the traffic.*
- 3 The decision has contravened Section 1.4.2 d) of the Council's constitution, in that openness has not been observed.
1. At least one of the crossings was designed in February but this was not communicated to ward members
 - **ANSWER:** *This has been addressed in answer to 2.c*
 2. Some information was shared with individual members of the public but was not made available to all
 - **ANSWER:** *Residents regularly contact the council asking for specific items of information. When the officers respond they do not make that conversation available to all.*
 3. No overall plan for the removal of the SCP was made available following the removal of the funding in the 2015 Medium Term Financial Plan

- **ANSWER:** *In the 2016/17 Medium Term Financial Plan it highlighted the initial plan was to seek alternative funding sources outside of the council to enable the service to continue. That was unsuccessful which then initiated phase 1. The press release clearly defines the plan behind phase 1 in the press release dated 1st March 2017. The penultimate paragraph in that press release clearly states that the plan is to subsequently assess the remaining 7 sites, the best way to ensure children continue to get to school safely.*
4. The decision has contravened Section 1.4.2 e) of the Council's constitution, in that clarity of aims and desired outcomes has not been achieved.
- a. The recommendations refer to a proposal for provide safe, permanent crossings-yet 4 of the crossing proposals have been rejected in the period shortly before the Executive meeting that made the decision, and therefore the decision has been made before there is certainty that the recommendation can be delivered, or that the costs are as given in the report
 - **ANSWER:** *the rejection is not on technical grounds but on requests from the ward members to make minor changes. This reinforces the comments made previously (see 2c) about the design is a provisional one pending that input from ward members. None of the rejections, I believe, are saying that a crossing is not a viable safety option.*
 - b. Due to the failure to present all costs for all options it is not possible for the Executive to have come to a properly informed decision
 - **ANSWER:** *This is back to the allegation that this is a cost cutting exercise. There is no price cap on the cost of children's safety. If a pedestrian crossing is a safer method of crossing a road then funds should be found to implement them.*
 - c. Due to the failure to present the Equality Impact Assessments it is not possible for the Executive to have come to a properly informed decision
5. The decision has contravened Section 1.4.2 f) of the Council's constitution, in that the details of all the options and reasons for the decision have not been recorded
- a. A set of options has been laid out but it excludes the most obvious option of replacing some but not all of the patrollers with crossings
 - **ANSWER:** *as started previously this is not an either or situation. A pedestrian crossing is a safer option for crossing a road than an individual stepping out in the road to stop traffic.*
 - b. There was a presumption that no funding was available for the service to continue, whereas a supplementary estimate could have been used to find the money, but was not considered
 - **ANSWER:** *this question assumes the driving force for this is cost cutting. As I have said several times this is a safety action aimed at increasing the safety for children to cross the road to school.*

This page is intentionally left blank